

Board of Selectmen

John A. Foresto, Chair

Maryjane White, Vice-Chair

Richard A. D'Innocenzo, Clerk

Dennis P. Crowley

Glenn D. Trindade



Medway Town Hall
155 Village Street
Medway, MA 02053
Phone (508) 533-3264
Fax (508) 321-4988

TOWN OF MEDWAY

COMMONWEALTH OF MASSACHUSETTS

Board of Selectmen's Meeting

December 21, 2015, 7:00 PM

Sanford Hall, Town Hall

155 Village Street

Agenda

7:00 PM

- Call to order; Recitation of the Pledge of Allegiance
- Executive Session, Exemption 3 – To discuss strategy with respect to litigation [Maritime Housing Fund, LLC v Planning Board and ZBA; Review of 8/31/15 Exec. Session Minutes and Vote on Their Release]
- Public Comments

Other Business

1. Appointment – Board of Assessors – Kathryn Regan
2. Introductions – Communications Director Mary Becotte; Treasurer/Collector Joanne Russo; Assistant Fire Chief Michael Fasolino
3. Authorization to Expend Mass. Council on Aging Grant - \$600
4. Authorization to Expend State 911 Grant - \$30,194
5. Approval – State (MassDOT) Contract to Move Fire Alarm Cable and Master Boxes for Fire Dept. - \$25,386
6. Presentation – New Design Review Guidelines
7. Discussion – Agricultural Committee (General Bylaws Art. II)
8. Discussion with Capital Improvement Planning Com. – Fiscal Year 2017-2021 Capital Plan
9. Discussion – Five-Year Revenue and Expense Forecast
10. Approval – One-Day Liquor License Applications
 - a. Linda Spiller – Thayer Homestead – 7/30/16
11. Town Counsel Appointment
12. Action Items from Previous Meeting

The listed matters are those reasonably anticipated by the Chair to be discussed at the meeting. Not all items may in fact be discussed and other items not listed may also be brought up for discussion to the extent permitted by law.

13. Approval of Warrants
14. Approval of Minutes
15. Town Administrator's Report
16. Selectmen's Reports

Upcoming Meetings, Agenda and Reminders

January 4, 2015 ---- Regular Meeting

January 9, 2015 ---- Legislative Breakfast

The listed matters are those reasonably anticipated by the Chair to be discussed at the meeting. Not all items may in fact be discussed and other items not listed may also be brought up for discussion to the extent permitted by law.

AGENDA ITEM #1

Appointment – Board of Assessors – Kathryn Regan

Associated back up materials attached:

- Email from Ms. Regan

Proposed motion: I move that the Board appoint Ms. Regan to the Board of Assessors for a term to expire June 30, 2018.

-----Original Message-----

From: Kathryn Regan

Sent: Monday, December 14, 2015 3:30 PM

To: Assessor

Subject: Board of Assessors

I understand that a position has become available on the Assessors Board. I am interested in filling that position. Please let me know what the requirements are.

I can be reached at | _____ or by phone at |

Thank you.

Kathy

Kathryn Regan

Sent from my iPhone

AGENDA

ITEM #2

Introductions –

Mary Becotte, Communications Director

Joanne Russo, Treasurer/Collector

Michael Fasolino, Asst. Fire Chief

No associated back up materials.

AGENDA ITEM #3

Authorization to Expend Grant Funds – Mass. Council on Aging - \$600

Associated back up materials attached:

- Grant expenditure authorization form
- Grant

Proposed motion: I move that the Board authorize the expenditure of this grant in the amount of \$600.

TOWN OF MEDWAY
NOTICE OF GRANT AWARD

DEPARTMENT: COA DATE: 12/10/15

PERSON RESPONSIBLE FOR GRANT EXPENDITURE: Missy Dziczek

NAME OF GRANT: MCOA Direct Grant Agreement

GRANTOR: Massachusetts Council on Aging

GRANT AMOUNT: \$600.00

GRANT PERIOD: January 1, 2016 - June 30, 2016

SCOPE OF GRANT/
ITEMS FUNDED Live Your Life Well One Day Event -
speakers and food

IS A POSITION BEING
CREATED: NO

IF YES: CAN FRINGE BENEFITS BE PAID FROM GRANT? _____

ARE MATCHING TOWN
FUNDS REQUIRED? NO

IF MATCHING IS NON-MONETARY (MAN HOURS, ETC.) PLEASE SPECIFY:

IF MATCHING IS MONETARY PLEASE GIVE ACCOUNT NUMBER AND DESCRIPTION OF TOWN FUNDS
TO BE USED:

ANY OTHER EXPOSURE TO TOWN? NO

IS THERE A DEADLINE FOR BOARD OF SELECTMEN APPROVAL: NO

APPROVAL SIGNATURES _____

DATE _____

DEPARTMENT HEAD MUST SUBMIT THIS FORM, A COPY OF THE GRANT APPROVAL, AND A COVER LETTER TO THE SELECTMEN'S OFFICE FOR APPROVAL OF DEPARTMENT TO EXPEND



MCOA Direct Grant Agreement (FY2016)

This Direct Grant Agreement (this "Agreement") is entered into by and between the Massachusetts Association of Councils on Aging, Inc. ("MCOA"), and the MEDWAY COA (the "GRANTEE").

GRANTOR		GRANTEE	
Massachusetts Association of Councils on Aging 116 Pleasant Street, Suite 306 Easthampton, MA 01027 413-527-6425 Primary Contact : Mary Kay Browne Email: marykay@mcoaonline.com		Entity: Address: Phone: Primary Contact: Email:	
GRANT PERIOD		January 1, 2016 – June 30, 2016	
Total Funds Awarded		\$ 6000. ⁰⁰	
<input type="checkbox"/> Job Fair and Networking Meeting Groups for Older Workers (\$1,500)	<input type="checkbox"/> Falls Prevention Exercise Class Instructor (\$1,680)	<input type="checkbox"/> Regional Monthly Caregiver Support Group with On Site Respite (\$1,200)	
<input type="checkbox"/> Keep Moving Walking Club (\$600)	<input type="checkbox"/> Regional Monthly Bereavement Support Group (\$500)	<input type="checkbox"/> Aging Mastery Program \$1,600 deploying in house staff as leader <input type="checkbox"/> AMP \$3,100 to hire a leader	
<input type="checkbox"/> Convening an Age Friendly Planning Project Team (\$2,000)	<input checked="" type="checkbox"/> Live Your Life Well 1-Day Event (\$600)	<input type="checkbox"/> Innovative Project ()	
MCOA's Funding Source Name:	FY'16 Service Incentive Grant from the MA Executive Office of Elder Affairs		
Method of Payment	<input checked="" type="checkbox"/> Cost Reimbursement	<input type="checkbox"/> Advance Payment and periodic cost reimbursement payments. See schedule details in Section III C.	
Approved for MCOA by:		Approved for GRANTEE by AUTHORIZING AGENT:	
Name: David P. Stevens		Name:	
Title: Executive Director		Title:	
Signature: Sign on page 9.		Signature: Sign on page 9.	

WITNESSETH

WHEREAS, it is the mission of MCOA to support the independence of older adults in the Commonwealth of Massachusetts by advocating for programs and services to meet their needs, promote the growth and quality of Councils on Aging and senior centers, and strengthen the professional skills of Council on Aging staff; and

WHEREAS, the GRANTEE wishes to provide certain services, as detailed below, in furtherance of MCOA's mission; and

WHEREAS, in compensation for the GRANTEE's services, the parties desire to enter into an agreement whereby MCOA will distribute certain funds received under its Fiscal Year 2016 Service Incentive Grant from the Massachusetts Executive Office of Elder Affairs to the GRANTEE;

NOW, THEREFORE, in consideration of the mutual promises and representations set forth herein, it is agreed by and between the parties hereto as follows:

I. PURPOSE

The GRANTEE understands and agrees that the purpose of this Agreement is to implement new programs for the benefit of older adults, generally sixty (60) years of age and older, in the Commonwealth of Massachusetts, based upon the program and project descriptions set forth in Exhibit 1 hereto.

The primary requirements of all funded projects, to be undertaken over the six (6) month period from January 1, 2016 to June 30, 2016, shall include:

- Designating a program-level staff member to serve as the project coordinator who will take responsibility for working with MCOA on initiative components including the implementation and evaluation of the project.
- Focusing upon increasing the participation of older adults throughout the time period of the initiative.
- Increasing local public awareness of the needs of older adults to lead healthy and fulfilling lives and the relevant issues underlying the initiative(s).
- Providing reports of participation figures, lessons learned during the project period, participant feedback, and required financial and end-of-grant reporting.

- Sharing best practices and project management tools.
- Participating in periodic conference calls and/or responding promptly to periodic emails aimed at gathering information, such as case studies and best practices that will be helpful to MCOA, the Executive Office of Elder Affairs, or other organizations in their resource development and/or project management efforts.
- Hosting MCOA staff for project site visits, if appropriate.

In the event that the GRANTEE is unable to perform any of the above-described services, or any of the services described in Exhibit 1, consistent with the letter and spirit of this Agreement, the GRANTEE shall immediately so notify MCOA in writing, and MCOA shall have the right (but not the obligation) to terminate this Agreement for cause.

II. PARTIES' RELATIONSHIP

A. Grantee's Legal Authority

The GRANTEE represents that the GRANTEE is in compliance with all applicable state and federal requirements and standards, and that it possesses the legal authority pursuant to any proper, appropriate and official motion, resolution or action passed or taken, giving the GRANTEE authority to enter into this Agreement, receive the funds authorized by this Agreement, and to perform the services the GRANTEE has obligated itself to perform under this Agreement.

The person or persons signing and executing this Agreement on behalf of the GRANTEE, or representing themselves as persons authorized to sign and execute this Agreement on behalf of the GRANTEE, do hereby represent that they have been fully authorized by the GRANTEE to execute this Agreement on behalf of the GRANTEE and to validly and legally bind the GRANTEE to all terms, conditions, performances and provisions set forth in this Agreement.

MCOA shall terminate this Agreement for cause, effective immediately, if it becomes apparent to MCOA that the GRANTEE, or any person acting or purporting to act on behalf of the GRANTEE, lacks legal authority to enter into this Agreement. In such event, the GRANTEE shall immediately reimburse and repay MCOA for any and all monies received from MCOA under the terms of or in connection with this Agreement.

B. Independent Contractors

Each of the parties is an independent contractor and neither party is, nor shall be considered to be, an agent, distributor or representative of the other. Neither party shall act or represent itself, directly or by implication, as an agent of the other or in any manner assume or create any obligation on behalf of, or in the name of, the other. Neither party has authorization to enter into any contracts, assume any obligations or make any warranties or representations on behalf of the other party. Nothing in this Agreement shall be construed to establish a relationship of co-partner or joint venture between the parties. MCOA shall not be responsible and shall have no obligation to GRANTEE, the employees of GRANTEE or any governing body to withhold Federal, State, or local income tax, or MCOA's employee portion of FICA or other payroll taxes, and other taxes relating from any individual assigned by GRANTEE to provide services under this Agreement; GRANTEE shall indemnify, defend and hold MCOA harmless from all liabilities, costs and expenses, including without limitation reasonable attorneys' fees resulting from all third party claims brought against MCOA for any FICA, FUTA, or SUI contributions and any other payroll taxes or any claims of any nature, by GRANTEE or other resources providing the Services under this Agreement.

C. Indemnification

The GRANTEE shall indemnify, defend and hold harmless MCOA for any and all liabilities, costs, claims and expenses, including, without limitation, reasonable attorneys' fees, arising from any third party claims brought against MCOA for personal injury or death or damage to real property or intangible or tangible personal property, to the extent caused by the negligent acts or omissions of the GRANTEE.

D. Lobbying Prohibited

The person signing this Agreement on behalf of the GRANTEE certifies, to the best of his or her knowledge and belief, that:

1. The GRANTEE will not attempt to influence any member of the Congress, or any State or local legislator, to favor or oppose any legislation or appropriation with respect to this Agreement.
2. Grant funds shall not be used for publicity or propaganda purposes, for the preparation, distribution, or use of any kit, pamphlet, booklet, publication, radio, television, or film presentation designed to support or defeat legislation pending before the Congress, or any State or local legislature.

3. Grant funds shall not be used to pay the salary or expenses of any grant or contract recipient, or agent acting for such recipient; related to any activity designed to influence legislation or appropriation pending before the Congress, or any State or local legislature.

E. Subcontracting Restrictions

The GRANTEE shall not subcontract or delegate any work under this Agreement to any third party except with MCOA's prior written consent, which must include MCOA's written approval of any sub-contracting agreement. The GRANTEE shall be solely responsible for the performance of any subcontractor, subsidiary or affiliate, and shall be liable for and indemnify, defend and hold MCOA harmless for any wrongdoing by any such subsidiary, affiliate or subcontractor, including without limitation the misuse or misappropriation of any funds.

F. Publicity

1. Either party may freely use in advertising, publicity, web sites, press releases, or otherwise, the name of the other party, or refer to the existence of this Agreement and the project(s) funded herein.
2. Any materials produced with grant funding should contain an acknowledgement to the effect that "This [product] has been produced in [part] [full] from a grant awarded to the Massachusetts Association of Councils on Aging by the Massachusetts Executive Office of Elder Affairs. Any opinions expressed herein are solely those of [GRANTEE]."
3. Use of MCOA's logo is encouraged.

III. ADMINISTRATIVE PROVISIONS

A. Term of Agreement

The term of this Agreement shall be the Grant Period specified in the table at page 1 above.

B. Grantee's Reporting Requirements

1. On March 15, 2016 and May 15, 2016, the GRANTEE shall report in writing to MCOA via the MCOA's website, www.mcoaonline.com. The purpose of these written reports shall be to

provide particulars regarding the GRANTEE's progress toward the goals and requirements of the funded project.

2. No later than July 15, 2016, and together with the GRANTEE's shall provide a final project narrative report (the "Final Report") to MCOA. The Final Report shall accompany the GRANTEE's Q2 invoice (See Section III.D. below) and shall consist of a brief description of project outcomes and lessons learned, so that MCOA and other agencies may improve upon the project model. The final invoice shall also contain an explanation of any variance in spending of ten percent (10%) or greater.
3. The GRANTEE shall respond promptly to the MCOA's emails and communications, and web-based surveys aimed at gathering information, such as case studies and best practices, which will be helpful to the other organizations in their outreach and enrollment efforts. The GRANTEE shall share samples of materials and tools that are developed under this project.

C. Total Funds Awarded

The GRANTEE shall be compensated in accordance with the payment schedule shown on Page 1 and at Section III.D. below. Payments will be distributed by check only after the parties' execution of this Agreement, and in satisfaction of complete and valid invoices submitted by the GRANTEE to MCOA. It is expressly understood that in no event shall the total distribution of funds to the GRANTEE under the terms of this Agreement exceed the amount set forth in the table on Page 1.

D. Payment Schedule

Disbursements to the GRANTEE shall be in the form of reimbursement for the GRANTEE's actual expenditures as of the end of each quarter, following MCOA's receipt and approval of a quarterly invoice for Q1 (January through March) and a quarterly invoice for Q2 (April through June). To be considered complete and valid, the Q2 invoice must be accompanied by the Final Report, unless MCOA waives this requirement in writing.

The GRANTEE shall be solely responsible for ensuring the accuracy of all invoices and any supporting documentation submitted to MCOA. MCOA shall terminate this Agreement for cause, effective immediately, if it becomes apparent to MCOA that the GRANTEE has, knowingly or otherwise, submitted falsified invoices other documentation.

MCOA may, with the consent of the GRANTEE, adjust or correct any invoice. A copy of any adjusted or corrected invoice shall be promptly sent to the GRANTEE.

Contingent upon MCOA's receipt of grant funding from the Executive Office of Elder Affairs, all complete and valid invoices shall, to the extent possible, be satisfied by MCOA within thirty (30) days of receipt. MCOA shall promptly notify the GRANTEE of any expected delay of payment beyond the specified period.

The GRANTEE is encouraged to use MCOA's Direct Grant Invoice Form, attached hereto as Exhibit 2, to prepare quarterly invoices.

E. Termination

1. Termination for Cause

In the event that either party fails to substantially perform under the terms of this Agreement, the other party shall be entitled to terminate this Agreement for cause in accordance with Section 3 ("Notice of Termination") below.

If the GRANTEE fails to provide any of the services contemplated herein, or fails to make sufficient progress, so as to endanger performance, MCOA shall notify the GRANTEE of such unsatisfactory performance in writing. The GRANTEE shall have ten (10) business days in which to respond with a written plan, acceptable to MCOA, for promptly addressing the deficiencies. The GRANTEE's failure to respond satisfactorily within the appointed time shall entitle (but not obligate) MCOA to terminate this Agreement for cause.

2. Termination for Convenience

Either party shall be entitled to terminate this Agreement without cause on thirty (30) days written notice. In the event of such termination, and subject to Section III.B ("Total Funds Awarded") above, the GRANTEE shall be entitled to equitable compensation for any allowable services actually and satisfactorily performed under this Agreement through the effective date of termination, and such compensation shall constitute the extent and entirety of the GRANTEE's recourse against MCOA in connection with this Agreement.

3. Notice of Termination

Termination shall be effectuated by one party's delivery to the other party of a Notice of Termination, specifying whether the termination is for cause or for convenience. In the event of termination for cause, the Notice of Termination shall also include a brief description of

reason(s) for termination. Except as otherwise provided in this Agreement, the effective date of termination shall be ten (10) days from a party's receipt of a Notice of Termination for cause, and thirty (30) days from a party's receipt of a Notice of Termination for Convenience. Notice of Termination shall be delivered by hand or certified mail to the party's address first set forth above.

IV. MISCELLANEOUS

A. Severability

The provisions of this Agreement are severable and if for any reason a clause, sentence, paragraph or other part of this Agreement shall be determined to be invalid by a court, federal agency, board or commission having jurisdiction over the subject matter thereof, such invalidity shall not affect those provisions of this Agreement which can be given effect without the invalid provision.

B. Successors and Assigns

The GRANTEE shall not assign, transfer or delegate any of the rights or obligations under this Agreement without the prior written consent of MCOA. This Agreement and all of its provisions shall inure to the benefit of and become binding upon the parties and the successors and permitted assigns of the respective parties.

C. Survival

Any provision of this Agreement which by its nature must survive termination or expiration in order to achieve the fundamental purposes of this Agreement shall survive any termination or expiration of this Agreement.

D. Governing Law

This Agreement shall be governed by, and construed in accordance with, the laws of the Commonwealth of Massachusetts without giving effect to choice of law principles. Any action brought under or in relation to this Agreement shall be brought in a court having subject matter jurisdiction and located in Hampshire County, Massachusetts.

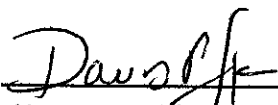
E. Entire Agreement

This Agreement, together with the Exhibits hereto, constitutes the parties' entire agreement concerning the work and services to be performed hereunder.

IN WITNESS HEREOF, the parties hereto have caused this instrument to be executed on the day and year first above written.

**MASSACHUSETTS ASSOCIATION OF
COUNCILS ON AGING, INC.**

GRANTEE

By: 
Name: David P. Stevens
Title: Executive Director
Date: _____

By: _____
Authorizing Agent Name:
Title: _____
Date: _____

By: _____
Primary Contact (Implementation Manager)
Name:
Title: _____
Date: _____

AGENDA ITEM #4

Authorization to Expend State 911 Grant - \$30,194

Associated back up materials attached:

- Grant expenditure authorization form
- Grant correspondence and page 1 of contract

Proposed motion: I move that the Board authorize the expenditure of the State 911 grant in the amount of \$30,194.

**TOWN OF MEDWAY
NOTICE OF GRANT AWARD**

DEPARTMENT: Police and Fire Communications DATE: 10-Dec-15

PERSON RESPONSIBLE FOR GRANT EXPENDITURE: Chief Tingley/ Lt. Boultenhouse

NAME OF GRANT: 911 Support and Incentive Grant FY 2016

GRANTOR: State 911 Department

GRANT AMOUNT: \$30,194

GRANT PERIOD: FY 2016

SCOPE OF GRANT/
ITEMS FUNDED Salary Reimbursement for 911 Certified Dispatchers

IS A POSITION BEING
CREATED: No

IF YES: CAN FRINGE BENEFITS BE PAID FROM GRANT? _____

ARE MATCHING TOWN
FUNDS REQUIRED? No

IF MATCHING IS NON-MONETARY (MAN HOURS, ETC.) PLEASE SPECIFY:

IF MATCHING IS MONETARY PLEASE GIVE ACCOUNT NUMBER AND DESCRIPTION OF TOWN FUNDS
TO BE USED:

ANY OTHER EXPOSURE TO TOWN?
None

BOARD OF SELECTMEN:

ACTION DATE _____

**DEPARTMENT HEAD MUST SUBMIT THIS FORM AND A COPY OF THE GRANT APPROVAL
TO THE TOWN ADMINISTRATOR'S OFFICE FOR APPROVAL BY THE BOS TO EXPEND
THE FUNDS RECEIVED FOR THE PURPOSE OF THE GRANT MGL 44 S53A**

ONCE APPROVED - ORIGINAL TO TOWN ACCOUNTANT



The Commonwealth of Massachusetts
EXECUTIVE OFFICE OF PUBLIC SAFETY AND SECURITY
STATE 911 DEPARTMENT
1380 Bay Street, Building C ~ Taunton, MA 02780-1088
Tel: 508-828-2911 ~ TTY: 508-828-4572 ~ Fax: 508-828-2585
www.mass.gov/e911



CHARLES D. BAKER
Governor

DANIEL BENNETT
Secretary of Public Safety
and Security

FRANK POZNIAK
Executive Director

December 9, 2015

Lieutenant William Boultenhouse
Medway Police Department
315 Village Street
Medway, MA 02053

Dear Lieutenant Boultenhouse,

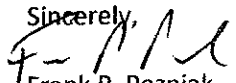
The Commonwealth of Massachusetts, State 911 Department would like to thank you for participating in the **FY 2016 State 911 Department Support and Incentive Grant** program.

For your files, attached please find a copy of the executed contract and the final approved Appendix A: Personnel List for your grant. Please note your contract start date is **December 9, 2015** and will run through June 30, 2016. Please keep in mind that there shall be no reimbursement for costs incurred prior to the effective date of the contract and all goods and services **MUST** be received on or before June 30, 2016.

Reimbursement requests should be submitted to the Department within **thirty (30) days** of the date on which the cost is incurred. We have made the request for payment forms available on our website www.mass.gov/E911. For any questions related to this process, please contact Michelle Hallahan at 508-821-7216. Please note that funding of reimbursement requests received more than three (3) months after the close of the fiscal year under which costs were incurred cannot be guaranteed.

If, in the future, you would like to make any changes to the authorized signatory, the contract manager, and/or the budget worksheet, please e-mail those proposed changes to 911DeptGrants@state.ma.us. Grantees are strongly encouraged to submit final, year-end budget modification requests on or before April 30, 2016.

Sincerely,


Frank P. Pozniak
Executive Director

cc: FY 2016 Support and Incentive Grant File



COMMONWEALTH OF MASSACHUSETTS ~ STANDARD CONTRACT FORM

This form is jointly issued and published by the Executive Office for Administration and Finance (ANF), the Office of the Comptroller (CTR) and the Operational Services Division (OSD) as the default contract for all Commonwealth Departments when another form is not prescribed by regulation or policy.

Any changes to the official printed language of this form shall be void. Additional non-conflicting terms may be added by Attachment. Contractors may not require any additional agreements, engagement letters, contract forms or other additional terms as part of this Contract without prior Department approval. Click on hyperlinks for definitions, instructions and legal requirements that are incorporated by reference into this Contract. An electronic copy of this form is available at www.mass.gov/osc under [Guidance For Vendors - Forms](#) or www.mass.gov/osd under OSD Forms.

CONTRACTOR LEGAL NAME: <u>Town of Medway</u> (and d/b/a): <u>Medway Police Department</u>		COMMONWEALTH DEPARTMENT NAME: <u>State 911 Department</u> MMARS Department Code: <u>EPS</u>	
Legal Address: (W-9, W-4, T&C): <u>155 Village Street Medway MA 02053</u>		Business Mailing Address: <u>1380 Bay Street, Building C, Taunton, MA 02780</u>	
Contract Manager: <u>Chief Allen Tingley</u>		Billing Address (if different):	
E-Mail: <u>atingley@medwaypolice.com</u>		Contract Manager: <u>Cindy Reynolds</u>	
Phone: <u>508-533-3212</u>	Fax: <u>508-533-3216</u>	E-Mail: <u>911DeptGrants@state.ma.us</u>	
Contractor Vendor Code: <u>VC6000191877</u>		Phone: <u>508-821-7299</u>	Fax: <u>508-828-2585</u>
Vendor Code Address ID (e.g. "AD001"): <u>AD001</u> (Note: The Address Id Must be set up for EFT payments.)		MMARS Doc ID(s): <u>CT SUPG</u>	
		RF/Procurement or Other ID Number: <u>FY2016 SUPPORT & INCENTIVE GRANT</u>	
<input checked="" type="checkbox"/> NEW CONTRACT PROCUREMENT OR EXCEPTION TYPE: (Check one option only) <input type="checkbox"/> <u>Statewide Contract</u> (OSD or an OSD-designated Department) <input type="checkbox"/> <u>Collective Purchase</u> (Attach OSD approval, scope, budget) <input checked="" type="checkbox"/> <u>Department Procurement</u> (Includes State or Federal grants <u>815 CMR 2.00</u>) (Attach RFR and Response or other procurement supporting documentation) <input type="checkbox"/> <u>Emergency Contract</u> (Attach justification for emergency, scope, budget) <input type="checkbox"/> <u>Contract Employee</u> (Attach <u>Employment Status Form</u> , scope, budget) <input type="checkbox"/> <u>Legislative/Legal or Other:</u> (Attach authorizing language/justification, scope and budget)		<input type="checkbox"/> CONTRACT AMENDMENT Enter Current Contract End Date <u>Prior</u> to Amendment: <u> </u> , 20 <u> </u> . Enter Amendment Amount: \$ <u> </u> . (or "no change") AMENDMENT TYPE: (Check one option only. Attach details of Amendment changes.) <input type="checkbox"/> <u>Amendment to Scope or Budget</u> (Attach updated scope and budget) <input type="checkbox"/> <u>Interim Contract</u> (Attach justification for Interim Contract and updated scope/budget) <input type="checkbox"/> <u>Contract Employee</u> (Attach any updates to scope or budget) <input type="checkbox"/> <u>Legislative/Legal or Other:</u> (Attach authorizing language/justification and updated scope and budget)	
The following COMMONWEALTH TERMS AND CONDITIONS (T&C) has been executed, filed with CTR and is incorporated by reference into this Contract. <input checked="" type="checkbox"/> <u>Commonwealth Terms and Conditions</u> <input type="checkbox"/> <u>Commonwealth Terms and Conditions For Human and Social Services</u>			
COMPENSATION: (Check ONE option): The Department certifies that payments for authorized performance accepted in accordance with the terms of this Contract will be supported in the state accounting system by sufficient appropriations or other non-appropriated funds, subject to intercept for Commonwealth owed debts under 815 CMR 9.00. <input type="checkbox"/> <u>Rate Contract</u> (No Maximum Obligation. Attach details of all rates, units, calculations, conditions or terms and any changes if rates or terms are being amended.) <input checked="" type="checkbox"/> <u>Maximum Obligation Contract</u> Enter Total Maximum Obligation for total duration of this Contract (or <u>new Total</u> if Contract is being amended). \$ <u>32,194.00</u>			
PROMPT PAYMENT DISCOUNTS (PPD): Commonwealth payments are issued through <u>EFT</u> 45 days from invoice receipt. Contractors requesting <u>accelerated</u> payments must identify a PPD as follows: Payment issued within 10 days <u> </u> % PPD; Payment issued within 15 days <u> </u> % PPD; Payment issued within 20 days <u> </u> % PPD; Payment issued within 30 days <u> </u> % PPD. If PPD percentages are left blank, identify reason: <input checked="" type="checkbox"/> <u>agree to standard 45 day cycle</u> <input type="checkbox"/> <u>statutory/legal or Ready Payments (G.L.c. 29, § 23A);</u> <input type="checkbox"/> <u>only initial payment</u> (subsequent payments scheduled to support standard EFT 45 day payment cycle. See Prompt Pay Discounts Policy.)			
BRIEF DESCRIPTION OF CONTRACT PERFORMANCE or REASON FOR AMENDMENT: (Enter the Contract title, purpose, fiscal year(s) and a detailed description of the scope of performance or what is being amended for a Contract Amendment. Attach all supporting documentation and justifications.) For disbursement of funds under the State 911 Department FY 2016 PSAP and Regional Emergency Communication Center Support and Incentive Grant as authorized and awarded in compliance with program guidelines and grantee's approved application.			
ANTICIPATED START DATE: (Complete ONE option only) The Department and Contractor certify for this Contract, or Contract Amendment, that Contract obligations: <input checked="" type="checkbox"/> <u>1. may be incurred as of the Effective Date</u> (latest signature date below) and <u>no</u> obligations have been incurred <u>prior</u> to the <u>Effective Date</u> . <input type="checkbox"/> <u>2. may be incurred as of <u> </u>, 20<u> </u>, a date LATER than the Effective Date below and no obligations have been incurred prior to the Effective Date.</u> <input type="checkbox"/> <u>3. were incurred as of <u> </u>, 20<u> </u>, a date PRIOR to the Effective Date below, and the parties agree that payments for any obligations incurred prior to the Effective Date are authorized to be made either as settlement payments or as authorized reimbursement payments, and that the details and circumstances of all obligations under this Contract are attached and incorporated into this Contract. Acceptance of payments forever releases the Commonwealth from further claims related to these obligations.</u>			
CONTRACT END DATE: Contract performance shall terminate as of <u>June 30, 2016</u> , with no new obligations being incurred after this date unless the Contract is properly amended, provided that the terms of this Contract and performance expectations and obligations shall survive its termination for the purpose of resolving any claim or dispute, for completing any negotiated terms and warranties, to allow any close out or transition performance, reporting, invoicing or final payments, or during any lapse between amendments.			
CERTIFICATIONS: Notwithstanding verbal or other representations by the parties, the "Effective Date" of this Contract or Amendment shall be the latest date that this Contract or Amendment has been executed by an authorized signatory of the Contractor, the Department, or a later Contract or Amendment Start Date specified above, subject to any required approvals. The Contractor makes all certifications required under the attached <u>Contractor Certifications</u> (incorporated by reference if not attached hereto) under the pains and penalties of perjury, agrees to provide any required documentation upon request to support compliance, and agrees that all terms governing performance of this Contract and doing business in Massachusetts are attached or incorporated by reference herein according to the following hierarchy of document precedence, the applicable <u>Commonwealth Terms and Conditions</u> , this Standard Contract Form including the <u>Instructions and Contractor Certifications</u> , the Request for Response (RFR) or other solicitation, the Contractor's Response, and additional negotiated terms, provided that additional negotiated terms will take precedence over the relevant terms in the RFR and the Contractor's Response only if made using the process outlined in <u>801 CMR 21.07</u> , incorporated herein, provided that any amended RFR or Response terms result in best value, lower costs, or a more cost effective Contract.			
AUTHORIZING SIGNATURE FOR THE CONTRACTOR: X: <u>[Signature]</u> Date: <u>12/18/15</u> (Signature and Date Must Be Handwritten At Time of Signature) Print Name: <u>Allen M Tingley</u> Print Title: <u>Chief of Police</u>		AUTHORIZING SIGNATURE FOR THE COMMONWEALTH: X: <u>[Signature]</u> Date: <u>12/18/15</u> (Signature and Date Must Be Handwritten At Time of Signature) Print Name: <u>Frank Pozniak</u> Print Title: <u>Executive Director</u>	

AGENDA ITEM #5

**Approval – State (MassDOT)
Contract to Move Fire Alarm Cable
and Master Boxes for the Fire
Dept. - \$25,386**

Associated back up materials attached.

- Contract

Proposed motion: I move that the Board authorize the Chairman to Execute the Mass Dept. of Transportation contract in the amount of \$25,386 as presented.



Charles D. Baker, Governor
Karyn E. Polito, Lieutenant Governor
Stephanie Pollack, Secretary & CEO
Frank DePaola, Administrator



September 9, 2015

SUBJECT: Medway – Route 109
MassDOT Standard Contract Form Number 89686
Utility Force Account Terms and Conditions

Mr. Jeffrey Lynch
Medway Fire Department
44 Milford Street
Medway, MA 02053

Dear Mr. Lynch:

Forwarded for signature are two (2) copies of the unexecuted MassDOT Standard Contract Form for the above-described project between the Massachusetts Department of Transportation ("MassDOT") and the Medway Fire Department, Inc.; plus one (1) copy of the Contractor Authorized Signatory Listing form.

This utility was authorized to commence preliminary engineering for this project upon receipt of the design submissions on September 28, 2011. This engineering and review of plans resulted in the scope, estimated costs, and duration spreadsheet included within the attached agreement.

Note: In accordance with the Federal regulations (23 U.S.C. 313 and 23 CFR 635.410) all utility reimbursable agreements must be in accordance with Buy America requirements regarding manufacturing processes for steel and iron products or predominantly of steel or iron. Therefore the utility must also sign a Buy America Certificate of Compliance for all force accounts (attached within this packet). This certificate must be signed whether or not the agreement meets the minimum requirements for iron or steel components.

Please have all FIVE (5) FORMS (2 copies of agreement, each agreement containing a signature page on the front and a Buy America form at the back, and 1 copy of Authorized Signatory form) signed by the proper authorities, and return all originals within 7 days to:

Guy Rezendes, Utilities Engineer
Massachusetts DOT
10 Park Plaza, Room 6340
Boston, Massachusetts 02116

This does not constitute a Notice to Proceed. MassDOT will issue a Notice to Proceed to your office following MassDOT approval and signature.

If you have any questions or comments, please contact me at 857-368-9489 or email them to guy.rezendes@state.ma.us.

Sincerely,

Guy Rezendes, P.E.
Utilities Engineer

Enclosures

Ten Park Plaza, Suite 4160, Boston, MA 02116
Tel: 857-368-4636, TTY: 857-368-0655
www.mass.gov/massdot

MASSDOT STANDARD CONTRACT FORM



This form is issued and published by the Massachusetts Department of Transportation (MassDOT or Department). Any changes to the official printed language of this form shall be void. Additional non-conflicting terms may be added by Attachment. Contractors may not require any additional agreements, engagement letters, contract forms or other additional terms as part of this Contract without prior Department approval. Click on hyperlinks for definitions, instructions and legal requirements that are incorporated by reference into this Contract. An electronic copy of this form is available at www.mass.gov/osc under [Guidance For Vendors - Forms](#) or www.mass.gov/osd under [OSD Forms](#).

CONTRACTOR LEGAL NAME: Town of Medway d/b/a Medway Fire Department		DEPARTMENT NAME: Department of Transportation - Highway	
		MMARS Department Code: DOT	
Legal Address: (W-9, W-4,T&C): 155 Village St Medway, MA 02053-1147		Business Mailing Address: 10 Park Plaza Room 6340 Boston, MA 02116	
Contract Manager: Jeffrey Lynch		Billing Address (if different):	
E-Mail:		Contract Manager: Guy Rezendes	
Phone: 508-533-3213	Fax:	E-Mail: Guy.Rezendes@state.ma.us	
Contractor Vendor Code: VC 8000 191 877		Phone: 657-368-0489	Fax: 657-368-0632
Vendor Code Address ID (e.g. "AD001"): AD 001		MMARS Doc ID(s): CT DOT 0608 INTF 00X0 2016 A00 69686	
(Note: The Address Id Must be set up for EFT payments.)		RFR/Procurement or Other ID Number: Utility Force Account 89686	
<input checked="" type="checkbox"/> NEW CONTRACT		<input type="checkbox"/> CONTRACT AMENDMENT	
PROCUREMENT OR EXCEPTION TYPE: (Check one option only)		Enter Current Contract End Date <i>Prior</i> to Amendment: _____, 20____.	
<input type="checkbox"/> Stalewide Contract (OSD or an OSD-designated Department)		Enter Amendment Amount: \$ _____ (or "no change")	
<input type="checkbox"/> Collective Purchase (Attach OSD approval, scope, budget)		AMENDMENT TYPE: (Check one option only. Attach details of Amendment changes.)	
<input type="checkbox"/> Department Procurement (includes State or Federal grants 815 CMR 2.00) (Attach RFR and Response or other procurement supporting documentation)		<input type="checkbox"/> Amendment to Scope or Budget (Attach updated scope and budget)	
<input type="checkbox"/> Emergency Contract (Attach justification for emergency, scope, budget)		<input type="checkbox"/> Interim Contract (Attach justification for Interim Contract and updated scope/budget)	
<input type="checkbox"/> Contract Employee (Attach <u>Employment Status Form</u> , scope, budget)		<input type="checkbox"/> Contract Employee (Attach any updates to scope or budget)	
<input checked="" type="checkbox"/> Legislative/Legal or Other: (Attach authorizing language/justification, scope and budget)		<input type="checkbox"/> Legislative/Legal or Other: (Attach authorizing language/justification and updated scope and budget)	
The following COMMONWEALTH TERMS AND CONDITIONS (T&C) has been executed, filed with CTR and is incorporated by reference into this Contract.			
<input checked="" type="checkbox"/> MassDOT Terms and Conditions <input type="checkbox"/> Commonwealth Terms and Conditions For Human and Social Services			
COMPENSATION: (Check ONE option): The Department certifies that payments for authorized performance accepted in accordance with the terms of this Contract will be supported in the state accounting system by sufficient appropriations or other non-appropriated funds, subject to intercept for MassDOT/Commonwealth owed debts under 815 CMR 9.00.			
<input type="checkbox"/> Rate Contract (No Maximum Obligation. Attach details of all rates, units, calculations, conditions or terms and any changes if rates or terms are being amended.)			
<input checked="" type="checkbox"/> Maximum Obligation Contract Enter Total Maximum Obligation for total duration of this Contract (or new Total if Contract is being amended), \$ <u>25,386.00</u>			
PROMPT PAYMENT DISCOUNTS (PPD): Commonwealth payments are issued through EFT 45 days from invoice receipt. Contractors requesting accelerated payments must identify a PPD as follows: Payment issued within 10 days ___% PPD; Payment issued within 15 days ___% PPD; Payment issued within 20 days ___% PPD; Payment issued within 30 days ___% PPD. If PPD percentages are left blank, identify reason: ___agree to standard 45 day cycle <input checked="" type="checkbox"/> statutory/legal or Ready Payments (G.L. c. 29, § 23A); ___ only initial payment (subsequent payments scheduled to support standard EFT 45 day payment cycle. See <u>Prompt Pay Discounts Policy</u> .)			
BRIEF DESCRIPTION OF CONTRACT PERFORMANCE or REASON FOR AMENDMENT: (Enter the Contract title, purpose, fiscal year(s) and a detailed description of the scope of performance or what is being amended for a Contract Amendment. Attach all supporting documentation and justifications.) The Medway Fire Department has performed design services for the relocation of its fire alarm facilities (which includes the scope of work, estimated budget and estimated duration), and will perform the actual utility relocation in order to accommodate the MassDOT construction project, pursuant to MassDOT's letter to the utility, attached hereto. The specific scope of work, basis of compensation, and method of compensation are set forth in the "Utility Force Account Terms and Conditions" attached hereto. MassDOT is authorized to reimburse the utility owner for all of the above-described work. FY 2016-20			
ANTICIPATED START DATE: (Complete ONE option only) The Department and Contractor certify for this Contract, or Contract Amendment, that Contract obligations:			
<input checked="" type="checkbox"/> 1. may be incurred as of the <u>Effective Date</u> (latest signature date below) and no obligations have been incurred prior to the <u>Effective Date</u> .			
<input type="checkbox"/> 2. may be incurred as of _____, 20____, a date LATER than the <u>Effective Date</u> below and no obligations have been incurred prior to the <u>Effective Date</u> .			
<input type="checkbox"/> 3. were incurred as of _____, 20____, a date PRIOR to the <u>Effective Date</u> below, and the parties agree that payments for any obligations incurred prior to the <u>Effective Date</u> are authorized to be made either as settlement payments or as authorized reimbursement payments, and that the details and circumstances of all obligations under this Contract are attached and incorporated into this Contract. Acceptance of payments forever releases the Commonwealth and MassDOT from further claims related to these obligations.			
CONTRACT END DATE: Contract performance shall terminate as of <u>July 1</u> , 20 <u>19</u> , with no new obligations being incurred after this date unless the Contract is properly amended, provided that the terms of this Contract and performance expectations and obligations shall survive its termination for the purpose of resolving any claim or dispute, for completing any negotiated terms and warranties, to allow any close out or transition performance, reporting, invoicing or final payments, or during any lapse between amendments.			
CERTIFICATIONS: Notwithstanding verbal or other representations by the parties, the "Effective Date" of this Contract or Amendment shall be the latest date that this Contract or Amendment has been executed by an authorized signatory of the Contractor, the Department, or a later Contract or Amendment Start Date specified above, subject to any required approvals. The Contractor makes all certifications required under the attached <u>Contractor Certifications</u> (incorporated by reference if not attached hereto) under the pains and penalties of perjury, agrees to provide any required documentation upon request to support compliance, and agrees that all terms governing performance of this Contract and doing business in Massachusetts are attached or incorporated by reference herein according to the following hierarchy of document precedence, the applicable <u>MassDOT Terms and Conditions</u> , this Standard Contract Form including the <u>Instructions and Contractor Certifications</u> , the Request for Response (RFR) or other solicitation, the Contractor's Response, and additional negotiated terms, provided that additional negotiated terms will take precedence over the relevant terms in the RFR and the Contractor's Response only if made using the process outlined in <u>801 CMR 21.07</u> , incorporated herein, provided that any amended RFR or Response terms result in best value, lower costs, or a more cost effective Contract.			
AUTHORIZING SIGNATURE FOR THE CONTRACTOR:		AUTHORIZING SIGNATURE FOR MASSDOT:	
X: _____ Date: _____ (Signature and Date Must Be Handwritten At Time of Signature)		X: _____ Date: _____ (Signature and Date Must Be Handwritten At Time of Signature)	
Print Name: _____		Print Name: _____	
Print Title: _____		Print Title: _____	

Acts

2009

CHAPTER 25 AN ACT MODERNIZING THE TRANSPORTATION SYSTEMS OF THE COMMONWEALTH

Whereas, The deferred operation of this act would tend to defeat its purpose, which is forthwith to reorganize and restructure transportation agencies in the commonwealth to help address anticipated funding deficiencies, therefore it is hereby declared to be an emergency law, necessary for the immediate preservation of the public convenience.

Be it enacted by the Senate and House of Representatives in General Court assembled, and by the authority of the same as follows:

SECTION 1. Section 17 of chapter 6 of the General Laws, as appearing in the 2006 Official Edition, is hereby amended by striking out, in line 13, the words ", the Massachusetts aeronautics commission".

SECTION 2. The second sentence of section 17A of said chapter 6, as most recently amended by section 1 of chapter 27 of the acts of 2008, is hereby amended by striking out the words "secretary of transportation and public works" and inserting in place thereof the following words:- "secretary of transportation".

SECTION 3. Sections 57, 58 and 59 of said chapter 6 are hereby repealed.

SECTION 4. Section 8C of chapter 6A of the General Laws, inserted by section 6 of chapter 233 of the acts of 2008, is hereby amended by striking out subsection (a) and inserting in place thereof the following subsection:-
(a) There shall be established a structurally deficient bridge improvement program coordination and oversight council. The council shall consist of a chair appointed by the governor, the secretary of administration and finance, the secretary of transportation, the secretary of energy and environmental affairs, the administrator of the division of highways of the Massachusetts Department of Transportation, and the commissioner of capital asset management and maintenance, or their designees.

SECTION 5. Sections 19, 19 1/2 and 19A of said chapter 6A are hereby repealed.

SECTION 6. Section 103 of said chapter 6A is hereby repealed.

SECTION 7. Section 104 of said chapter 6A is hereby repealed.

SECTION 8. The General Laws are hereby amended by inserting after chapter 6A the following chapter:-

**Chapter 6C
MASSACHUSETTS DEPARTMENT of TRANSPORTATION**

<https://malegislature.gov/Laws/SessionLaws/Acts/2009/Chapter25/Print>

Section 3. The department shall have all powers necessary or convenient to carry out and effectuate its purposes including, without limiting the generality of the foregoing, the power to:

(21) place and maintain or grant permission by easement or otherwise to any public utility, corporation or person to place and maintain on or under or within the state highway system, the metropolitan highway system or the turnpike, or any part thereof, ducts, pipes, pipelines, mains, conduits, cables, wires, towers, poles or other structures to be so located as not to interfere with the safe and convenient operation and maintenance of the state highway system, the metropolitan highway system or the turnpike, and to contract with any such public utility, corporation or person for such permission on such terms and conditions as may be fixed by the department; provided, however, that in case of any such relocation or removal of facilities, the public utility, corporation or person owning or operating the same, its successors or assigns may maintain and operate such facilities, with the necessary appurtenances, in the new location for as long a period and upon the same terms and conditions as it had the right to maintain and operate such facilities in their former location; and provided further, that otherwise, the department shall have the power to grant such easements over any real property held by the department as will not, in the judgment of the department, unduly interfere with the operation of any of its mass transportation facilities;

UTILITY FORCE ACCOUNT
TERMS AND CONDITIONS
Number 89686

FEDERAL AID PROJECT NUMBER: STP-002S(837)

MUNICIPALITY: MEDWAY

BRIDGE NUMBER: M-13-12

COUNTY OF: NORFOLK

ROUTE 109 INCLUDING BRIDGE NUMBER M-13-12

This Agreement made and entered into by and between the MASSACHUSETTS DEPARTMENT OF TRANSPORTATION, hereinafter called "MassDOT", and the TOWN OF MEDWAY FIRE DEPARTMENT, hereinafter called the "Owner", and

WHEREAS, MassDOT proposes to reconstruct a portion of Route 109 including Bridge Number M-13-12 in the above Municipality, in Commonwealth of Massachusetts, and

WHEREAS, the Owner has installed and is operating and/or maintaining fire alarm distribution facilities hereinafter called the "Facilities", and

WHEREAS, the reconstruction of said bridge will make necessary the alteration and/or relocation of the Facilities hereinafter called the "Adjustment" to the extent shown on the construction plans, and

WHEREAS, the eligibility of Federal participation has been established in accordance with the current edition of Title 23, CFR, of the United States Department of Transportation, Federal Highway Administration, hereinafter called "FHWA".

NOW THEREFORE, in consideration of the premises and mutually dependent covenants herein contained, it is hereby agreed between the parties hereto as follows:

WORK ORDER SYSTEM
DIVISION OF WORK

Section 1. All necessary labor, materials, equipment and other services shall be furnished by the Owner in accordance with 23 code of Federal Regulations Part 645. Utilities and the work shall be done by method a, b or c as shown below:

- a) By the Owner's Forces.
- b) By a contractor, who is the lowest pre-qualified bidder based on appropriate solicitation.
- c) By a continuing contract subject to approval by MassDOT under which certain work is regularly performed for the Owner.

When either method b or c is used, the Owner agrees to supply to MassDOT's Resident Engineer a copy of the contract between the Owner and the contractor, prior to the start of any work by the Owner's contractor.

Any relocation of utilities or utility facilities carried out under this agreement, in accordance with M.G.L. c. 6C, sec. 44(c), which is not performed by employees of the utility owner shall be subject to the prevailing wage law, M.G.L. c. 149, sections 26 to 27F. If the utility relocation work is performed by employees of the utility owner, the work will not be subject to the prevailing wage rates.

Prevailing wage rates for the utility's contractor(s) are available from MassDOT. The wage rates shall either accompany this agreement or be provided to the utility company under separate cover. The wage rates shall be incorporated into the actual costs incurred by the utility's contractor for the relocation work's cost. MassDOT will obtain annual updates for the wage rates. The wage schedule and updates must be posted at the worksite for the duration of the work. The utility owner must keep and submit to MassDOT weekly the certified payroll records and a signed statement of compliance (email is acceptable) for each subcontractor. No reimbursements under MGL c. 149, section 27B will be made until certified payroll records for the work have been received.

For further information on the prevailing wage requirement and to obtain wage schedules, go to the Department of Labor Standards website at www.mass.gov/dols.

Section 2. It is understood and agreed that certain preliminary engineering has been performed by the Owner during the period from September 28, 2011 to the date of this agreement.

Section 3. The Owner hereby agrees that except for minor revisions, the scope of the work necessary to complete the Adjustment cannot be altered or enlarged without the prior written approval of MassDOT. If a change other than minor is necessary or desirable, the owner agrees to submit a written request to MassDOT for approval with a revised force account and plan. Written approval from MassDOT is not necessary when a condition results in an actual threat to the public safety or convenience, but a written report shall be submitted by the owner to the Engineer as soon as feasible.

Section 4. The District Highway Director shall notify the Owner, in writing, when the Owner is authorized to start work described in this agreement. When MassDOT's general contract has been executed, the District Highway Director shall furnish the name of the general contractor and state that the contractor has been notified that an Agreement is in force between the Commonwealth and the Owner. The Owner hereby agrees to notify the District Highway Director, in writing, prior to the start of any construction work by the Owner in connection with this agreement.

Section 5. The Owner hereby agrees to coordinate its force account work with the Resident Engineer of the project on a daily basis at a mutually agreeable time and to advise the Resident Engineer when work is suspended for a period of one day or more, when work is resumed and when work is completed on the project. Such coordination of work between the Owner and the Resident Engineer, can be done either verbal or in writing. The Owner also agrees that any work not previously scheduled to be done on a Saturday, Sunday or Legal Holiday will be done only after a three day advance notice has been given to the Resident Engineer, or lesser notice with written approval by the Resident Engineer, except when a condition exists which is an actual threat to the public safety or convenience.

Section 6. The Owner hereby agrees that its foreman or other authorized representative shall furnish to the Resident Engineer a written daily report of labor, materials and/or units installed (as shown and detailed on "Exhibit A"), equipment, and salvage, exclusive of prices, in connection with work by the Owner within two (2) working days from the close of the working day reported. Such daily report shall be submitted to the Resident Engineer on Form CSD-123 entitled: "Daily Work Force Report" (attached hereto and/or available on the MassDOT website)

Section 7. Upon the completion of the physical work, the Owner shall complete a "Utility Completion form" (attached hereto and/or available on the MassDOT website) to the District Highway Director showing the work actually done in connection with this Agreement.

Section 8. At the time of final billing or prior thereto, the Owner agrees to furnish MassDOT, in writing an explanation of any overruns or underruns amounting to more than twenty-five percent (25%) of the preliminary estimated cost. If the actual cost of the force account work does not exceed one thousand dollars (\$1000), an explanation will not be necessary unless the overrun or underrun is in excess of fifty percent (50%).

WORK ORDER SYSTEM **DIVISION OF EXPENSE**

Section 1. The Commonwealth will reimburse the Owner of the actual costs and related indirect costs accumulated in accordance with a work order accounting procedure prescribed by the applicable Federal or State regulatory body for the adjustment of the Owner's Facilities under this Agreement, including the preliminary engineering performed by the Owner during the period from September 28, 2011 to the date of this Agreement less the salvage value of materials removed, determined in accordance with the applicable provisions of the Federal Highway Administration presently in effect.

MassDOT and the Owner prior to the start of construction will agree upon a relocation schedule. If the relocation is completed within the agreed upon time, MassDOT will reimburse the Owner 100% of its costs. If the Owner has inexcusably failed to meet the agreed upon schedule after written notice to the Owner outlining such failure, MassDOT will assess the Owner 1% of the amount to be reimbursed for each work day delayed.

If the relocation is so far behind schedule due to the Owner's sole negligence so that a contractor successfully brings a claim against MassDOT for costs of delay, the costs of said claim will be deducted from the relocation monies due the owner.

Section 2. Notification to commence preliminary engineering was issued dated September 28, 2011. An estimate of the cost of the labor, materials, equipment and other services to be furnished by the Owner for the Adjustment, dated May 15, 2015 is attached hereto and made a part hereof as "EXHIBIT A". Any supplementary estimate which is subsequently approved by the Chief Engineer of MassDOT and by the Owner will be attached hereto and made a part hereof.

It is understood that rates and cost components used in the estimate are not intended for use as reimbursable billing rates or costs in lieu of actual costs supported by the Owner accounting records.

In order to determine reimbursable actual costs for the Owner's equipment, the Owner shall maintain accounting and usage records for each item of equipment in sufficient detail to develop hourly billing rates acceptable to MassDOT upon audit. As an alternative to maintaining such detailed records for the Owner's equipment, the Owner may request and receive reimbursement at hourly rates not in excess of rates determined by use of the Construction Equipment Ownership Expense Schedule, Region 1, U.S. Army Corps of Engineers (A.C.E. Schedule) in effect at the time of usage. Invoices based upon such A.C.E. rates shall be accompanied by descriptive detail on each equipment item sufficient to permit identification in the A.C.E. Schedule; plus appropriate page references to the A.C.E. Schedule and/or A.C.E. rate computations. For equipment items not identifiable in the A.C.E. Schedule, or in special circumstances, the Owner may request MassDOT pre-approval of rates estimated on a reasonable basis. This paragraph does not apply to items of equipment rented by the Owner from third parties.

Section 3. All reimbursable charges in connection with this Agreement will be subject to audit by representatives of MassDOT and/or the FHWA; and the Owner will retain all records and documents pertaining to the Agreement charges until such audit is completed or until written approval to destroy the records is given by MassDOT.

The Governor or his designee, the Secretary of Administration and Finance, and the State Auditor or his designee shall have the right at reasonable times and upon reasonable notice to examine the books, records and other compilations of data of the Owner which pertain to the performance of the provisions and requirements of this Agreement.

Section 4. There is no extension of service life in the Adjustment of the Owner's facilities in accordance with the conditions of said memorandum of the FHWA.

METHOD OF PAYMENT TO THE OWNER

Section 1. During the course of the project, the Owner may present monthly progress bills of the incurred costs for approval and payment by MassDOT.

Section 2. Upon completion of the physical portion of the Adjustment to the satisfaction of MassDOT and the Owner, written notification shall be given to the District Highway Director by the Owner that said work has been completed and, within 120 days thereof, the Owner will submit to MassDOT a final detailed bill as required by the FHWA, and final settlement will then be made between the MassDOT and the Owner.

FUTURE MAINTENANCE

Upon the completion of the Adjustment of the Owner's Facilities, the Owner shall thereafter maintain said Facilities as adjusted including the cost thereof.

MISCELLANEOUS

This Agreement shall not be considered fully executed until signed by the Department and nothing under **Division of Work**, shall commence until the effective start date of performance, **Notice to Proceed**, is issued by MassDOT.

Title to said Facilities as adjusted shall remain with the Owner.

Any permit as required shall be issued by MassDOT. The conditions of said permit providing for removal of the Owner's Facilities and also any conditions inconsistent with the provisions of this Agreement shall not apply thereto.

This agreement is set to expire on the date noted on the Standard Contract Form "Contract End Date". If an extension of time is needed beyond this date, a request is needed in writing with a minimum 60-day advance notice before the expiration date. The request will need to address the current expiration date, the proposed expiration date and any other information deemed necessary. An extension for this agreement after this date will not be granted and will be terminated. Any work done after this Contract End Date may not be reimbursable to the Owner.

BUY AMERICA REQUIREMENTS

In accordance with the BUY AMERICA requirements of the Federal regulations (23 U.S.C. 313 and 23 CFR 635.410) all manufacturing processes for steel and iron products or predominantly of steel or iron (at least 90% steel or iron content) furnished for permanent incorporation into the work on this project shall occur in the United States. The only exception to this requirement is the production of pig iron and the processing, pelletizing and reduction of iron ore, which may occur in another country. Other than these exceptions, all melting, rolling, extruding, machining, bending, grinding, drilling, coating, etc. must occur in the United States.

(a) Products of steel include, but are not limited to, such products as structural steel piles, reinforcing steel, structural plate, steel culverts, guardrail, steel supports for signs, signals and luminaires, and cable wire/strand. Products of iron include, but are not limited to, such products as cast iron frames and grates and ductile iron pipe. Coatings include, but are not limited to, the applications of epoxy, galvanizing and paint. The coating material is not limited to this clause, only the application process.

(b) A Certificate of Compliance shall be furnished for steel and iron products as part of the backup information with the billing. The form for this certification entitled "**Buy America Certificate of Compliance**" is attached to this agreement as "**Exhibit B.**" Records to be maintained by the RAILROAD/UTILITIES and the Department for this certification shall include a signed mill test report and/or a signed certification by a supplier, distributor, fabricator, or manufacturer that has handled the steel or iron product affirming that every process, including the application of a coating, performed on the steel or iron product has been carried out in the United States of America, except as allowed by this Section. The lack of these certifications will be justification for rejection of the steel and/or iron product or nonpayment of the work.

(c) The requirements of said law and regulations do not prevent the use of miscellaneous steel or iron components, subcomponents and hardware necessary to encase, assemble and construct the above products, manufactured products that are not predominantly steel or iron or a minimal use of foreign steel and iron materials if the cost of such materials used does not exceed one-tenth of one percent (0.1%) of the total contract price or \$2,500.00, whichever is greater.

Utility Completion Form (UF-1)

UTILITY COMPANY INPUT			
Contract No:	Contract Name:	Contract Location:	District:
Name of Utility:		Name of Utility Authorized Representative:	
Utility Transmittal No:	Date: <small>Click here to enter a date.</small>		
Name of Contractor:		Name of Contractor Authorized Rep:	
Force Account Agreement No.	Force Account Date:	Utility Phase <small>(if applicable):</small>	
Was the work for the utilities completed? <input type="checkbox"/> Yes <input type="checkbox"/> No			
*Attach all documentation (i.e. Daily Force Accounts and Chargeable Invoices)			
<small>Start Date: Click here to enter a date.</small>		<small>End Date: Click here to enter a date.</small>	
If yes, was the work completed within the current approved Utility Duration as referenced in the Force Account agreement? <input type="checkbox"/> Yes <input type="checkbox"/> No			
Has all the completed force account paperwork been attached? <input type="checkbox"/> Yes <input type="checkbox"/> No			
If yes, have you, the Utility, notified the MassDOT R.E. of completion date and forwarded confirmation, to the Contractor? <input type="checkbox"/> Yes <input type="checkbox"/> No			
_____ <i>Utility/Authorized Representative Signature</i>		_____ <i>Date</i>	
CONTRACTOR INPUT			
To the best of my (<i>Contractor Rep</i>) knowledge, I agree that the Utility has completed their (<i>Utility</i>) known scope, for this applicable phase of the Utility work, to allow the Contract Work to progress. <input type="checkbox"/> Yes <input type="checkbox"/> No			
<small>Note: this form does not serve as a proper notice of delay – see Contract requirements. If no, has MassDOT been formally notified, via letter, identifying what work is preventing the Contractor from progressing?</small>			
<small>Confirmed Start Date: Click here to enter a date.</small>		<small>Confirmed End Date: Click here to enter a date.</small>	
If there are no known issues with the Utility's declared completion of their work – Contractor to sign here. Otherwise, provide (here or attached) a summary of what is not complete and preventing work from progressing and forward to MassDOT and the Utility).			
_____ <i>Contractor/Authorized Representative Signature</i>		_____ <i>Date</i>	
<small>Send To: Name of MassDOT Auth. Representative:</small>		<small>Date sent to MassDOT:</small>	
<small>Choose Name</small>		<small>Click here to enter a date.</small>	
MassDOT DISTRICT CONFIRMATION			
Was the work for the utilities completed? <input type="checkbox"/> Yes <input type="checkbox"/> No			
<small>Start Date: Click here to enter a date.</small>		<small>End Date: Click here to enter a date.</small>	
If yes, was the work completed within the current approved Utility Schedule as referenced in the Force Account? <input type="checkbox"/> Yes <input type="checkbox"/> No			
In compliance with agreement duration? <input type="checkbox"/> Yes <input type="checkbox"/> No			
If Yes, Resident Engineer drafts memo to State Utility Engineer to initiate payment CC: Resident Engineer, District Utility Constructability Engineer, District Construction Engineer			
Confirming this is a <u>phase</u> payment? <input type="checkbox"/> Yes <input type="checkbox"/> No			
Confirming this is a <u>final</u> payment? <input type="checkbox"/> Yes <input type="checkbox"/> No			
CC: Resident Engineer, State Utility Engineer, District Utility Constructability Engineer, District Construction Engineer			
_____ <i>MassDOT/Authorized Representative Signature</i>		_____ <i>Date</i>	

REPORT DATE: **13-Feb-14**



DAILY WORK FORCE REPORT

UTILITY - Force Account:

NAME OF UTILITY CO. _____
 Force Account Agreement # _____
 Agreement with _____
 Agreement Date: _____
 Eligible for Reimbursement Yes No

CONTRACTOR - Time & Materials:

NAME OF PRIME CONTRACTOR CO. _____
 NAME OF SUB-CONTRACTOR _____

* Check one only

Note: * All Sub-Contractors work force shall be checked individually and separately from the Prime Contractor

DATE OF WORK PERFORMED: **13-Feb-14**

Project Description/Town City: _____ Project #: _____ Contract #: _____

Description of Work Performed (including Location / Area):

LABOR

NAME FIRST LAST	CLASSIFICATION	No.	TIME WORKED	HOURS		
				REG'R	OT	TOTAL
			7:30 AM TO 4:00 PM			0
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
		0		0	0	0

EQUIPMENT

DESCRIPTION / MODEL / MAKE / YEAR	NOTES	No.	TIME WORKED	HOURS		
				REG'R	STAND BY	TOTAL
			7:30 AM TO 4:00 PM	0	0	0
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
			TO			
		0		0	0	0

MATERIAL

DESCRIPTION	INVOICE #	\$ AMOUNT	SALVAGE / CREDIT DESCRIPTION	INV. #	QTY. / UNIT	\$ AMOUNT
						50
		50				50

RECEIVED _____
 TITLE _____
 MassDOT - Highway Division
 DATE: _____

SIGNED _____
 TITLE _____
 Utility Company Representative (or) Contractor
 DATE _____

Note: The signatory above certifies acknowledgment of labor, materials, and equipment used and that he/she is contractor as authorized by request for payment or claim.

UTILITY RELOCATION COVER SHEET



Date: 5/15/2015
MassDOT Project File No: 605657
Project Description: Rte. 109 Project

Utility : Medway Fire Alarm 44 Milford St
Design Submittal: Medway, MA 02053
Submitted By: Robert O'Neill Jeffrey Lynch 508-533-3213

Name: Robert O'Neill Tel. #: 508-922-4472
Email: boneill@medwayfd.com

Estimated Labor Cost: \$4,000
Estimated Material Cost: \$7,925.07
Estimated Equipment Cost (if applicable): \$1,220
Estimated Police Details / Traffic Protection Cost: \$3,590.40
Estimated SUB CONTRACTOR Cost (if applicable): \$8,650
Estimated Salvage Value (to be subtracted from total): 0
Estimated TOTAL Cost: \$25,385.47

- Temporary Relocations Included
 - Sketches Attached
 - Facility Betterment
- (If CHECKED, provide brief explanation)

If NO salvage value included, provide brief explanation: Current wire in copper-clad steel with little to no value.

Aerial Relocations:

Total Number of poles necessitating relocation (even if utility is NOT installing the pole): 85
Total amount of wire / cable for relocation (in feet): 12,000'

Brief Explanation of Scope of Work / Other Remarks:
Replace and relocate fire alarm wire to new poles in area of Rte 109 project. Entails replacement of wire, wire supports, hooks, 18 junction boxes, relocation of streetboxes, and tie-ins to several masterboxes and side roads.

Underground Relocations:

Total Number of pipe or conduit for relocation: 0
Total amount of manholes for RELOCATION (Adjustments not included): 0

Brief Explanation of Scope of Relocation Work / Other Remarks:

Exhibit A
1/3

MassDOT Utility Relocation Scheduling Spreadsheet

*Please complete this form for all work required for your utilities relocation. Each event shall be broken down and categorized for the work required. Each task shall have the required duration based on a normal 8 hour work day. These durations do not include holidays or weekends unless specifically noted. Each Phase shall be noted whether it is a Phase proposed by the utility or by MassDOT. It should also be noted if work is required by another party during any of the Phases which are outlined below. The estimated time durations submitted by the utility company shall be binding for reimbursement and shall be documented as to how the duration was determined. If MassDOT determines that a duration is excessive and can not be documented, MassDOT will reject the proposal and the project will become non-reimbursable.

An initial lead time (30 days) for the first utility to begin relocations will be granted. All other lead time will run concurrent with actual work being performed on site.

	Is there necessary construction work to be done by MassDOT contractor prior to utility relocations:	Yes	No	
MassDOT Construction Phase 1:	Description			Estimated duration by work days*
Utility Task 1	Replace approximately 12,000' of fire alarm wire, ATB's, alarm boxes, and associated hardware for 109 project.			3 Days
Utility Task 2				
Utility Task 3				
Utility Task 4				
Utility Task 5				
Utility Task 6				
Utility Task 7				
Utility Task 8				
MassDOT Construction Phase 2:	Total Phase 1			3 Days
Utility Task 1				
Utility Task 2				
Utility Task 3				
Utility Task 4				
Utility Task 5				
Utility Task 6				
Utility Task 7				
Utility Task 8				
MassDOT Construction Phase 3:	Total Phase 2			
Utility Task 1				
Utility Task 2				
Utility Task 3				
Utility Task 4				
Utility Task 5				
Utility Task 6				
Utility Task 7				
Utility Task 8				
	Total Phase 3			

The Contractor and/or MassDOT's designated contact shall contact the Utilities within a specified lead time to properly notify utilities of the start of scheduled relocation work. Such coordination of work between the Contractor and/or MassDOT's designated contact and the Utility shall be done in writing, including electronic mail.

MassDOT Project File No:605657

Project Description: Reconstruction of Rte 109

Breakdown of Unit Prices for Fire Alarm Wire Replacement

Labor:

FD Lineman:	40 hours @ \$50/Hr x2 per day	\$4,000.00
Sub-Cont Lineman	32 hours @ \$101/Hr	\$3,232.00
Sub-Cont Ground Man	32 hours @ \$82/Hr	\$2,624.00
Sub-Cont Reel Man	16 hours @ \$82/Hr	\$1,312.00
Sub-Cont Design & Layout	2 hours @ \$101/Hr	\$202.00
	Total Labor	\$11,370.00

Materials:

Fire Alarm Wire	12,000 feet of Rural C-Wire, IMSA 29-3	\$5,564.00
Junction Boxes	18 ATB's @ \$43.74 each	\$787.32
Pole Hardware	85 J-Hooks @ \$2.95 each	\$250.75
Wire Supports	85 Tangent Supports @ \$9.00 each	\$765.00
	36 Dead-End Supports @ 15.50 each	\$558.00
	Total Materials	\$7,925.07

Equipment:

FD Bucket Truck	32 hours @ \$38.13/Hr	\$1,220.00
Sub-Cont Bucket Truck	32 hours @ \$40/Hr	\$1,280.00
	Total Equipment	\$2,500.00

Police Details:

Police Officer	32 hours @ \$51/Hr x 2 (estimate)	\$3,264.00
	<u>Police Admin Fee 10% of detail</u>	<u>\$326.40</u>
	Total Police Detail	\$3,590.40

Project Total **\$25,385.47**

3/3

MASSACHUSETTS
DEPARTMENT OF TRANSPORTATION
BUY AMERICA
CERTIFICATE OF COMPLIANCE

Date _____, 20_____

WE,

(UTILITY/RAILROAD OWNER)

Address: _____

Hereby certify that we are in compliance with the "Buy America" requirements of the Federal regulations 23 U.S.C. 313 and 23 CFR 635.410 of this project.

As required, we will maintain all records and documents pertinent to the Buy America requirement, at the address given above, for not less than 3 years from the date of project completion and acceptance, if we do not provide the records and documents during invoicing. If all records and documents pertinent to the Buy America requirement are delivered during invoicing, then we will maintain all records and documents pertinent to the Buy America requirement for not less than three (3) years from the date conditional final payment has been received by the COMPANY. These files will be available for inspection and verification by the Department and/or FHWA.

We further certify that the total value of foreign steel as described in the Buy America requirements for this project does not exceed one-tenth of one percent (0.1%) of the total contract price or \$2,500.00, whichever is greater.

By: _____

Name: _____

Title: _____

Subscribed and sworn to before me this _____ day of _____ 20_____

My Commission Expires: _____

Notary Public/Justice of the Peace

EXHIBIT B

AGENDA

ITEM #6

Presentation – New Design Review Guidelines

Associated back up materials attached:

- Memo from Design Review Committee
- Guidelines



Town of Medway
DESIGN REVIEW COMMITTEE
155 Village Street
Medway MA 02053
508-533-3291
drc@townofmedway.org

December 15, 2015

TO: Medway Board of Selectmen
RE: Update of Medway Design Review Guidelines from

Thank you for making time available during the December 21st Selectmen's meeting for the Design Review Committee to present the updated *Medway Design Review Guidelines* [DRG]. The DRC and PEDB are very pleased to have had the opportunity to review and update this important document. The result of this effort is a highly professional text that provides clear and coherent guidance for the purpose of a more streamlined and effective development permitting application process.

During our meeting, PEDB member Tom Gay and I would like to present the following:

- A brief summary of the timeline and process that produced these new *Guidelines*.
- The layout, function and usefulness of this new document in contrast to the prior version.
- Plans to roll-out the new *Guidelines* to fellow Town boards/committees, Town staff, and private organizations that will benefit from this improved document.

We will provide bound copies of the DRG and a handout that correlates to a short PowerPoint presentation. We look forward to the input of the Selectmen on any of these matters.

Sincerely,

Matthew Buckley
DRC Chairman

Medway, Massachusetts

*Adopted on September 15, 2015 by the
Planning and Economic Development Board*



TOWN OF MEDWAY

Design Review Guidelines

AUGUST 2015

TOWN OF MEDWAY, MASSACHUSETTS
DESIGN REVIEW GUIDELINES



PLANNING AND ECONOMIC DEVELOPMENT BOARD (PEDB) MEMBERS

Andy Rodenhiser
Robert Tucker
Thomas Gay
Matthew Hayes
Richard Di Iulio

DESIGN REVIEW COMMITTEE (DRC) MEMBERS

Matthew Buckley
Julie Fallon
Thomas Gay
Lisa Graves
Rachel Walsh
Mary Weafer

DESIGN REVIEW GUIDELINES TASK FORCE

Susan Affleck-Childs
Matthew Buckley
Thomas Gay
Matthew Hayes
Dan Hooper
Stephanie Mercandetti
Mary Weafer

PLANNING AND DESIGN CONSULTANT

The Cecil Group



TOWN of MEDWAY
Board of Selectmen, Planning and Economic Development Board, Design
Review Committee
155 Village Street • Medway, MA 02053
508-533-3200

October 1, 2015

The Town of Medway is pleased to present these newly updated *Medway Design Review Guidelines* as adopted by the Planning and Economic Development Board on September 15, 2015. The Town recognizes the importance of well-designed sites, buildings and signage and how such can reinforce a sense of New England village character. The *Guidelines* reflect this recognition and are an implementation tool that is consistent with the *Medway Master Plan*. Design guidelines enhance the overall quality of the environment of a community, protect and enhance property values, and bring a community-based design review process to bear on incremental development.

The purpose of the *Design Review Guidelines* is to establish the minimum requirements and expectations for the quality of design for development and investment in Medway. The *Guidelines* are a useful and informative document to assist land developers and property owners and their architects and site engineers to prepare site design and building elevation plans that are consistent with the community's vision. Adherence to the *Guidelines* helps applicants to achieve land use permitting approval and accelerates the review process.

The new *Design Review Guidelines* are a substantive revision to Town's first *Design Review Guidelines* adopted in 2007. They are clearer, beautifully illustrated, and easier to use. The Medway Design Review Committee (DRC) will use the *Guidelines* to review proposed development projects that are referred to it by the Planning and Economic Development Board as part of its review process for site plans and special permits and by other Town departments/agencies who seek the DRC's advice. The *Medway Zoning Bylaw* also charges the DRC with the responsibility to review proposed signs for compliance with the *Guidelines*.

The development of the new *Guidelines* was undertaken by a Task Force comprised of a citizen member, representatives of the Planning and Economic Development Board and the Design Review Committee, and Town staff. The team was capably guided by The Cecil Group, a Boston based architecture/landscape architecture/urban design consulting firm. We are grateful to The Cecil Group for their expertise and knowledge and their fine work in Medway. Throughout the process, public input was sought and provided, in particular from the Medway Business Council which we appreciate.

You are encouraged to contact Stephanie Mercandetti, Director of Community and Economic Development, at 508-321-4918 to discuss property development options and to gain a better understanding of permitting in Medway. Any questions regarding the *Design Review Guidelines* should be directed to the Planning and Economic Development office at 508-533-3291.

Best regards,

John A. Foresto, Chairman
Board of Selectmen

Andy Rodenhiser, Chairman
Planning & Economic Development Board

Matthew J. Buckley, Chairman
Design Review Committee

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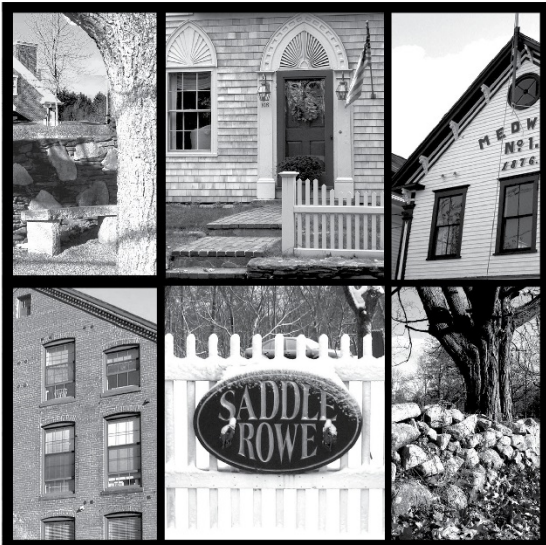
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Medway, Massachusetts, August 2015



Section 1. Introduction

**TOWN OF MEDWAY
Design Review Guidelines**

SECTION 1. INTRODUCTION

A. Purpose and Overview

The Town of Medway recognizes the importance of well-designed sites, buildings and signage to reinforce a sense of a rural New England village character. The *Design Review Guidelines* reflect this recognition and are an implementation tool that is consistent with the *Medway Master Plan*. Design guidelines enhance the overall quality of the environment of the community, protect and enhance property value and bring a community-based design review process to incremental development. The Medway General Bylaws – Section 2.15 established the Design Review process and Design Review Committee (DRC) in 2003. The Town has also adopted *Sign Regulations* which are part of the *Zoning Bylaw*. The regulations require DRC review of any free-standing sign and any individual sign with six square feet or more of sign surface area before a sign permit is issued by the Town.

Any development that requires Site Plan or Special Permit Approval administered by the Planning and Economic Development Board is required to follow these *Design Review Guidelines* and is subject to recommendations of the DRC.

The purpose of the *Design Review Guidelines* is to establish the minimum requirements and expectations for the quality of design for development and investment in the Town of Medway. Applicants are encouraged, but not required, to achieve beyond the scope of these *Guidelines* in each topic area outlined. Adherence to the *Design Review Guidelines* helps applicants to achieve approval and accelerates the review process.

The *Design Review Guidelines* establish a framework for review by the Planning and Economic Development Board. The DRC is also responsible for signage review pursuant to the *Zoning Bylaw*. DRC members are appointed by the Planning and Economic Development Board. The committee is comprised of Medway residents with expertise in a design discipline who volunteer their time for the benefit of the Town.

The DRC provides advisory review of development design using the *Design Review Guidelines* to prepare the review. The DRC endeavors to serve the people of Medway in a capacity that openly, creatively and appropriately addresses issues of land, site, architectural and sign design. The DRC works within the broad intention of maintaining and/or improving the quality of life of Town citizens, the value of property and

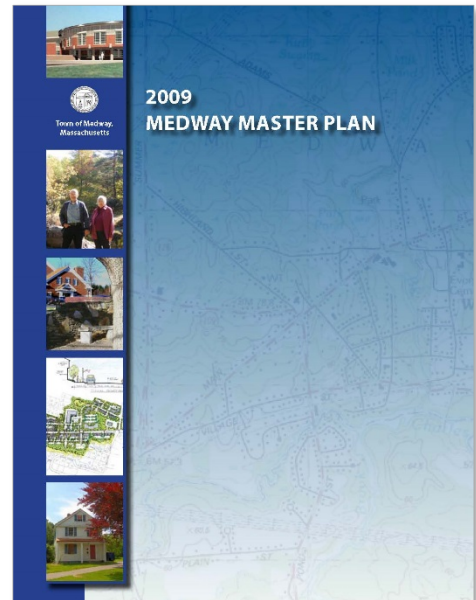


Figure 1: Medway Master Plan

the viability of commerce through the use of thoughtful and community-appropriate design and development practices. The *Design Review Guidelines* and review process encompass a range of topics across the multiple disciplines and perspectives required for the meaningful, purposeful, and aesthetically pleasing composition of places. These topics include architecture, landscape architecture, graphic design, site design and community planning.

B. New England Village Character

The *Medway Master Plan* established a broadly shared community goal of maintaining Medway's *traditional New England village* character. This goal is a foundational premise for the *Design Review Guidelines* and underpins many of the components of the *Design Review Guidelines*. The following characteristics provide definition for what this important term means and offers examples of its application. For each part of the *Design Review Guidelines* (Commercial, Industrial or Residential) more specific terms and examples are provided to further define "New England village" character in each context pertaining to site, building and materials. Inspiration for Medway's New England village character should be drawn from Medway's two historic districts which are listed on the National Register of Historic Places, Rabbit Hill and Medway Village, as well as the heritage of historic mill structures in the Town and the scenic quality of Medway's Scenic Roadways.

One of the great attractions of historic development patterns is that they are well-proportioned as a human-scaled environment. Human-scaled is a fundamental component to the New England Village Character. Human-scale refers to a size, texture, and articulation of physical elements that match the size and proportions of humans and, equally important, correspond to the speed at which humans walk. Building details, pavement texture, street trees, and street furniture are all physical elements that contribute to human-scale. Human-scaled designs may include a building design that is made of materials that relate directly to the scale of the human hand, such as a brick, or include details that relate directly to the size of the human body such as steps, doorways, railings and windows.



Figure 2: Historic image of Medway Village and its New England village character at Sanford Street looking north

2. Building Character

The composition of buildings that reflect New England village character are varied and eclectic, but several distinct characteristics of New England buildings are identifiable:

- A classic village character is one that has evolved incrementally over time. The composition of places and buildings should reflect the additive nature of building a village gradually over time through a variety of architectural features and rooflines. For example, building forms may reflect this through telescoping additions, dormers, or multiple rooflines.
- Agricultural and early industrial architecture is characterized by a simplicity and elegance of form, in which form follows function, and the building's purpose and structure are evident. This type of simplicity has allowed these building types to be adaptable for reuse.
- Medway's architectural character is varied in terms of architectural style. Historically, styles in the community have included Federalist, Colonial, Shingle, Georgian, and Greek revival. All new construction should respect this historical context of style. However, a single project should not attempt to incorporate or reflect more than a single style.

3. Material Character

Traditional building and landscape materials contribute significantly to the sense of New England village character. The following materials reflect that character.

- Informal cottage-style landscape, with native and indigenous plantings, loose groupings, and a casual appearance is reflected in New England landscapes with tree lines, picket fences, and stone walls demarcating property lines and the edges of clearings.
- Traditional New England building materials may include wood, brick, granite, and slate with the materials reflecting a sense of permanence and durability.

C. Organization of Guidelines

The *Design Review Guidelines* are organized by the three principal land use types (Commercial, Residential and Industrial) and the zoning districts in which these uses would be located. This organization includes *Part II – Commercial Zones*, *Part III – Residential Zones*, and *Part IV – Industrial Zones*. For example, an industrial development, located in the Industrial I (IND-I) zoning district would be subject to the design guidelines contained in *Part IV – Industrial Zones*. Each major part of the *Design Review Guidelines* is organized with a parallel structure and hierarchy which includes guidelines for site improvements, building and signage.

D. Administration

1. Pre-Application for Development

Applicants are encouraged to meet with the DRC prior to submitting a development application to the Planning and Economic Development Board and its formal review. During the pre-application phase, both site and building designs can be discussed as they relate to the *Design Review Guidelines*. The objective is to provide an open discussion to form a mutual understanding of both the development opportunities and challenges posed by the particular site and development program. This type of dialogue can enhance the efficiency of the approval process and outline design directions that are mutually beneficial to the Town and applicant. At any point in the process, the Medway Department of Community and Economic Development is available for permitting guidance and assistance.

2. Application for Development

The DRC development review is initiated by the Planning and Economic Development Board upon receipt of a Site Plan or Special Permit Application and referral to the DRC. The DRC signage review occurs prior to the filing of a sign permit application. The DRC will provide an initial review, according to a *Design Review Committee Checklist*. This review determines the areas of focus for discussion between the applicant and DRC. The DRC and applicant then meet to discuss the development and its compliance with the *Design Review Guidelines*. The DRC presents final written recommendations to the Planning and Economic Development Board or Building Department.

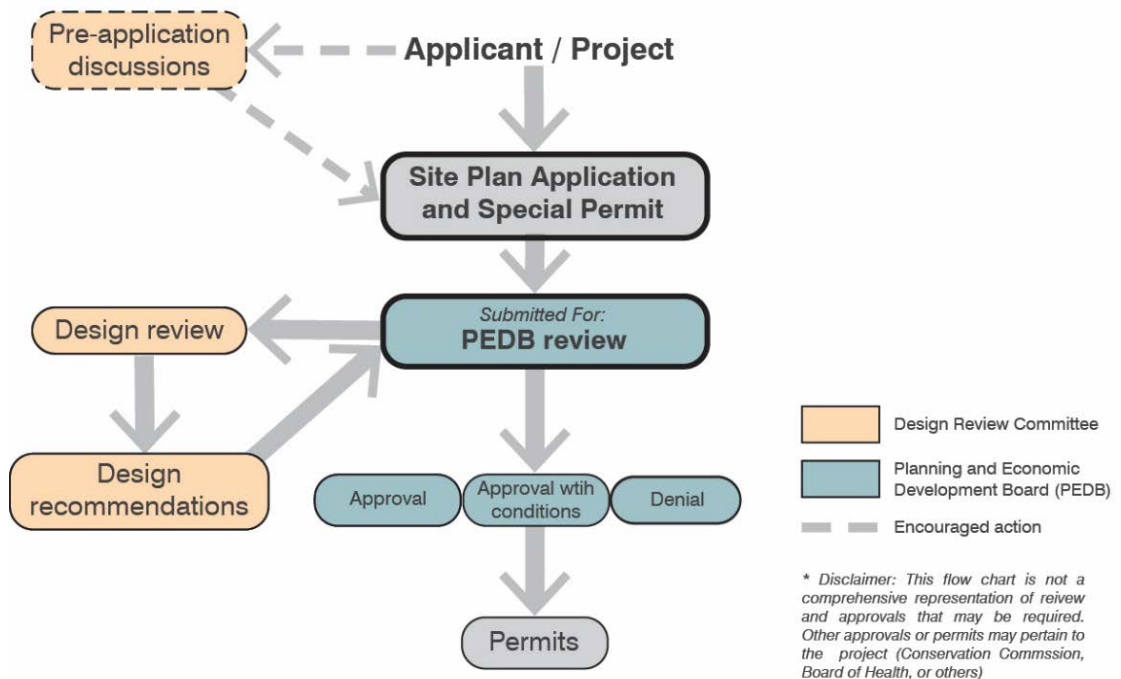


Figure 4: Design Review process diagram

3. Application for Signage

As a pre-requisite to submitting a sign permit application to the Building Department, the Medway Zoning Bylaw requires a business or property owner to meet with the Design Review Committee to review any proposed free-standing sign and or any individual sign that has six square feet or more of sign surface area. The DRC also available for pre-application meetings and is able to assist in creating a well-designed sign appropriate to the building, site, and surrounding environment. The DRC presents final written recommendations to the Inspector of Buildings and then the applicant may submit a Building Permit or other applicable Permit Applications.

E. Compliance Alternatives

It is difficult for *Design Review Guidelines* to predict all possible development or design scenarios or anticipate new trends, technologies or best practices. If specific *Guidelines* will not be followed by an applicant because they feel a better approach to reinforcing the Town's goals and principles exists for their individual development and design circumstances, a compliance alternative may be reached as an agreed upon method to comply with the intention of the *Guidelines*. This option provides a process to arrive at innovative design solutions that all parties agree will follow the intent of the *Guidelines*. The applicant must specifically identify the areas in which they seek a compliance alternative with a *Compliance Alternative Request Form*, and the DRC provides an opinion and recommendation regarding the alternative approach in writing to the Planning and Economic Development Board. Please contact the Medway Planning and Economic Development office for further information.

F. Glossary of Terms

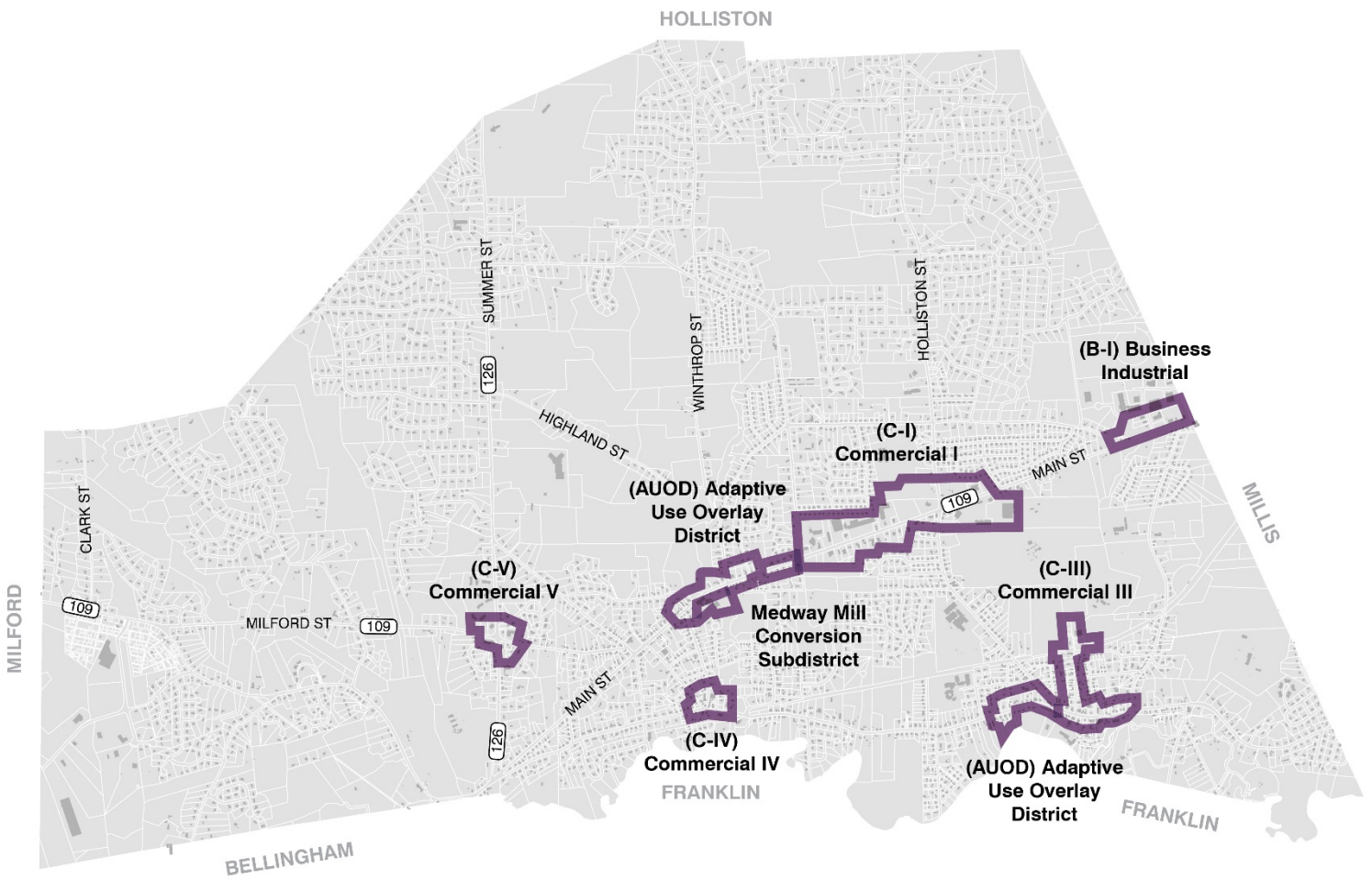
All terms or phrases used in these *Guidelines* that are technical in nature or may be part of a certain design discipline's terminology are defined in Part 5 – Glossary of Terms. Please refer to the Glossary of Terms if a certain *Guideline* or recommendation is unclear.

Medway, Massachusetts, August 2015



Section 2. Commercial Zones

**TOWN OF MEDWAY
Design Review Guidelines**



Disclaimer: For Official Zoning Map district boundaries and most current information refer to the Zoning Map in Section 6. *References and Resources*

Commercial Zones

TOWN OF MEDWAY Design Review Guidelines

SECTION 2. COMMERCIAL ZONES

A. Applicability

The *Design Review Guidelines* for commercial zones are applicable to developments located within the Commercial I (C-I), Commercial III (C-III), Commercial IV (C-IV), Commercial V (C-V), Business/Industrial (BI), and the Adaptive Use Overlay District (AUOD), and its Medway Mill Conversion Subdistrict. The Design Review Guidelines are also intended to guide Municipal building projects.

Generally, the commercial zones represent two contexts for development, smaller-scale village commercial districts and larger-scale automobile-oriented commercial corridor. These two contexts generally follow zone boundaries with the C-III, C-IV, C-V and AUOD districts fostering smaller-scale, traditional, village-type development. The C-I district context is a more automobile-dependent commercial corridor character. The *Design Review Guidelines* are applicable in both contexts to encourage a New England village character and pedestrian-oriented environment.

B. Principles and Intentions

The *Design Review Guidelines* for the commercial zones are intended to shape commercial development to reinforce the traditional New England village character of Medway.

For commercial zones, the following design principles encourage a human-scaled, well-designed, New England village environment:

- An overall development character that reflects the heritage of the Town of Medway and highlights features of the development or site that may connect the current design to the Town's history
- Building massing and site composition with a configuration and appearance that suggest evolution or modification over time through organic and incremental growth
- Building scale and site composition that reinforces the human scale of the built environment with buildings that are relatable to the pedestrian and a walkable site design with inviting streetscapes or small public spaces for socializing and gathering
- Building architecture that is varied and eclectic in style that echoes traditional New England building character with traditional details, materials and colors



Figure 6: Historic commercial uses in Medway at Sanford Hall

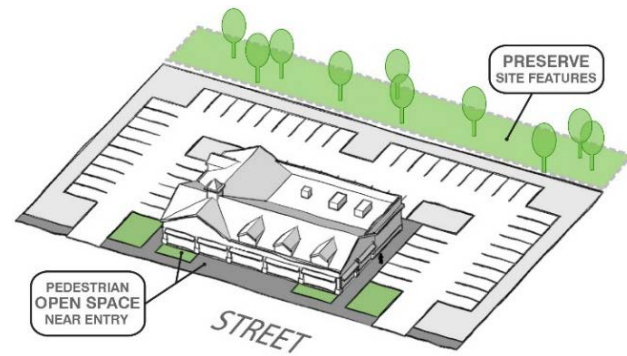
C. Site Improvement Guidelines

The following guidelines outline the site design and layout practices that should be viewed as baseline components for a well-designed commercial development in the Town of Medway.

1. Site Composition

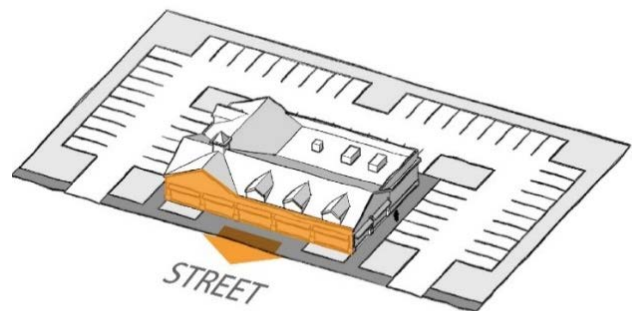
The development's land planning and site design should provide a thoughtful and responsive approach that adapts the development program and site requirements to the conditions of the land.

- a) **Reinforce New England Village Environment** – Site design should be used to reinforce the sense of a New England village environment by clustering buildings and site features. Purposeful composition of buildings and site features encourages a sense of community and frames open spaces. It also provides opportunities to maintain existing scenic views and to reinforce deliberate clearings for a variety of vistas and views.
- b) **Integrate Natural Site Features** – The site design should take advantage of the natural site features by maintaining, incorporating or adapting the inherent characteristics of the property (topography, landscape features and vegetation, rock formations, stone walls, etc.) to guide and benefit the layout and design of the site.
- c) **Cluster Components** – Site and building components should be clustered to maintain the maximum amount of natural and undisturbed open space on the property.
- d) **Reduce Impact of Parking** – Site layout should be designed to minimize the visibility and impact of parking, service and utility-oriented functions of the property.



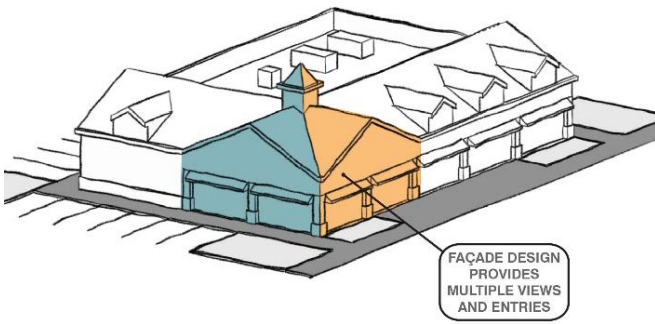
SITE FEATURES AND OPEN SPACE

Figure 7: Site features and site composition



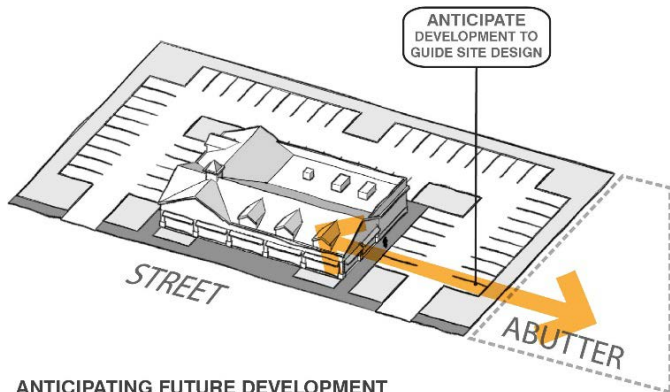
STREET-FACING ORIENTATION

Figure 8: Building orientation to frame streets and open spaces



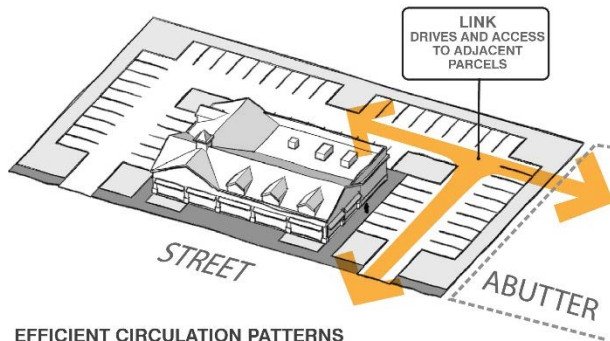
MULTIPLE PRIMARY FAÇADES

Figure 9: Buildings with multiple frontages should be designed with multiple primary façades and entries



ANTICIPATING FUTURE DEVELOPMENT

Figure 10: Anticipate future development in the abutting properties



EFFICIENT CIRCULATION PATTERNS

Figure 11: Site access efficiency between parcels

2. Building Orientation

Buildings should be sited to organize development on the land, reinforce a sense of community, frame open space, and conceal parking, service and loading areas.

- a) **Orient Building to the Street** – Primary building facades should be oriented to public street frontages and/or open spaces.
- b) **Position Entry to the Street** – The front facades and primary pedestrian entries to a building should face the public street that provides primary access to the property.
- c) **Respect Patterns of Context** – Building setbacks should be consistent with the zoning requirements and consider the pattern of buildings on adjacent parcels. A setback distance similar to neighboring buildings reinforces a rhythm and pattern of the district. Front setback distances should be minimized to encourage a relationship between the building and the primary street frontage.
- d) **Articulate Multiple Primary Façades** – For buildings with multiple frontage orientations, design for all views and façades should be considered with multiple primary façades and building entries.
- e) **Anticipate Future Improvement** – Building design and orientation should anticipate that abutting vacant land may be a future development opportunity. Thoughtful attention should be paid to anticipating potential future development that could change the context of a building façade to enhance adjacent relationships and avoid awkward building orientations.

3. Site Access

Site access should provide clear and legible routes for all modes of transportation (pedestrians, bicycles, vehicles and public transportation) to connect to the site and to enter internal site circulation systems.

- a) **Minimize Site Access** – The number and width of vehicular access points into and out of the site should be minimized. Pedestrian crossings should be marked and differentiated with variations in

paving materials (for example by using stamped concrete or asphalt). Refer to the Medway Department of Public Services for additional requirements as part of the Street Opening Permit process.

- b) **Connect to Public Frontages** – Inviting and efficient sidewalks should be provided along any and all street frontages at the site perimeter. Additionally, sidewalk paths should be provided linking public frontage street(s) to all building entries.
- c) **Connect to Adjoining Properties** – To encourage pedestrian access between properties pedestrian pathways should be provided between buildings on adjacent parcels.

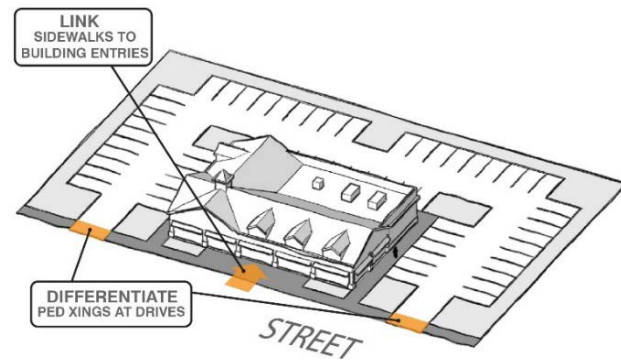
4. Internal Site Circulation

Circulation internal to the site should provide clear and legible routes for all modes of transportation to connect to the public way, building entries and other site components.

- a) **Promote Pedestrian Circulation** – Internal site vehicular circulation routes should have narrow travel lanes and small turning radii to reduce vehicular traveling speeds and reinforce a safe and welcoming pedestrian environment. At pedestrian crossings and intersections a further reduction of the travel lane width enhances the pedestrian environment and shortens crossing distance. This is referred to as a curb extension or neckdown at the intersection.
- b) **Define Building Entry Landscape** – Landscape at the building entry should be designed to provide a buffer between the building entry and the roadway. The landscape should be used as a transition from a pedestrian entry area to the roadway to enhance safety.
- c) **Create Efficient Site and Parking Circulation** – Adjoining parking areas should share access drives whenever possible. A well-organized system of drives should be used to shorten pedestrian crossing areas, reduce the amount of paved area, limit gaps

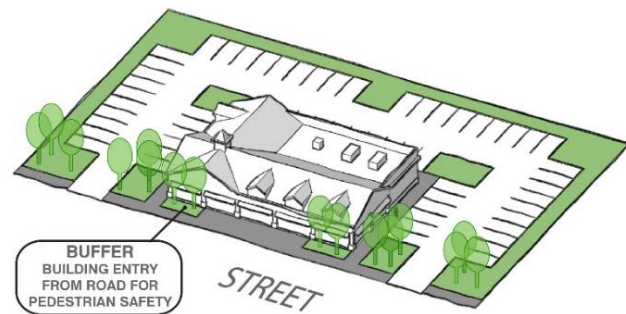


Figure 12: Connecting pedestrian path from building to public frontage



SAFE AND INVITING SIDEWALKS

Figure 13: Safe and inviting pedestrian circulation connecting street, site and building features



BUILDING ENTRY LANDSCAPE

Figure 14: Design landscape to provide a buffer between the road and the building entry



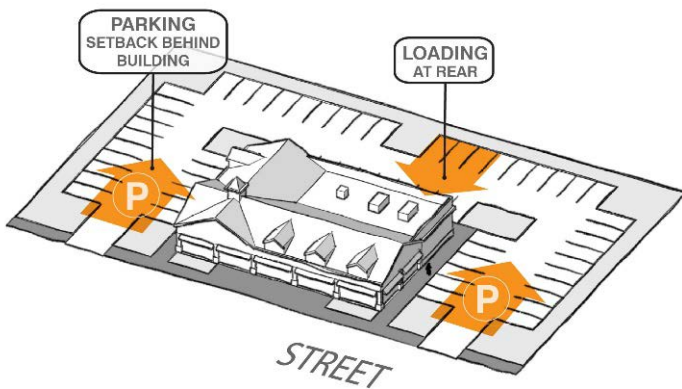
Figure 15: Bicycle circulation and place to lock bikes near the building entry

- d) **Reinforce Existing Circulation Connections** – Existing footpaths or old cartpaths on the site should be accommodated and integrated into the pedestrian site circulation to provide access across or through the site. Site circulation that contributes to connectivity of existing trail systems (Medway Link Trail or others) should be integrated into the site access and circulation.
- e) **Integrate Bicycle Circulation and Connections** – Access and circulation for bicycles on site should be considered for safety and amenity with provision for places to lock bicycles near building entries.
- f) **Conceal Loading and Service Circulation** – Loading and service areas should be located at the side or rear of buildings, and away from view of public streets. For larger buildings or where heavier loading/truck traffic is anticipated, loading routes should be separated from the regular travel routes used by customer and employee vehicles.

5. Parking

Parking should be placed convenient to the building entries, but not at the expense of the pedestrian safety, attractiveness, and aesthetics of the property. Parking is necessary to support the function and economic vitality of a development, but it should not be viewed as utilitarian only. Parking should be integrated with other site amenities that support a sense of place and community. For specific parking requirements refer to the Parking Regulations of the *Zoning Bylaw*.

- a) **Minimize Parking Location and Orientation** – Parking should be located to the rear and side of buildings with respect to the front or any side street. Where a parcel is located at the corner of two streets, parking should be located at the rear or at the internal side (not the street side). Where located to the side of buildings, parking areas should be set back from the street by at least the same distance as the building. Parking should never be placed within the front yard zoning setback.



PARKING AND LOADING

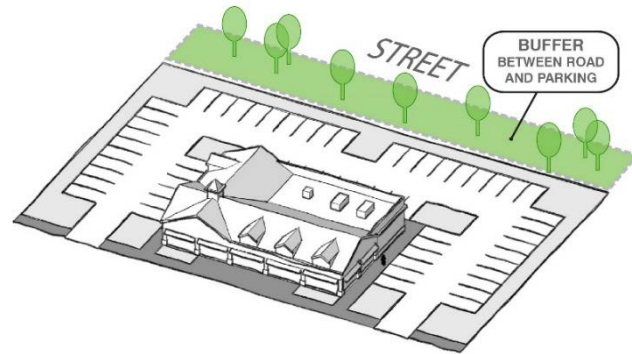
Figure 16: Parking, service and loading site orientation

- b) **Reinforce Parking Screening** – Parking areas are encouraged to be at the side or rear of buildings to reinforce a village feel and pedestrian environment. Where parking areas are unavoidable at the fronts of buildings, relative to the street, they should be screened with low landscaped berms, landscape beds, and/or low fences or stone walls; and softened with additional planting internal to the parking area.
- c) **Integrate Parking Landscape** – Large parking areas should be broken into smaller areas by means of landscaped islands containing low plantings and trees. Such islands should be placed at regular intervals across the parking lot to reduce the visual impact of the parking area and to reinforce a more pleasant pedestrian environment. Landscape islands should also be integrated with pedestrian circulation and crossing routes through parking areas.

6. Landscape

Parcels should include a hierarchy of landscape that contributes to the overall site design and integrates with adjacent properties. The hierarchy should include entry and gateway landscape, building and building entry landscape, street landscape, site feature landscape and landscape buffers. Refer to the required list of species in the *Site Plan Rules and Regulations* that are drought tolerant, native to New England and non-invasive.

- a) **Define Entry and Gateway Landscape** – Entry and gateway landscape should be used to define site access and reinforce a sense of arrival and layout of circulation on the site.
- b) **Integrate General Site Landscape** – The layout of primary or secondary vehicular or pedestrian circulation should be reinforced with a consistent landscape treatment that contributes to site wayfinding. All portions of a site will not or should not be landscaped, but the landscape should be used to reinforce the character, circulation and features of the site.

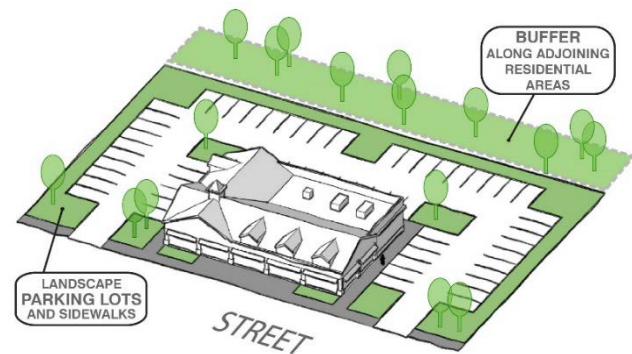


PARKING LANDSCAPING BUFFER

Figure 17: Landscape screening between parking and the street



Figure 18: Landscape integrated into a parking area



LANDSCAPING

Figure 19: Landscape to enhance, integrate and buffer site components



Figure 20: Landscape used to integrate site and building features



Figure 21: Landscape buffer between sidewalk and parking



Figure 22: Landscape frontage integrated with surrounding streetscape

- c) **Coordinate Scale of Landscape** – Selection of plantings and maturity of plantings should be carefully considered relative to the overall scale of development. The scale of the installed landscape should be directly tied to the overall scale of the development and buildings. New plantings should be selected for reasonable maturity at the time of installation to achieve the intended buffering and effect immediately.
- d) **Define Building Landscape** – Building landscape should be used to integrate the buildings into the overall site plan, soften building edges, and enhance public sidewalks, building entries, and plaza areas. Foundation plantings, planter beds, window planters, and sidewalk street trees and shrubs are all appropriate for this purpose. Landscape may be used to mitigate or screen less desirable components or features of a building façade.
- e) **Highlight Feature Landscape** – The landscape should be used to reinforce the importance of locations that are significant in the overall site design or near natural site features or amenities. Incorporating rock outcroppings found on site into landscape treatments is one example of a landscape feature. An additional number of plantings, unique composition or variation in planting species, or plant species with special seasonal variation should be used to reinforce such site features.
- f) **Provide Landscape Buffers** – Landscape buffers and fencing consistent with architecture and other site features should be used to conceal dumpsters, recycling areas, staging areas, utilities and other outdoor equipment or service uses from pedestrian views.
- g) **Reinforce Landscape Buffers at Property Lines** – Where a commercially-zoned property abuts a residentially-zoned or used property, a variety of landscape buffering elements and screening fencing should be provided along the adjoining yard(s). Landscape buffering should be effective four-

seasons and of lushly-planted vegetation averaging four to five feet tall.

- h) **Coordinate Landscape at Street Frontages** – Landscape for the site frontages on public ways should contribute to the character of the street and reinforce a consistent street frontage that is integrated with the character, type and spacing of adjacent landscape improvements.
- i) **Integrate Functional Features and Materials** – The materials used for functional features, such as retaining walls, drainage structures or other required site elements, should be integrated with the overall site design and material palette. For example, a functional retaining wall should include stone facing to match stone walls on the site.
- j) **Integrate Functional Features into Landscape** – Stormwater retention areas should be integrated with the site landscape and treated as a naturalized environment and site feature that is sustainable from a plant material and maintenance perspective. Retaining walls, fencing, guardrails and other utilitarian or screening features should be integrated with the overall landscape design and designed to contribute to the overall site character. Functional site features should be designed and considered for views of them from adjacent properties.



Figure 24: Landscape at street frontages contribute to the character of the street



Figure 23: Integrated and screened utilities

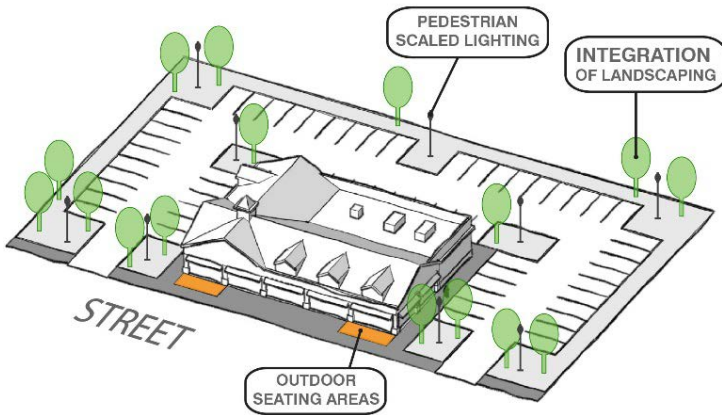
7. Site Amenities

Site amenities should enhance activity and serve a function near site and building entries and serve to enhance the pedestrian experience. Site amenities should include benches, trash and recycling receptacles, bike racks, and other components appropriate to the use and scale of the development.

- a) **Coordinate Location of Amenities** – The amenities should be located in high activity areas that are most likely to receive use. For example, places to sit should be provided where people are waiting or congregating as part of the use of the building and site.



Figure 25: Retention wall integrated into landscaped



SITE AMENITIES

Figure 26: Site amenities are designed at a pedestrian scale to maximize use



Figure 27: Character and design of street furniture and lighting consistent with the site and building design



placed to allow for clear walking and bicycling paths

- b) **Provide Open Space Amenities** – As part of commercial sites over 1 acre, a small but well-designed and inviting open space should be provided, of minimum size 300 square feet. The open space should be located in a prominent location adjacent to the building, and near a primary building entry that will bring pedestrian activity to the space. The open space should include outdoor seating, pedestrian-scaled lighting, and landscape, including both sunny and shady areas. Outdoor seating areas are encouraged.
- c) **Coordinate Design of Amenities** – The character and design of the site amenities selected should be consistent with the overall character of the site and building design.
- d) **Integrate Amenities** – Site amenities should be integrated with the site design to allow appropriate clearances, space and circulation around them to allow busy areas to function appropriately.

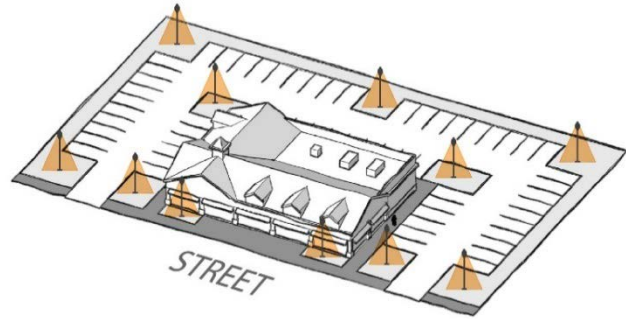
8. Site Lighting

Site lighting is intended to provide for pedestrian safety in areas with evening activity, particularly near site and building entries and across parking lots, and to provide a minimum level of lighting for nighttime safety. Lighting design must comply with the lighting requirements of the *Zoning Bylaw*.

- a) **Minimize Lighting** – Site lighting should comply with minimum lighting requirements and standards, but not provide lighting in excess of requirements. Downward-directed, dark-sky compliant lighting is required as per the lighting requirements of the *Zoning Bylaw* to minimize excess glare and spillage.
- b) **Create Multiple Layers of Site Lighting** – Site lighting should perform multiple functions on multiple areas on the site for multiple users. A site lighting approach should be designed for vehicles, pedestrians, building entry areas and site features. Each of these multiple areas should be designed in coordination and to complement the overall character of the site. Lighting should be used to highlight key areas and

attractive features of the site design. Lighting heights and poles should be scaled appropriate to the use, pedestrian height lighting and light bollards should be used when not lighting a vehicular area. Light fixtures of varying height should be of a compatible design and cohesive lighting fixture palette.

- c) **Integrate Lighting Fixtures with Design** – Lighting fixtures should be selected to contribute to the overall character of the building and site, consistent with the overall design and sense of place.



PEDESTRIAN-SCALED LIGHTING

Figure 29: Multiple layers of site lighting designed at a pedestrian scale for multiple users

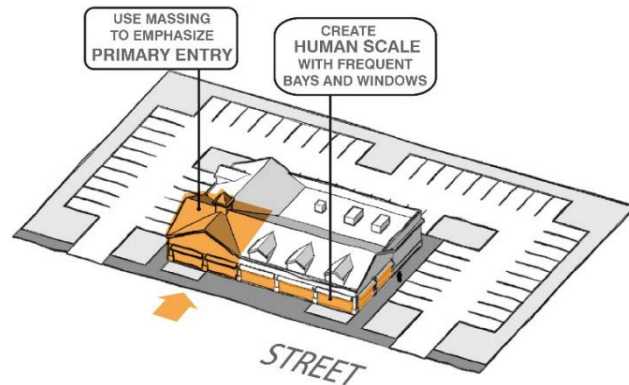
D. Architectural Guidelines

The following guidelines outline the architectural design elements that should be viewed as a baseline for well-designed architecture in the Town of Medway.

1. Building Massing

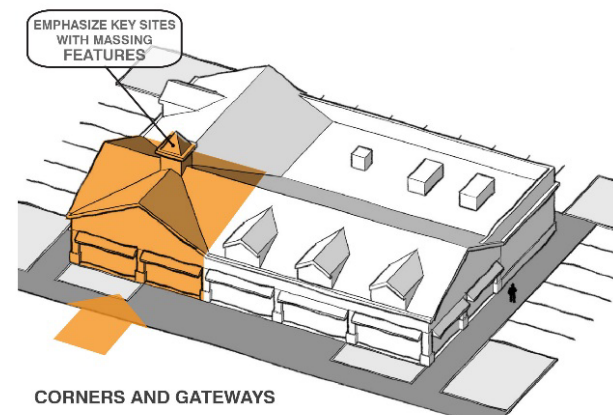
Building massing should be designed to reduce the overall perceived scale and provide simple and evocative forms that reinforce both a sense of a New England village and a sense of a human-scaled environment.

- a) **Strengthen Prominence of Building Entry** – Building massing should reinforce the purpose and readability of the building. For example, building massing should emphasize and highlight the location of the primary building entrance.
- b) **Visually Reduce Larger Building Scale** – Large building masses should be broken down through variations in roof lines, bays, setbacks, upper-level stepbacks, horizontal or vertical articulation, or other types of architectural detailing as described in Façade Composition and Components. Overall building form should be appropriate to the scale of the building and not become overly complicated.
- c) **Simplify Smaller Buildings** – Smaller building masses should remain simple and not overly complicated.
- d) **Reinforce Corners and Gateways** – Sites located at a prominent corner, intersection, or gateway should have building features and orientation that recognize the corner or gateway and respond to it with a suitable building form. Examples of prominent



BUILDING MASSING

Figure 30: Building massing to reinforce human-scaled environments



CORNERS AND GATEWAYS

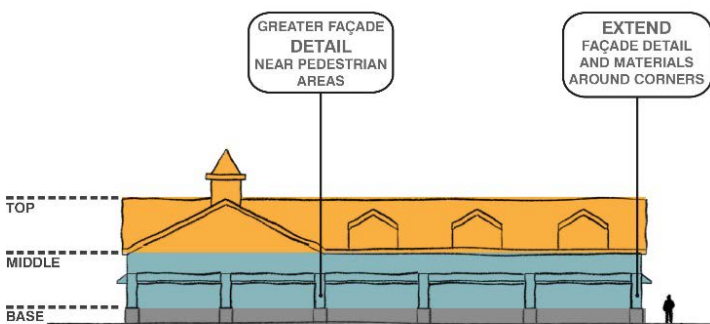
massing features that emphasize corners and gateways

building features include tower or cupola elements, corner detailing, additional building height, or other building forms that provide a visual anchor.

- e) **Integrate Historic Structures** – Existing historic structures should be integrated into any new development plan. New buildings and additions should complement and reflect the style of existing older structures. Historic buildings should be considered for restoration, sensitive rehabilitation, preservation or adaptive reuse as may be appropriate to the historic structure and nature of its reuse. Refer to the *Secretary of the Interior’s Standards for Rehabilitation*.
- f) **Integrate Accessibility Features** – Accessibility ramps, lifts or other access requirements should be integrated into the design of the building entry at the building exterior and interior. Accessibility components should be a purposeful part of the building entry design.

2. Façade Composition and Components

Composition of building façades should include architectural features and building components that reduce the scale of large building masses, reinforce the character of the building to reflect a New England village style, and provide detail and articulation of the overall building, particularly in areas with pedestrian traffic.



FAÇADE COMPOSITION

Figure 32: Façade composition should reinforce a New England village character

- a) **Emphasize Façade Rhythm and Patterns** – A building façade should be broken into vertical and horizontal parts that reinforce a rhythm and pattern. Vertically, a building should be seen to have a base, middle and top. Horizontally, the building should be broken down into sections that correspond to and indicate bays of the structural system.
- b) **Avoid Long and Blank Façades** – Building façades should be differentiated at intervals typically not less than 50 feet or less by a change in material, a variation in the plane of the wall, decorative components, or functional element such as entryway or portico. Sections of continuous, uninterrupted, or blank building façades typically should not exceed 50 feet.

- c) **Emphasize Primary Façade Height** – The principal façade should not be less than typically about 20 feet in height with an articulation of the base, middle and top.
- d) **Encourage Neutral Building Identity** – Building design and architectural features should reflect a New England village character and should not over-prioritize franchise features or identity. Signage, colors, awnings and other design features should be used to communicate brand and franchise identity. The building form, roof form and façade design should not be overly specific to a franchise or brand.
- e) **Use Human-scaled Façade Features** – Awnings, canopies or other elements that break-down the overall scale of the building façade and provide protection and visual interest at building entries are encouraged. Refer to *Sign Regulations* and design guidelines for specific sign, material and lighting requirements.
- f) **Design Façade for Signage** – The façade design and architectural detailing should provide a purposeful place for signage, if signage is intended to be a part of the façade. An extended parapet, entablature, or sign band should be designed and integrated into the façade layout with appropriate spacing for both the height and width of anticipated signage. Refer to *Sign Regulations* for specific sign, material and lighting requirements.
- g) **Integrate Utilitarian Components into the Façade Design** – All functional, utilitarian, or mechanical components of the building façade should be integrated into the façade or screened so as to be part of the composition of the overall building design. Mechanical vents, service rooms, utilitarian and staging areas, and similar portions of buildings should be hidden to match other materials and colors of the façade. Utilitarian aspects should also be screened by the site and building landscape.



Figure 33: Building façade broken into vertical and horizontal bays reflecting an overall human-scale



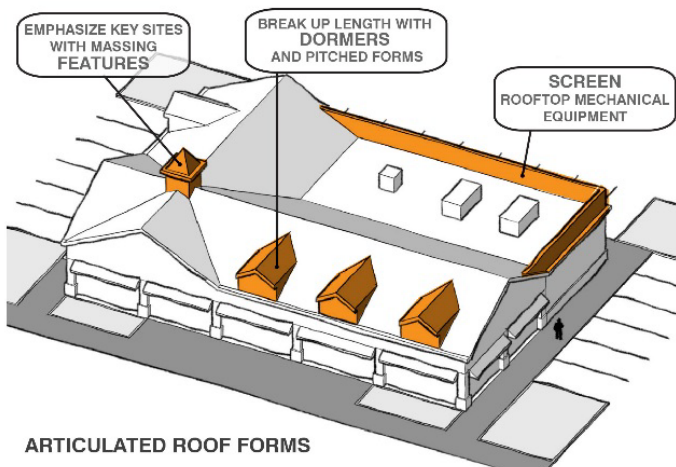
Figure 34: Façade design and architectural detailing provide a purposeful place for signage

3. Building Roof Forms

Building roof form has a significant impact on the character and style of the architecture. Building roof forms should be both authentic to the type of building they are part of and strive to reinforce a sense of New England village character and scale.



Figure 35: Traditional New England roof typologies are encouraged to reinforce a regional sense of place



ARTICULATED ROOF FORMS

Figure 36: Roof forms should reinforce a New England village character

- a) **Reinforce New England Village Character** – Traditional steeply-pitched roof forms are encouraged in order to reinforce a New England sense of place and assist in managing snow loads. Roof slopes should be in the range of 8:12 to 12:12 (vertical: horizontal). Roof styles may include gable, hip, half-hip, mansard, gambrel, saltbox, and shed.
- b) **Reinforce a Human-scale to Buildings** – Large uninterrupted roof forms should be avoided and articulated with roof gables, dormers, chimneys or other roof forms that provide variety and interest to the overall building form.
- c) **Integrate and Screen Utilities** – Mechanical equipment on rooftops should be screened from visibility of pedestrians standing at grade on surrounding walkways by means of walls, decorative grilles, or roof parapets. Screening features should be a part of the building composition and design and use materials that complement the overall roof and façade design. Other utilities, such as solar panels should be integrated into the design of the roof.

4. Building Lighting

Building lighting should be used to highlight and emphasize functional and decorative aspects of the building massing and facades. Building lighting should be energy efficient and designed to be minimized and focused on key components of the building. Lighting design must comply with the lighting requirements of the *Zoning Bylaw*.

- a) **Define Hierarchy of Lighting** – Building entries should be a primary focus of building lighting to reinforce safety, security and convenience for access to the building. Lighting to highlight building features, key architectural elements, accents or

- signage should be a secondary focus of building lighting.
- b) **Minimize Quantity of Lighting** – Illumination levels should be provided at the minimum level that is required to provide the function desired.
 - c) **Coordinate Light Fixture Design** – Lighting fixtures should be selected to contribute to the overall character of the building and site consistent with the overall design and sense of place.

E. Signage Guidelines

The signage guidelines for commercial zones provide guidance in the design of signage that is functional and attractive. In addition to the Design Review Committee’s (DRC) responsibilities to review signage for the Building Department, the DRC is available to provide design assistance for signage applicants that request additional assistance or design resources. Signage design must comply with the dimensional, usage, placement, and other regulations contained in the *Sign Regulations* of the *Zoning Bylaw*. The DRC is also available to review proposed master signage plans as part of a site plan or special permit review process. The guidelines focus on well-designed signage in the context of compliance with applicable regulations.

1. Principles and Intentions

Signage for commercial uses or businesses should communicate a positive and clear identity for the establishment, be part of the building and façade design, harmonize with its surroundings, and respect Medway’s New England village character.

- a) **Reinforce New England Village Character** – Sign design should be appropriate to Medway’s New England village character, including use of historic, muted colors; traditional-style sign face materials such as wood or wood composite; and lettering that is painted, gold foil stamped, carved dimensionally, vinyl cut, sand-blasted or etched, or metal channel-cut.
- b) **Emphasize Legibility and Clarity** – A sign should be readable, simple, and legible with careful



Figure 37: Sign design should reflect a New England village character

consideration of the proportion of lettered and graphic areas to the overall size and location of the sign. Consideration should be given to the purpose and intended audience of the sign, whether vehicular or pedestrian. Signage should be concise and graphically balanced.

- c) **Define Hierarchy of Signage and Purposes** – The most important sign should be most prominent on the site. The most important information on a sign should be the most prominent and emphasized component of the sign. Sign design should optimize communication of the name of the business.

2. Sign Harmony

- a) **Reinforce Compatibility with Context** – Signs should be designed to be compatible with neighboring properties, storefronts and buildings. Compatibility should be considered through sign style, height, type, scale and location.
- b) **Reinforce Compatibility with Residential Context** – Where business uses are interspersed with residential uses, signs should be designed and located with sensitivity to the residential areas. Illumination should be designed to minimize impact on adjacent residences.
- c) **Coordinate Compatibility with Building Architecture** – Sign design and placement should relate to and harmonize with the building architecture. Signs should not overwhelm or obscure building features.
- d) **Complement Other Signs** – Where a business or development has more than one sign, all signs should be designed to be compatible in terms of materials, color, lettering, style and logo use. Design and placement of multiple signs should reflect a clear hierarchy and coordinated overall visual effect.



Figure 38: Sign design and placement should harmonize with the building architecture

3. Sign Characteristics

- a) **Focus Signage Design and Readability** – A sign should be readable, simple, and legible, with sign content that fits comfortably within the space it will

occupy on the building. Generally, a sign’s text and graphic elements should not occupy more than two-thirds of the sign panel area.

- b) **Emphasize Signage Legibility** – Signage typeface should also be simple and legible; ornate or unusual typefaces should be used only for emphasis and restricted to single words or short phrases. The use of both upper and lower case letters reinforces sign legibility.
- c) **Consider Signage Scale** – Signs should be scaled to their use and intended viewer, be that the driving or walking public. Sign lettering and graphics should be clear, simple, and legible from a distance, under different lighting conditions. Scale of sign should be appropriate for its intended audience and its location on a building or site. For automobile-oriented signs, signs should be legible at posted driving limits.
- d) **Focus Signage Content** – Signage messaging should be simple and brief to maximize a sign’s visibility and clarity. Signage should primarily communicate the name of the business or establishment through lettering, graphics or logos. In order to reinforce signage purpose, the following information should not be included on a primary sign: telephone numbers, business hours, website address, sale information, listing of goods and services, brand names carried, or credit cards accepted.
- e) **Use Signage Color** – Signage color should complement building materials and color palette. Signage color should also consider signage legibility and readability from a distance during the day and night. High contrast between signage lettering and backgrounds helps increase legibility. Lighter colored lettering on darker backgrounds is recommended.
- f) **Coordinate Signage Materials** – Signage materials should be selected for durability, ease of maintenance, and compatibility with building materials and design.



Figure 39: Sidewalk signs should be scaled appropriately at a pedestrian scale



Figure 40: Signage integrated with the site features and landscape

4. Site Signage

Site signage includes any sign that is not attached to a building, but is part of the site design and layout to assist in the identification of the development, businesses, or wayfinding on the site.



Figure 41: Multiple tenant directory signage is clear and legible



Figure 42: Wayfinding signage compatible with area character

- a) **Integrate Signage Design with Landscape** – Site signage should be integrated with site landscape design and be used to reinforce gateway locations and site entry points. Landscape plantings should be included to anchor and integrate signage into the site plan. Refer to *Sign Regulations* in the *Zoning Bylaw*.
- b) **Coordinate Signage Placement** – Sign locations should consider lot characteristics in regard to roadway and access considerations, building location, views in and out of the property, pedestrian and vehicular circulation and vehicular safety and visibility. Refer to *Sign Regulations* in the *Zoning Bylaw*.
- c) **Coordinate Signage Style** – Free-standing signage should complement the overall character and design of other site and building components. Free-standing signage should be balanced and proportional. A lollipop sign, which is a single pole sign that has a disproportionately large top and overly slender support base, is discouraged. Incorporate elements of the building design into a free-standing sign design.
- d) **Define Multiple Tenant Directory Signage** – For multi-tenant developments, an internal site directory sign may be provided listing names of businesses and establishments. Directory signage should be clear and legible with the ability to conveniently change business names as tenants move in and out. Design of the sign should be consistent with other development signage.
- e) **Coordinate Wayfinding Signage** – Simple directional signage may be provided on the site to inform visitors of entries, parking areas, building names, numbers or other information. Wayfinding signage should be consistent and compatible with other development signage. Wayfinding signage should

not obstruct or cause conflict with regulatory or traffic-related signage.

5. Building Signage

Building signage includes any sign that is attached to a building to provide identification of businesses.

- a) **Integrate Signage Design with Building** – Signs should integrate with the building on which they are placed, by considering the architectural style, character, or historic significance, rhythm and scale of façade features, and patterns of window and door openings. Particularly with older buildings, care should be taken not to obscure, damage or otherwise interfere with design details and architectural features that contribute to the building’s character.
- b) **Coordinate Signage Placement** – Signs should be designed for the specific building on which they will be placed, and for the specific location on the building. Signs should generally be centered within the wall area of the façade on which they will be located. Signs should not extend beyond the boundaries of the area of the building on which it will be mounted. Signs previously installed on other buildings or locations should not be used.
- c) **Define Multiple Tenant Building Signage** – Multiple tenant or business signs on a building should have a consistent placement and be of a coordinated design. Using signage to reinforce or establish a rhythm, scale and proportion for a building is encouraged, especially where such elements are weak or absent in the building’s architecture. A Master Signage Plan should be developed for multi-tenant developments to encourage a coordinated and compatible approach to signage according to the *Sign Regulations*.



Figure 43: Signage integrated with the design of the building



Figure 44: Sign centered within the façade and within the boundaries of the building



Figure 45: Signage is appropriate for the building’s architectural design and corner storefront location



Figure 46: Awning is opaque and integrated into the building façade

- d) **Coordinate Secondary Signage** – Window and door signage should be coordinated with the overall signage program and may include more detailed information that is not appropriate for larger signs. Window signage is generally directed toward the pedestrian viewer. Window signage should not dominate the glazed surface. Window signage and displays should not include the stockpiling of products or inventory inside the windows.
- e) **Design Awning Signage** – Awning fabric should be opaque, and any awning signage should use cut or screen-printed letters or logos. Lettering and graphic elements should comprise no more than 30 percent of the total awning surface.
- f) **Integrate Sign Mounting** – Projecting signage should be integrated into the design of the façade with attractive sign mounting hardware.
- g) **Complete Sign Location Preparation** – The areas of the building to receive the sign should be prepared, cleaned and painted prior to installation of the sign. Previously installed signs should be completely removed and any remnants or wall surface damage repaired and covered prior to the installation of a new sign.

6. Sign Illumination

External signage illumination is encouraged and should be targeted only onto the sign, not onto adjacent buildings or towards vehicles or pedestrians.



Figure 47: Signs are strongly encouraged to have lighting that projects from above the sign

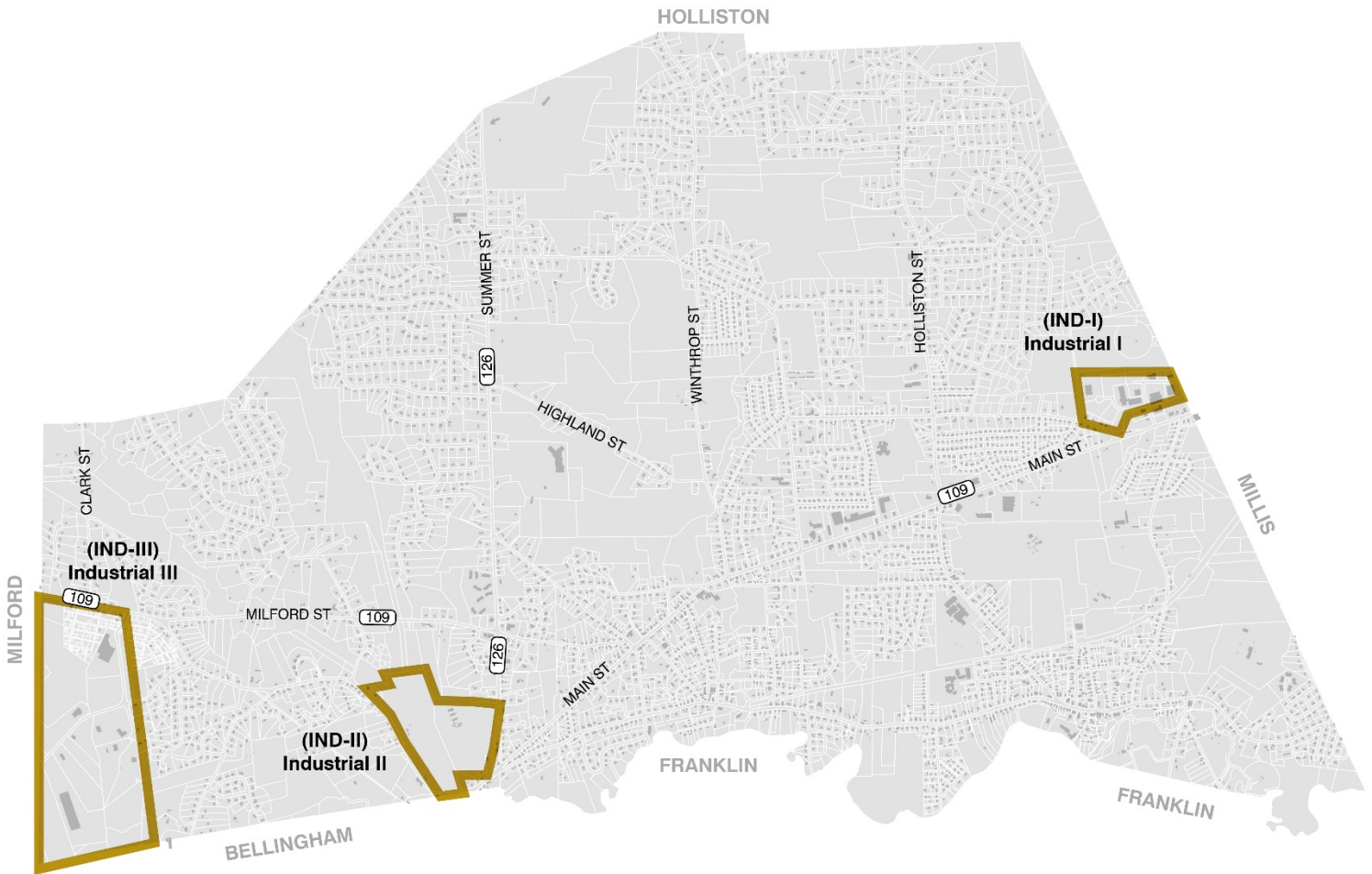
- a) **Focus Awning Sign Illumination** – If a window awning sign is internally illuminated, only the sign letters, logo and ornamentation should be translucent. The background material should be opaque.
- b) **Limit Internal Sign Lighting** – The preferred forms of internally lit signs are those using push-through graphics and text; standard channel letters, also called back-lit or halo-lit; and reverse channel letters with a halo effect. When signs other than channel letters are internally lit, only the sign copy (words/logo) should be illuminated. The sign background or field should be opaque and of a non-

- reflective material. Internally illuminated box cabinet signs are discouraged.
- c) **Integrate Lighting Utilities** – Raceways, conduits and other electrical components should be concealed from public view. When it is not possible to conceal, such utilitarian components should be painted to match the background of the wall on which they are mounted to reduce the visual impact.
 - d) **Coordinate Signage Lighting Fixtures** – External lighting fixtures that project the light from above the sign are strongly encouraged. Light fixtures should be simple and unobtrusive, and should not obscure the sign’s message and graphics.
 - e) **Provide Consistent Lighting Levels** – Lighting should provide a consistent and even wash of light across the sign.



Section 3. Industrial Zones

**TOWN OF MEDWAY
Design Review Guidelines**



Disclaimer: For Official Zoning Map district boundaries and most current information refer to the Zoning Map in Section 6. *References and Resources*

Industrial Zones

**TOWN OF MEDWAY
Design Review Guidelines**

SECTION 3. INDUSTRIAL ZONES

A. Applicability

The *Design Review Guidelines* for industrial zones are applicable to developments located within the Industrial I (IND-I), Industrial II (IND-II), and Industrial III (IND-III) districts. The Design Review Guidelines are also intended to guide Municipal building projects.

B. Principles and Intentions

The Industrial Zone *Design Review Guidelines* are intended to provide specific recommendations for both smaller- and larger-scale industrial developments to ensure the design and site planning respects the traditional New England village character of Medway.

For industrial zones, the following principles will help create a high-quality development that respects its site and surroundings and reinforces a New England village character:

- Buildings should incorporate traditional New England building materials
- Create a building appearance that suggests evolution or modification over time through organic, incremental growth
- Model development on historic agrarian and industrial building themes, such as repeated bays of large windows or a grid of repeated smaller windows, multi-paned windows, corrugated metal siding and roofing, sawtooth roof forms, and simple, straightforward structures that matched the utilitarian purpose
- Site landscape that retains a naturalistic appearance, preserving existing vegetation, enhanced with informal landscape clusters
- Site circulation that provides a pedestrian-friendly pathway network connecting the building to public streets
- Building design that includes human-scale features and detailing, particularly near building entries
- An overall site and building configuration that is sensitive to the surrounding abutters and mitigates the most negative aspects of the industrial use through appropriate building orientation, landscape buffers and screening

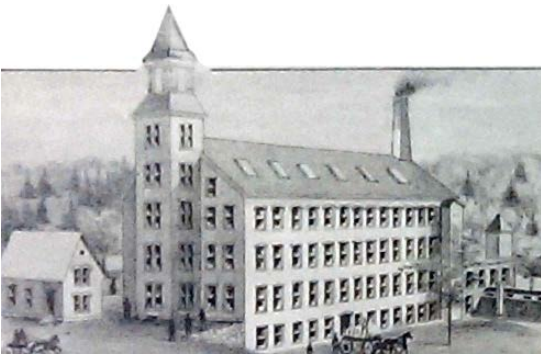


Figure 49: Historic industrial uses in Medway at Sanford Mill

C. Site Improvement Guidelines

The following guidelines outline the site design and layout practices that should be viewed as baseline components for a well-designed industrial development in the Town of Medway.

1. Site Composition

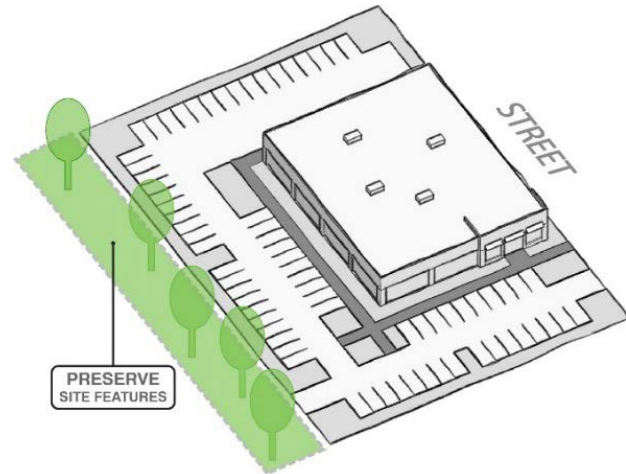
The development's land planning and site design should provide a thoughtful and responsive approach that adapts the development program and site requirements to the conditions of the land.

- a) **Cluster Arrangement** – Site and building components should be clustered to maintain the maximum amount of natural and undisturbed area on the property including maximizing natural and landscape buffers at property boundaries.
- b) **Reduce Negative Impacts** – Site layout should be designed to minimize the visibility and impact of parking, service and utility-oriented functions.

2. Building Orientation

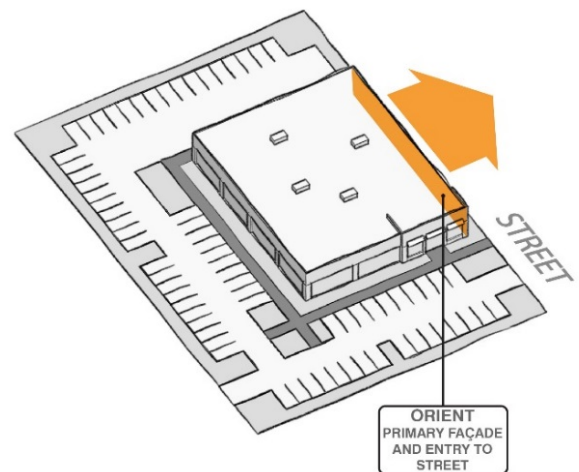
Buildings should be sited to organize the development on the land, frame open space, conceal parking, service and loading areas and reduce negative impacts of the property.

- a) **Orient Building to the Street** – The primary entrances to buildings should face onto and be visible from a public street. Parking should be located to the rear of buildings as viewed from the primary access street.
- b) **Orient Building to Conceal** – Building should be used to conceal parking, service and utility-oriented functions of the property. The building should be used to screen these uses from the street and primary public views into the property.
- c) **Orient Utilities Away from Street** – Utilitarian portions of the building which may include loading docks, mechanical equipment, utilities, outdoor storage or other functional requirements should be oriented away from the street and concealed by the building, site and landscape.



SITE FEATURES AND OPEN SPACE

Figure 50: Cluster industrial development to retain natural buffers, site features and open space



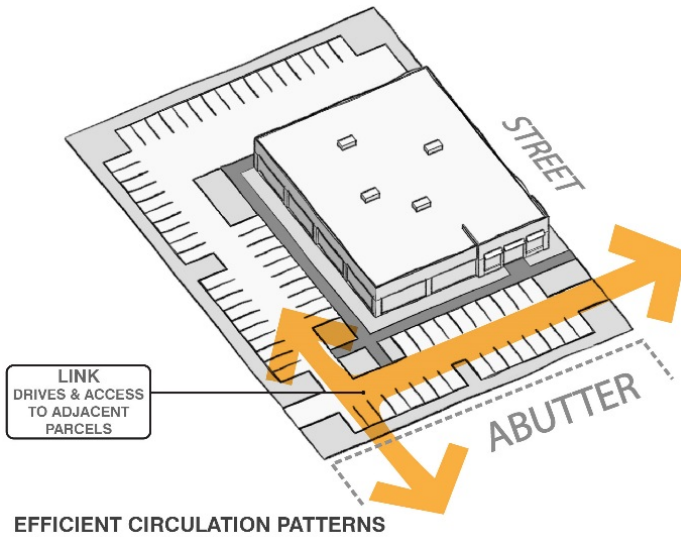
STREET-FACING ORIENTATION

Figure 51: Reinforce an orientation of the primary building façade to the street

3. Site Access

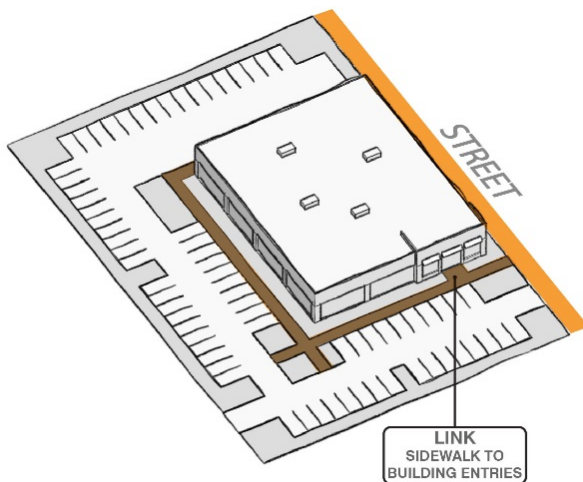
Site access should provide clear and legible routes for all modes of transportation (pedestrians, bicycles, vehicles and public transportation) to connect to the site and to enter internal site circulation systems.

- a) **Minimize Site Access** – The number and width of vehicular access points into and out of the site should be minimized. Where vehicular access drives cross pedestrian routes, pedestrian crossings should be marked and differentiated with variations in paving materials (for example by using stamped concrete or asphalt). Refer to the Medway Department of Public Services for additional requirements as part of the Street Opening Permit process.
- b) **Connect to Public Frontages** – Inviting and efficient sidewalks should be provided along any and all street frontages at the site perimeter. Additionally, sidewalk paths should be provided linking public frontage street(s) to all building entries.
- c) **Connect to Adjoining Properties** – To encourage pedestrian access between properties pedestrian pathways should be provided between buildings on adjacent parcels.
- d) **Coordinate Service, Delivery and Loading Access** – If separate service, delivery or loading access is required for industrial development operations it should be clearly distinct and distinguished from other forms of site access. Functional access that is required for appropriate site operation should not be combined with other uses, but should be coordinated with safe pedestrian routes and crossings on the site.



EFFICIENT CIRCULATION PATTERNS

Figure 52: Site access efficiency between parcels



SIDEWALK CONNECTIONS

Figure 53: Pedestrian connections on site should provide safe and easy access

4. Internal Site Circulation

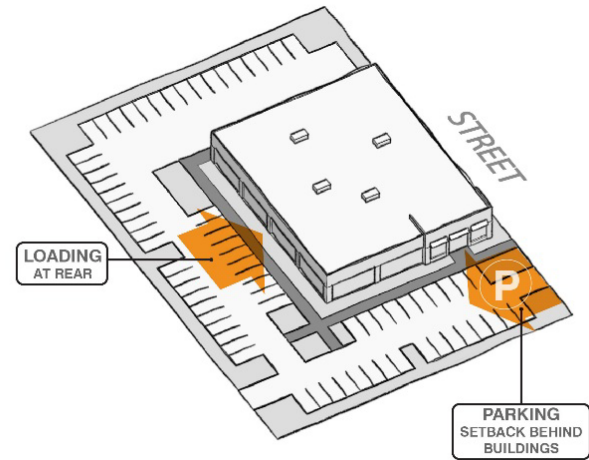
Circulation internal to the site should provide clear and legible routes for all modes of transportation to connect to the public way, building entries site and other site components.

- a) **Promote Pedestrian Circulation** – Functional use of the site should be balanced with pedestrian circulation and safety. Walkways, parking areas, service areas and driveways should be planned so that there is minimal vehicular crossing of pedestrian walkways.
- b) **Design Efficient Site and Parking Circulation** – A well-organized system on-site circulation, drives, parking areas, loading areas and service areas should be used to reduce the overall amount of paved area and reduce the footprint of development.
- c) **Conceal Loading and Service Circulation** – Loading and service areas should be located at the side or rear of buildings, and away from view of public streets. For larger buildings or where heavier loading traffic is anticipated, loading routes should be separated from routes used by other vehicles.

5. Parking

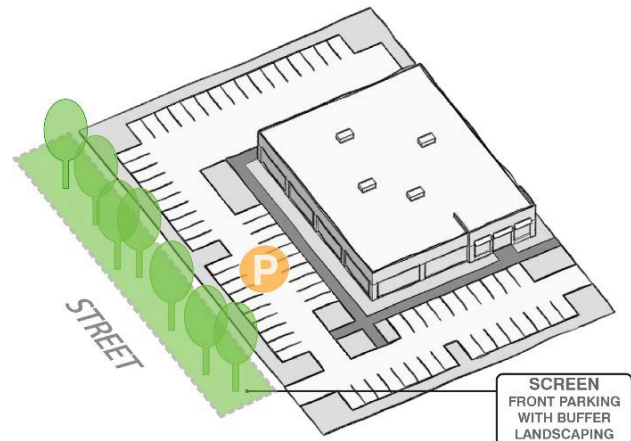
Parking should be placed convenient to the building entries, but not at the expense of pedestrian safety, attractiveness, and aesthetics of the property. Attention should be paid to the appearance of the front yard with appropriate landscape screening of parking. For specific parking requirements refer to the Parking Regulations of the *Zoning Bylaw*.

- a) **Conceal Parking Location** – Parking for employees and service vehicles should be located at the side or rear of buildings, concealed from principal views into the site, and buffered with landscape. Where located to the side of buildings, parking areas should be set back from the street by at least the same distance as the building. Parking in front of a building should be limited to visitor parking.
- b) **Provide Parking Screening** – If a parking area is located in front of a building, relative to the street, it should be screened with low landscaped berms, landscape beds, and/or low fences or stone walls.
- c) **Integrate Parking Landscape** – Large parking areas should be broken into smaller areas by means of landscaped islands containing low plantings and trees. Such islands should be placed at regular intervals across the parking lot to reduce the visual



PARKING AND LOADING

Figure 54: Parking located to the side and rear of buildings with loading in the rear



FRONT PARKING AREAS

Figure 55: Landscape buffer and screening at the street frontage

impact of the parking area and to reinforce a more pleasant pedestrian environment. Landscape islands should also be integrated with pedestrian circulation and crossing routes through parking areas.

- d) **Screen Service, Loading and Storage Areas** – Large parking areas used for service, loading or storage should be located to the rear of the building and appropriately screened and buffered from adjacent property lines.

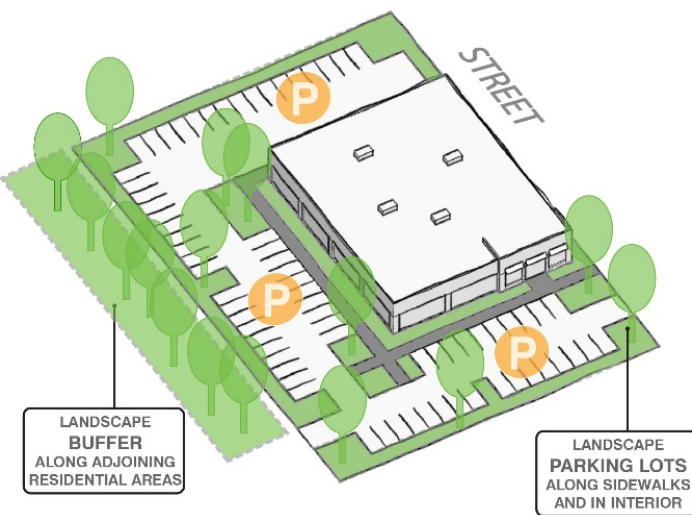
6. Landscape

Parcels should include a hierarchy of landscape that contributes to the overall site design and integrates with adjacent properties. The hierarchy should include entry and gateway landscape, building and building entry landscape, street landscape and landscape buffers. The landscape of an industrial properties edges should provide buffering and screening to adjacent properties. Refer to the required list of species in the *Site Plan Rules and Regulations* that are drought tolerant, native to New England and non-invasive.

- a) **Define Entry and Gateway Landscape** – Entry and gateway landscape should be used to define site access and reinforce a sense of arrival and layout of circulation on the site.
- b) **Integrate General Site Landscape** – Site and building landscape should be used to integrate the buildings into the overall site plan, soften building edges, and enhance public sidewalks, building entries, and any plaza areas.
- c) **Coordinate Landscape at Street Frontages** – Landscape for the site frontages on public ways should contribute to the character of the street and reinforce a consistent street frontage that is integrated with the character, type and spacing of adjacent landscape improvements.
- d) **Provide Screening and Buffering** – Landscape buffers, screening and fencing should be used to conceal dumpsters, recycling areas, stockpiling areas, storage, parking, service, maintenance yards and other equipment or service uses from pedestrian views, streets or property lines.



Figure 56: General site landscape integrates parking, circulation and buffer areas



LANDSCAPING

Figure 57: Landscape used to screen and buffer utilitarian areas of the site and building

- e) **Reinforce Additional Residential Landscape Buffer** - Where an industrial-zoned property abuts a residentially-zoned or used property, landscape buffering and fencing should be provided along the adjoining yard(s). Landscape buffering should be at least three-season and of lushly-planted vegetation averaging two to three feet tall.
- f) **Integrate Functional Features into Landscape** – Stormwater retention areas should be integrated with the site landscape and treated as a naturalized environment and a site feature that is sustainable from a plant material and maintenance perspective. Retaining walls, fencing, guardrails and other utilitarian or screening features should be integrated with the landscape design and designed to contribute to the site character. Functional site features should be designed and considered for views of them from adjacent properties. Utilities or other utilitarian components, such as solar panels, should be screened from public view with berms and plantings.
- g) **Coordinate Functional Features and Materials** – The materials used for functional features, such as retaining walls, drainage structures or other required site elements, should be integrated with the overall site design and material palette. For example, a functional retaining wall should include stone facing to match stone walls on the site.



Figure 58: Industrial properties abutting residential areas should provide landscape buffering

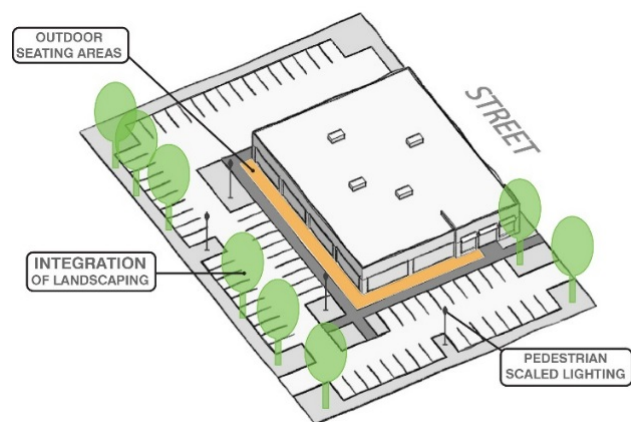


Figure 59: Integrate natural and utilitarian site features into a natural landscape

7. Site Amenities

Site amenities should enhance activity and serve a function near site and building entries and serve to enhance the pedestrian experience. Site amenities should include benches, trash and recycling receptacles, bike racks, and other components appropriate to the use and scale of the development.

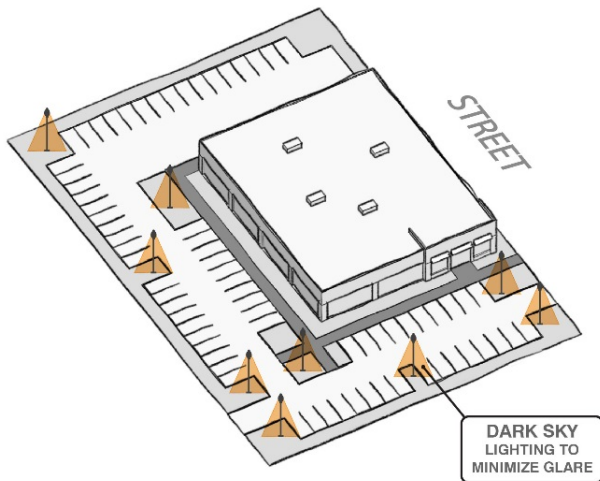
- a) **Coordinate Location of Amenities** – The amenities should be located in high activity areas that are most likely to receive use. For example, places to sit



SITE AMENITIES
pedestrian experience



Figure 61: Design of amenities to match overall site design



SITE LIGHTING

Figure 62: Integrate lighting with the overall site design and circulation pattern

should be provided where people are waiting or congregating as part of the use of the building and site.

- b) **Complement Design with Amenities** – The character and design of the site amenities selected should be consistent with the overall character of the site and building design.
- c) **Integrate Amenities** – Site amenities should be integrated with the site design to allow appropriate clearances, space and circulation around them to allow busy areas to function appropriately.

8. Site Lighting

Site lighting is intended to provide safety in areas with evening activity, particularly near site and building entries and across parking lots, and to provide a minimum level of lighting for nighttime safety. Lighting design must comply with the lighting requirements of the *Zoning Bylaw*.

- a) **Minimize Lighting** – Site lighting should comply with minimum lighting requirements and standards, but not provide lighting in excess of requirements. Downward-directed, dark-sky compliant lighting is required as per the lighting requirements of the *Zoning Bylaw* to minimize excess glare and spillage.
- b) **Integrate Lighting Fixtures with Design** – Lighting fixtures should be selected to contribute to the overall character of the building and site, consistent with the overall design and sense of place.

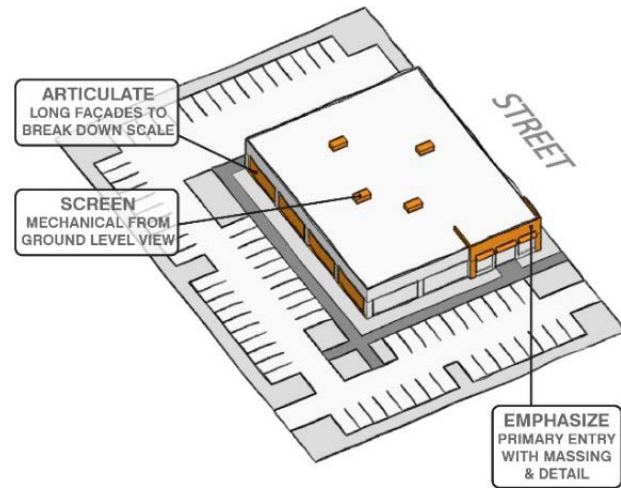
D. Architectural Guidelines

The following guidelines outline the architectural design elements that should be viewed as a baseline for well-designed architecture in the Town of Medway.

1. Building Massing

The building massing should be designed to reduce the overall perceived scale and provide simple and evocative forms that reinforce both a sense of a New England village and a sense of a human-scaled environment.

- a) **Strengthen Prominence of Building Entry** – Building massing should emphasize and highlight the location of the primary building entrance.
- b) **Visually Reduce Large Building Scale** – Large building masses should be broken down in scale through variations in façade bays, setbacks, or other types of architectural articulation.
- c) **Interrupt and Balance Uniform Massing** – Large industrial development result in horizontally oriented structures. Long horizontal building masses should be interrupted by vertical building components or features that break-down the scale and reduce uniformity.
- d) **Integrate Accessibility Features** – Accessibility ramps, lifts or other access requirements should be integrated into the design of the building entry at the building exterior and interior. Accessibility components should be a purposeful part of the building entry design.



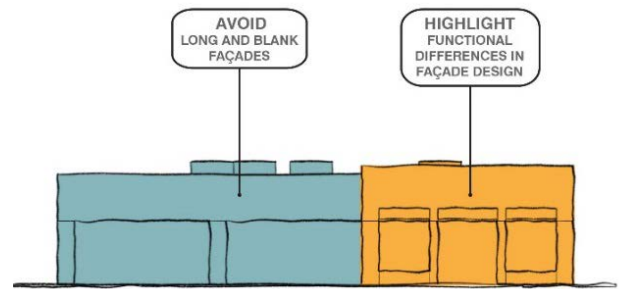
BUILDING MASSING

Figure 63: Articulate building massing to emphasize building entry and reduce overall scale

2. Façade Composition and Components

Composition of building façades should include architectural features and building components that reduce the scale of large building masses, reinforce the character of the building to reflect a New England sensibility and provide detail and articulation of the overall building to break down the scale of large industrial development.

- a) **Emphasize Façade Rhythm and Patterns** – A building façade should be broken into parts that reduce the overall horizontal building scale. Horizontally, the building should be broken down into sections that correspond to and indicate bays of the structural system.
- b) **Avoid Long and Blank Façades** – Building façades should be differentiated at intervals by a change in material, a variation in the plane of the wall, decorative components, or functional element such as entryway or portico. Sections of continuous, uninterrupted, or blank building façades should be avoided. Window patterns should also be used to add interest and variation to building façade.
- c) **Emphasize Primary Façade Height** – The principal façade should not be less than 20 feet in height with an articulation of the base, middle and top.



FAÇADE COMPOSITION

Figure 64: Design buildings to avoid blank façades and to highlight functional differences



Figure 65: Building forms that reflect an industrial heritage

- d) **Reference New England Industrial Heritage** – Model façade design on historic New England mill, agrarian and industrial buildings. Including façade treatments such as repeated bays of large windows or a grid of repeated smaller windows, multi-paned windows, corrugated metal siding and roofing, and simple, straightforward structures that matched the utilitarian purpose.
- e) **Highlight Functional Differences** – Façade materials should be used to differentiate between different functional building areas. For example, the front office portion of a building may have different a different façade design or materials than the larger industrial portion of the building. Plain concrete block should not be used as a façade material for any visible portion of exterior walls.
- f) **Integrate Utilitarian Aspects of Design** – All functional, utilitarian, or mechanical components of the building facade should be integrated into the façade or screened so as to be part of the composition of the overall building design. Mechanical vents, service rooms, and similar portions of buildings should be hidden to match other materials and colors of the façade.

3. Building Roof Forms

Building roof form has a significant impact on the character and style of the architecture. Building roof forms should be both authentic to the type of building they are part of and strive to reinforce a sense of New England village character and scale.

- a) **Articulate Special Roof Areas** – Application of most traditional building roof forms on larger scale industrial buildings will be limited. Smaller accent roof forms that highlight and articulate building entries or other special architectural features are encouraged.
- b) **Reference New England Industrial Heritage** – Other roof types that reflect New England mill and industrial building types with lower roof slopes, roof monitors, or sawtooth roof forms may be appropriate for larger industrial buildings and should be considered as a roof form to reinforce a sense of New England character.

- c) **Integrate and Screen Utilities** – Mechanical equipment on rooftops should be screened from visibility of pedestrians standing at grade on surrounding walkways by means of walls, decorative grilles, or roof parapets. Screening features should be a part of the building composition and design and use materials that complement the overall roof and façade design. Other utilities, such as solar panels should be integrated into the design of the roof.

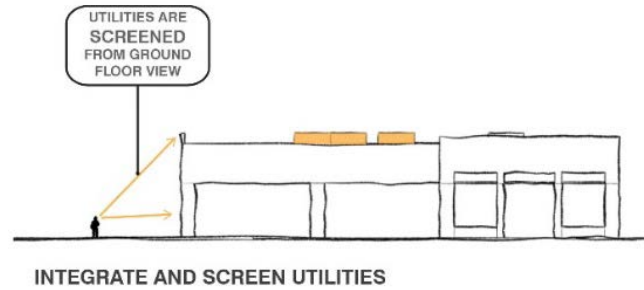


Figure 66: Rooftop mechanical equipment is screened from ground level view

4. Building Lighting

Building lighting should be used functionally, should be energy efficient and designed to be minimized and focused on key components of the building or site that need nighttime light. Lighting design must comply with the lighting requirements of the *Zoning Bylaw*.

- a) **Minimize Quantity of Lighting** – Illumination levels should be provided at the minimum level that is required to provide the function desired.
- b) **Coordinate Light Fixture Design** – Lighting fixtures should be selected to contribute to the overall character of the building and site consistent with the overall design and sense of place.

E. Signage Guidelines

The signage guidelines for industrial zones provide guidance in the design of signage that is functional and attractive. In addition to the Design Review Committee’s (DRC) responsibilities to review signage for the Building Department, the DRC may also provide design assistance for signage applicants that request additional assistance or design resources. Signage design must comply with the dimensional, usage, placement, and other regulations contained in the *Sign Regulations* of the *Zoning Bylaw*. The guidelines focus on well-designed signage in the context of compliance with applicable regulations.

1. Principles and Intentions

Signage for industrial uses or businesses should communicate a positive and clear identity for the establishment, be part of the building and façade design,



Figure 67: Lighting fixtures contribute to the overall character of the site and building

harmonize with its surroundings, and respect Medway’s New England village character.

- a) **Promote Legibility and Clarity** – A sign should be readable, simple, and legible, with careful consideration of the proportion of lettered and graphic areas to the overall size and location of the sign and consideration of the purpose and intended audience of the sign. Signage should be concise and graphically balanced.
- b) **Define Hierarchy of Signage and Purposes** – The most important sign and most important information on a sign should be the most prominent and emphasized component of the sign. Secondary and support information should defer to the most important information. Sign design should optimize communication.



Figure 68: Hierarchy of signage clearly displays the most important information in the largest text sign

2. Sign Harmony

- a) **Ensure Compatibility with Residential Context** – Where industrial uses are near residential uses, signs should be designed and located with sensitivity to the residential areas. Illumination should be designed to minimize impact on adjacent properties.
- b) **Coordinate Compatibility with Other Signs** – Where a business or industrial development has more than one sign, all signs should be designed to be compatible in terms of materials, color, lettering, style and logo use. Design and placement of multiple signs should reflect a clear hierarchy and coordinated overall visual effect.

3. Sign Characteristics

- a) **Focus Signage Design and Readability** – A sign should be readable, simple, and legible, with sign content that should fit comfortably within the space it will occupy on the building. Generally, a sign’s text and graphic elements should not occupy more than two-thirds of the sign panel area.
- b) **Emphasize Signage Legibility** – Signage typeface should also be simple and legible; ornate or unusual

typefaces should be used only for emphasis and restricted to single words or short phrases.

- c) **Coordinate Signage Scale** – Signs should be scaled to their use and intended viewer. Sign lettering and graphics should be clear, simple, and legible from a distance, under different lighting conditions. Scale of sign should be appropriate for its intended audience and its location on a building or site. For automobile-oriented signs, signs should be legible at posted driving limits.
- d) **Simplify Signage Content** – Signage messaging should be simple and brief. Signage should primarily communicate the name of the business or establishment through lettering, graphics or logos. In order to reinforce signage purpose and clarity, the following information should not be included on a primary sign: telephone numbers, business hours, website address, sale information, listing of goods and services, brand names carried, or credit cards accepted.
- e) **Use Signage Color** – Signage color should complement building materials and color palette. Signage color should also consider signage legibility and readability from a distance during the day and night. High contrast between signage lettering and backgrounds helps increase legibility.
- f) **Coordinate Signage Materials** – Signage materials should be selected for durability, ease of maintenance, and compatibility with building materials and design.

4. Site Signage

- a) **Integrate Signage Design with Landscape** – Site signage should be integrated with site landscape design and be used to reinforce gateway locations and site entry points. Landscape plantings should be included to anchor and integrate signage into the site plan. Refer to *Sign Regulations* in the *Zoning Bylaw*.
- b) **Coordinate Signage Placement** – Sign locations should consider lot characteristics in regard to roadway and access considerations, building location, views in and out of the property, pedestrian



Figure 69: Sign is appropriately scaled for the business' customers coming by automobile



Figure 70: Sign is integrated into landscape and reinforces project entry point



Figure 71: Wayfinding signs can help guide visitors to their destination



Figure 72: Sign design integrated with building and placed to mark a building entry point

and vehicular circulation and vehicular safety and visibility. Refer to *Sign Regulations* in the *Zoning Bylaw*.

- c) **Coordinate Signage Style** – Free-standing signage should complement the overall character and design of other site and building components. Free-standing signage should be balanced and proportional. A lollipop sign, which is a single pole sign that has a disproportionately large top and overly slender support base, is discouraged. Incorporate elements of the building design into a free-standing sign design.
- d) **Coordinate Development Signage** – Signage should indicate the overall industrial development and name or branding features. This type of signage should be balanced and in scale with both the scale of the development and the surrounding context.
- e) **Define Directory Signage** – For multi-tenant industrial developments, a directory sign may be provided listing names of businesses and establishments. Directory signage should be clear and legible with the ability to conveniently change business names. Design of the sign should be consistent with other development signage.
- f) **Use Wayfinding Signage** – Simple directional signage may be provided on the site to inform visitors of entries, parking areas, or other information. Wayfinding signage should be consistent and compatible with other development signage. Wayfinding signage should not obstruct or cause conflict with regulatory or traffic-related signage.

5. Building Signage

- a) **Integrate Signage Design with Building** – Signs should integrate with the building on which they are placed, including the architectural style, character, or historic significance, rhythm and scale of façade features, and patterns of window and door openings. Particularly with older buildings, care should be taken not to obscure, damage or otherwise interfere with design details and architectural features that contribute to the building's character.

- b) **Coordinate Signage Placement** – Signs should be designed for the specific building on which they will be placed, and for the specific location on the building. Signs should be centered within the wall area of the façade on which they will be located. A wall sign should not extend beyond the boundaries of the area of the building on which it will be mounted.
- c) **Coordinate Multiple Tenant Building Signage** – Multiple tenant or business signs on a building should have a consistent placement in order to establish a rhythm, scale and proportion for a building, especially where such elements are weak or absent in the building’s architecture. A Master Signage Plan should be developed for multi-tenant developments to encourage a coordinated and compatible approach to signage according to the *Sign Regulations*.
- d) **Coordinate Secondary Signage** – Window and door signage should be coordinated with the overall signage program and may include more detailed information that is not appropriate for larger signs. Window signage should not dominate the glazed surface. Window signage and displays should not include the stockpiling of products or inventory in the windows.
- e) **Coordinate Awning Signage** – Awning fabric should be opaque, and any awning signage should use cut or screen-printed letters or logos. Lettering and graphic elements should comprise no more than 30 percent of the total awning surface.
- f) **Integrate Sign Mounting** – Projecting signage should be integrated into the design of the façade with attractive sign mounting hardware.
- g) **Complete Sign Location Preparation** – The areas of the building to receive the sign should be prepared, cleaned and painted prior to installation of the sign. Previously installed signs should be completely removed and any remnants or wall surface damage repaired and covered prior to the installation of a new sign.



Figure 73: Sign is consistent with site architecture and provides information on the multiple tenants in the site area

6. Sign Illumination

External signage illumination is encouraged and should be targeted only onto the sign, not onto adjacent buildings or towards vehicles or pedestrians.

- a) **Minimize Awning Sign Illumination** – If a window awning sign is internally illuminated, only the sign letters, logo and ornamentation should be translucent. The background material should be opaque.
- b) **Minimize Internal Sign Lighting** – The preferred forms of internally lit signs are those using push-through graphics and text; standard channel letters, and reverse channel letters with a halo effect. When signs other than channel letters are internally lit, only the sign copy (words/logo) should be illuminated. The sign background or field should be opaque and of a non-reflective material. Internally illuminated box cabinet signs are discouraged.
- c) **Integrate Lighting Utilities** – Raceways, conduits and other electrical components should be concealed from public view. When it is not possible to conceal, such utilitarian components should be painted to match the background of the wall on which they are mounted to reduce the visual impact.
- d) **Coordinate Signage Lighting Fixtures** – External lighting fixtures that project the light from above the sign are strongly encouraged. Light fixtures should be simple and unobtrusive, and should not obscure the sign’s message and graphics.
- e) **Provide Consistent Lighting Levels** – Lighting should provide a consistent and even wash of light across the sign.



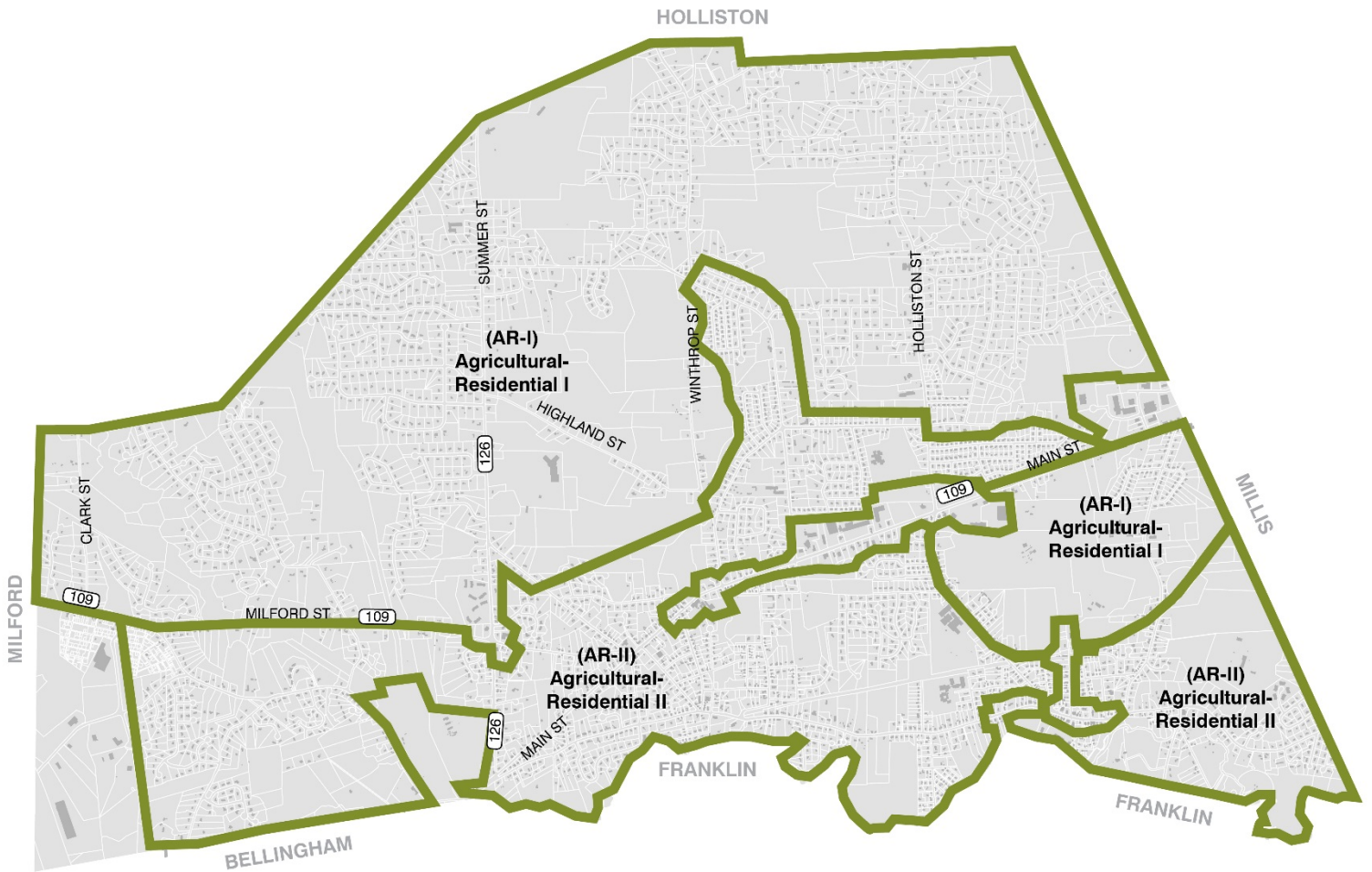
Figure 74: External lighting fixtures that project light onto the sign are strongly encouraged

Medway, Massachusetts, August 2015



Section 4. Residential Zones

**TOWN OF MEDWAY
Design Review Guidelines**



Disclaimer: For Official Zoning Map district boundaries and most current information refer to the Zoning Map in Section 6. *References and Resources*

Residential Zones

**TOWN OF MEDWAY
Design Review Guidelines**

SECTION 4. RESIDENTIAL ZONES

A. Applicability

The *Design Review Guidelines* for residential zones are applicable to developments within the Adult Retirement Community Planned Unit Development (ARCPUD), Open Space Residential Development Overlay District (OSRD) and Multifamily Overlay districts in the AR-I and AR-II zoning districts. Generally, the residential zone *Design Review Guidelines* apply to large scale residential development, multifamily residential development and Special Permit residential development. Site improvements in conventional subdivisions are subject to the *Design Review Guidelines* for residential zones. The Design Review Guidelines are also intended to guide Municipal building projects.

B. Principles and Intentions

The design and construction of high quality residential communities is becoming more important in the residential market. High quality site planning, building design and residential amenities are a competitive advantage in the marketplace and result in economic benefit. The Residential Zone *Design Review Guidelines* provide specific recommendations for larger scale residential development that requires a Special Permit with the intention of reinforcing the traditional New England village character of Medway and high quality design. For residential zones, the following principles apply:

- Building scale and site composition that reinforces the human scale of the built environment with buildings that are relatable to the pedestrian and a walkable site design with inviting streetscapes or small public spaces for socializing and gathering
- Building architecture that is varied and eclectic in style that echoes traditional New England building character with traditional details, materials and colors
- Cluster smaller residential buildings together on smaller lots, while preserving larger contiguous tracts of open space
- Arrange larger buildings around a central green space that is framed by paths, roads or landscape
- For larger buildings, employ an additive building massing that suggests evolution or modification over time through organic, incremental growth. For example, the classic New England connected farmhouse



Figure 76: Historic residential uses in Medway on Mechanic Street

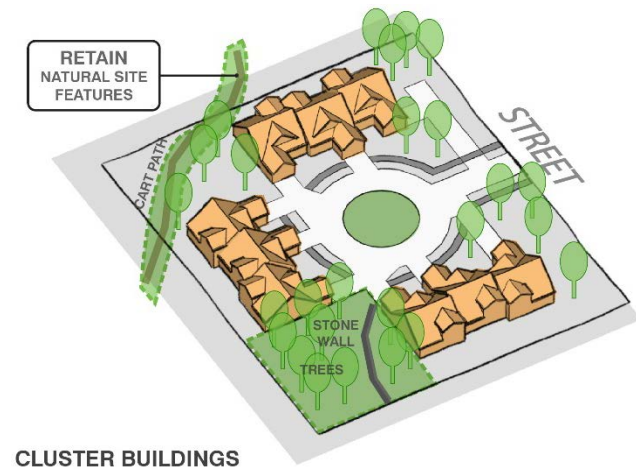
C. Site Improvement Guidelines

The following guidelines outline the site design and layout practices that should be viewed as baseline components of a well-designed residential development in the Town of Medway.

1. Site Composition

The development's land planning and site design should provide a thoughtful and responsive approach that adapts the development program and site requirements to the conditions of the land.

- a) **Cluster Arrangement** – Site design should be used to reinforce the sense of a New England village environment with clustered buildings that reinforce a sense of community.
- b) **Frame Views and Spaces** – Site and building layouts should frame purposeful clearings, enhance desirable views and reinforce privacy between residential buildings.
- c) **Integrate Natural Site Features** – The site design should take advantage of the natural site features by maintaining, incorporating or adapting the inherent characteristics of the property (topography, landscape features and vegetation, rock formations, stone walls, etc.) to guide and benefit the layout and design of the site.
- d) **Create Compact Development Footprint** – Site and building components should be clustered to maintain the maximum amount of natural and undisturbed open space on the property. Natural site features, such as mature trees, groves, and woodland buffers, should be retained as part of the residential development to benefit the site layout and surrounding community.
- e) **Reduce impact of parking** – Site layout should be designed to minimize the visibility and impact of parking, service, and utility-oriented functions of the property. Parking should be provided in smaller areas distributed among residential buildings and by means of on-street parking or parking in driveways.
- f) **Share Open Space** – Use common open space as a design feature in the layout of building clusters.



CLUSTER BUILDINGS

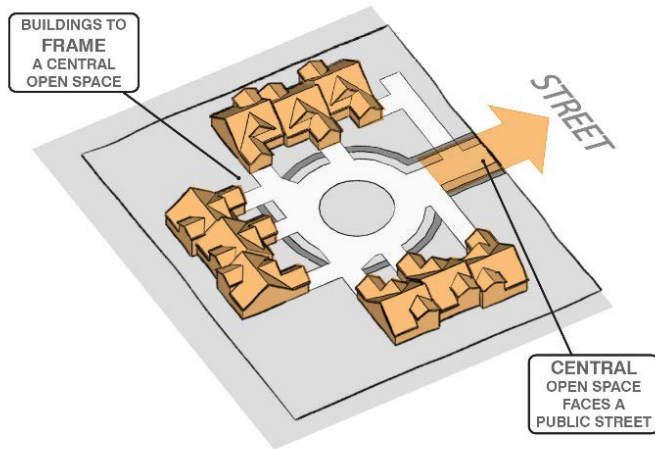
Figure 77: Cluster building and site components to frame views, open space and natural features



Figure 78: Natural site features integrated into the residential development

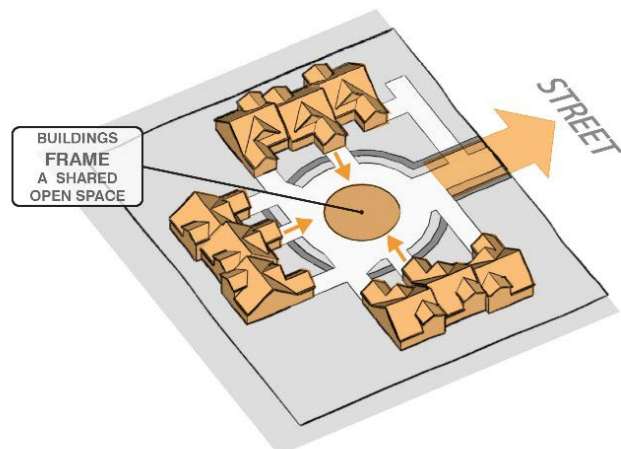
2. Building Orientation

For larger-scale residential development, buildings should be used to organize the site, reinforce a sense of community, frame open space, and conceal parking, service and loading.



STREET-FACING ORIENTATION

Figure 79: Buildings should frame open space and street edges while concealing parking



BUILDING RELATIONSHIPS

Figure 80: Buildings address each other to form a central, open space

- a) **Orient Building to Street and Open Space** – Primary building facades should be oriented to primary public street frontages and/or primary open spaces that in turn are bordered by public streets.
- b) **Relate Buildings to Each Other** – A development that includes multiple residential buildings should orient the buildings to address each other and to frame street frontages and shared open spaces.
- c) **Respect Patterns of Context** – Building setbacks should be consistent with the zoning requirements and consider the pattern of buildings in the context of the surrounding residential community. A setback distance similar to neighboring buildings reinforces a rhythm and pattern of the district.
- d) **Vary Building Relationships** – Variation between buildings and the manner in which they frame open spaces should occur to create distinct relationships between buildings and open spaces.
- e) **Conceal Parking with Buildings** – Buildings should be oriented so that surface parking and garages are concealed in secondary locations and are not the primary visual focus of the development.

3. Site Access

Site access should provide clear and legible routes for all modes of transportation (pedestrians, bicycles, vehicles and public transportation) to connect to the site and to enter internal site circulation systems.

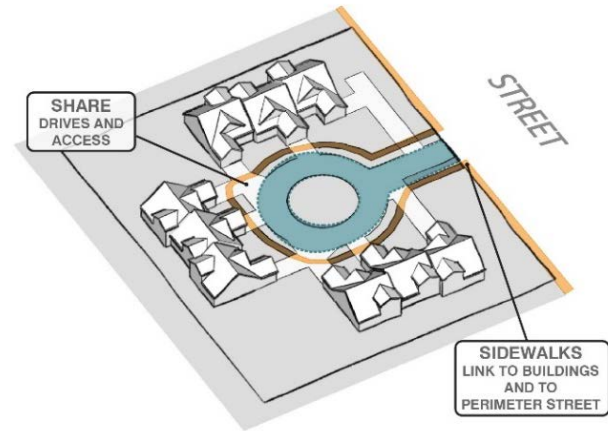
- a) **Minimize Site Access** – The number and width of vehicular access points into and out of the site should be minimized. Pedestrian crossings should be marked and differentiated with variations in paving materials (for example by using stamped concrete or asphalt). Refer to the Medway Department of Public Services for additional requirements as part of the Street Opening Permit process.

- b) **Connect to Public Frontages** – Inviting and efficient sidewalks should be provided along any and all street frontages at the site perimeter. Additionally, sidewalk paths should be provided linking public frontage street(s) to all building entries.

4. Internal Site Circulation

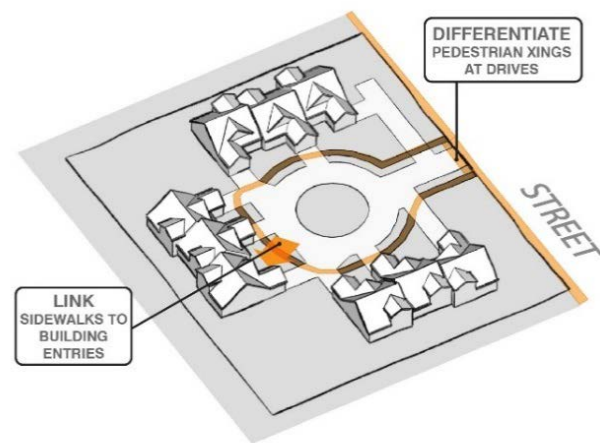
Circulation internal to the site should provide clear and legible routes for all modes of transportation to connect to the public way, building entries site and other site components.

- a) **Complete Circulation System** – A complete access system for all modes of transportation, pedestrians, bicycles and vehicles, should be integrated into the site design. The vehicular road network should provide connecting routes between adjacent parcels and opportunities to enhance connectivity within the Town.
- b) **Promote Pedestrian Circulation** – Internal site vehicular circulation routes should be designed with traffic calming, such as narrow travel lanes and marked pedestrian crossings, to slow vehicular traveling speeds and reinforce a safe and welcoming pedestrian environment. Pedestrian crossings should be marked and differentiated with variations in paving materials such as stamped concrete or asphalt.
- c) **Enhance Pedestrian Connectivity** – Sidewalks should be provided along all street frontages at the site perimeter. Sidewalk paths should be provided on new streets within the site, linking public frontage street(s) to building entries. Existing footpaths on the site should be accommodated and integrated into the site circulation to provide access across or through the site. Site circulation that contributions to connectivity of existing trail systems (Medway Link Trail or others) should be integrated into the site access and circulation and be a part of the residential community amenities.



SITE CIRCULATION

Figure 81: Site circulation should provide safe and convenient access for all modes of travel



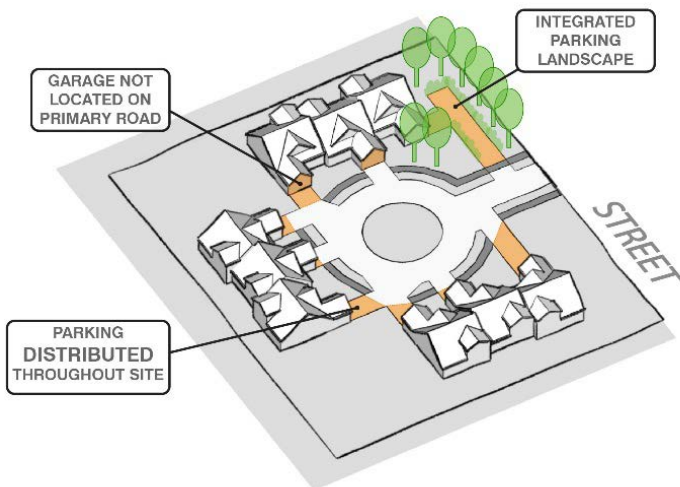
SIDEWALKS CONNECT TO SITE AMENITIES

Figure 82: Pedestrian connectivity to and from the site should be reinforced by internal circulation paths

- d) **Integrate Bicycle Circulation and Connections** – Access and circulation for bicycles on site should be considered for safety and amenity with provision for places to lock bicycles near building entries.
- e) **Create Efficient Parking and Circulation** – Shared driveways should be used for adjacent residences. Efficient parking and access configurations should be employed that minimize repetitious infrastructure and impervious surfaces. Clustered infrastructure and access should be used to reinforce clustered building patterns.

5. Parking

Parking should be placed convenient to the building entries, but not at the expense of pedestrian safety, attractiveness and aesthetics of the property. Parking is necessary to support the function and economic vitality of a development, but it should not be viewed as utilitarian only. Parking should be integrated with other site amenities that support a sense of place and community. For specific parking requirements refer to the Parking Regulations of the *Zoning Bylaw*.



ON-SITE PARKING

Figure 83: Parking is placed on secondary or auxiliary driveways and distributed throughout the site

- a) **Minimize Parking Location and Orientation** – For residential uses with a garage, the garage door should not be located on the primary building façade or street frontage. On secondary building facades, the location and design of garage doors should be integrated with the design of the façade so that the garage door is not the prominent feature of the façade.
- b) **Distribute Parking Areas** – Parking areas should be distributed on the site in multiple smaller parking areas that are integrated with the site plan, building layout and site amenities to reduce the overall visual impact of parking on the residential community. Distributed parking areas should be located to the rear and side of buildings with respect to the front or any side street.

- c) **Create On-street Parking in Pockets** – On-street parking (parallel, angled and perpendicular) are encouraged in pockets integrated with the overall site circulation and landscape plan to meet parking requirements. Parking in individual garage driveways should also be used as part of the overall parking supply within an integrated site plan. Where parking pockets are located near a residential building they are encouraged to be of a material other than asphalt to reinforce the appearance of a pedestrian plaza.
- d) **Provide Visitor Parking** – Visitor parking should be provided for a residential development beyond that required for unit parking. Visitor parking should be located in a central area convenient to most units or near shared open space.
- e) **Reinforce Parking Screening** – When adjacent to a shared open space or residential building, parking should be screened from view through the use of low landscape berms, landscape beds, and/or low fences or stone walls.
- f) **Integrate Parking Landscape** – Large parking areas should be broken into smaller areas by means of landscaped islands containing low plantings and trees. Such islands should be placed at regular intervals across the parking lot to reduce the visual impact of the parking area and to reinforce a more pleasant pedestrian environment.



Figure 84: On-street parking is encouraged in pockets integrated with the overall site circulation

6. Open Space

Larger scale residential development projects have a unique opportunity to provide open space integrated with the overall site plan design. Several characteristics of this type of open space are important to the character and quality of the residential community.

- a) **Define Public and Private Space** – In a residential community creating clear and distinct boundaries between public space and private space are very important. This can be accomplished through the



Figure 85: Shared public open space in the form of a community garden



Figure 86: Town-wide open space integrated within a residential development

configuration of buildings, paths, fences and landscape.

- b) **Design Shared Open Space** – In the context of the residential development, shared open space, is open space that is shared by the residential community. It is a common resource shared among residential units of the development, but is not considered a public open space. A shared open space area should be provided. Depending on the scale and use of the open space, this area could include natural park areas, small pedestrian plazas, playgrounds, community gardens, outdoor seating, landscape, and other amenities.
- c) **Create Private Open Space** – In addition to shared open space, private open space may also be provided in a residential development. It would be dedicated for use by a single unit with clear boundaries and potentially associated with ownership, leases or deed restrictions.
- d) **Consider Town-wide Open Space** – Different from a shared open space, a Town-wide Open Space would be a public open space that would be available for community use beyond the residential development.
- e) **Create Open Space Connections** – Development open spaces should also link to existing or proposed trails or pathways in and around the property, creating a network of connected open spaces and walking routes.



Figure 87: Entry and gateway landscape that anchors a sense of arrival

7. Landscape

Residential development should include a hierarchy of landscape that contributes to the overall site design and integrates with adjacent properties. Refer to the required list of species in the *Site Plan Rules and Regulations* that are drought tolerant, native to New England and non-invasive.

- a) **Define Entry and Gateway Landscape** – Entry and gateway landscape should be used to define site access and reinforce a sense of arrival on the site. The gateway landscape may be integrated with signage and branding features for the residential

community. This type of feature should be simple and balanced with the overall development.

- b) **Integrate General Site Landscape** – Residential site landscape should be used to provide privacy, frame views and reinforce a sense of New England character by defining edges and clearings.
- c) **Coordinate Scale of Landscape** – Selection of plantings and maturity of plantings should be carefully considered relative to the overall scale of development. The scale of the installed landscape should be directly tied to the scale of development and buildings. New plantings should be selected for reasonable maturity at the time of installation to achieve a more full appearance quickly.
- d) **Define Building Landscape** - Building landscape should be used to integrate the buildings into the overall site plan, soften building edges, and enhance public sidewalks, building entries, and any shared open spaces, community or plaza areas.
- e) **Create a Layered Landscape** – Building landscape should be used to establish zones of privacy for residential uses with a pattern of plantings and landscape design that reinforces the design and pattern of interconnected public, semi-public and private open spaces.
- f) **Highlight Feature Landscape** – At locations that are significant in the overall site design or near site features or amenities, the landscape should be used to reinforce the importance of this site component. An additional number of plantings, unique composition or variation in planting species, scale, or plant species with special seasonal variation should be used to reinforce site features.
- g) **Provide Landscape Screening and Buffers** – Landscape should be used to integrate and conceal dumpsters, recycling areas, and other equipment or service uses from view by residential units or pedestrian areas. Additional landscape buffering should be provided where the property abuts another type of use to reinforce the buffer between the properties.



Figure 88: A layered landscape that indicates semi-private and shared space

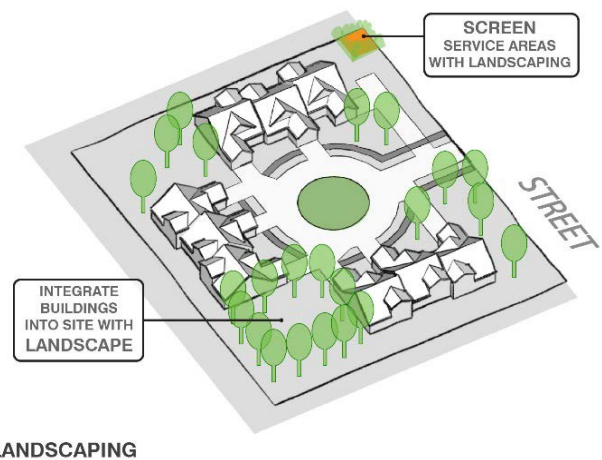
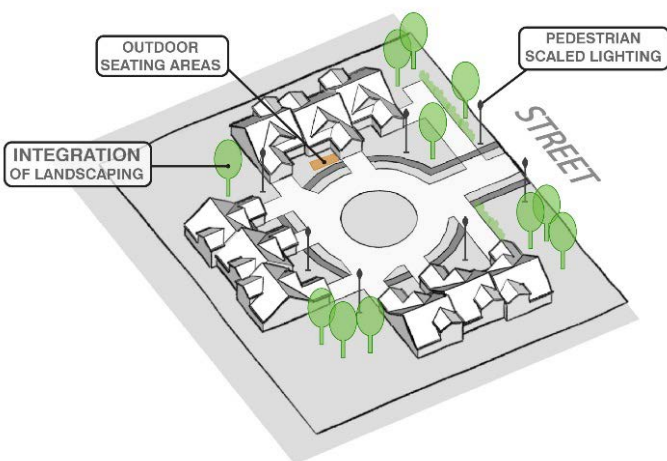


Figure 89: Use landscape to integrate buildings, add privacy and screen unwanted views



Figure 90: A stormwater component integrated into a natural landscape

- h) **Coordinate Landscape at Street Frontages** – Streetscape for the site frontages on public ways should contribute to the character of the street and reinforce a consistent street context that is integrated with the character, type and spacing of adjacent landscape improvements. The landscape frontage should particularly respect and reflect the character of the environment if it fronts on a Scenic Road.
- i) **Integrate Functional Features into Landscape** – Stormwater retention areas should be integrated with the site landscape and treated as a naturalized environment and site feature that is sustainable from a plant material and maintenance perspective. Retaining walls, fencing, guardrails and other utilitarian or screening features should be integrated with the overall landscape design and designed to contribute to the overall site character. Functional site features should be designed and considered for views of them from adjacent properties.
- j) **Integrate Functional Features and Materials** – The materials used for functional features, such as retaining walls, drainage structures or other required site elements, should be integrated with the overall site design and material palette. For example, a functional retaining wall should include stone facing to match stone walls on the site.



SITE AMENITIES

Figure 91: Site amenities are designed at a pedestrian scale to maximize use

8. Site Amenities

Site amenities should enhance activity and serve a function near site and building entries and serve to enhance the pedestrian experience. Site amenities may include benches, trash and recycling receptacles, bike racks, and other components appropriate to the use and scale of the development.

- a) **Coordinate Location of Amenities** – Site and open space amenities should be located as appropriate to level of activity and site use in higher activity areas that are most likely to receive use.
- b) **Coordinate Design of Amenities** – The character and design of the site amenities selected should be

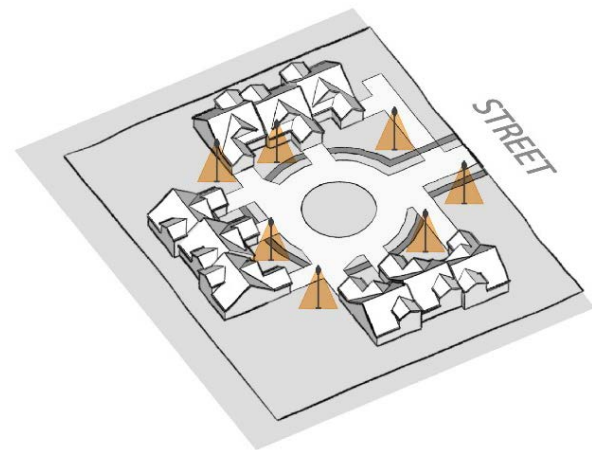
consistent with the overall character of the site and building design.

- c) **Integrate Amenities** – Site amenities should be integrated with the site design to allow appropriate clearances, space and circulation around them to allow busy areas to function appropriately.

9. Site Lighting

Site lighting is intended to provide safety in areas with evening activity, particularly near site and building entries, across parking lots, paths and open spaces, and to provide a minimum level of lighting for nighttime safety. Lighting design must comply with the lighting requirements of the *Zoning Bylaw*.

- a) **Minimize Lighting** – Site lighting should comply with minimum lighting requirements and standards, but not provide lighting in excess of requirements. Downward-directed, dark-sky compliant lighting is required as per the lighting requirements of the *Zoning Bylaw* to minimize excess glare and spillage.
- b) **Integrate Lighting Fixtures with Design** – Lighting fixtures should be selected to contribute to the overall character of the building and site, consistent with the overall design and sense of place.
- c) **Create Multiple Layers of Site Lighting** – Site lighting should perform multiple functions on multiple areas on the site for multiple users. A site lighting approach should be designed for vehicles, pedestrians, building entry areas and site features. Each of these multiple areas should be designed in coordination and to complement the overall character of the site. Lighting should be used to highlight key areas and attractive features of the design.
- d) **Define Entry, Gateway and Feature Lighting** – Site lighting that is highlighting a specific site element should be confined to focusing on that site element and become too prevalent as a lighting technique. Such feature lighting should be used in the foreground of element or accent. This may include lighting a sign, a wall, landscape plantings or other feature.



PEDESTRIAN-SCALED LIGHTING

Figure 92: Multiple layers of site lighting designed at a pedestrian scale for multiple users



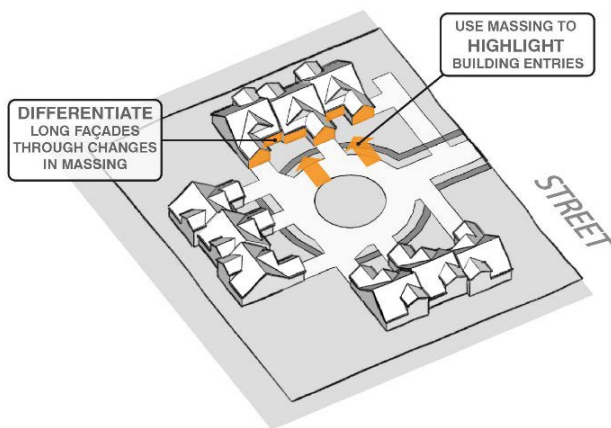
Figure 93: Simple pedestrian-scale light fixtures integrated with the landscape

D. Architectural Guidelines

The following guidelines outline the architectural design elements that should be viewed as a baseline for well-designed residential architecture in the Town of Medway.

1. Building Massing

The building massing should be designed to reduce the overall perceived scale and provide simple and evocative forms that reinforce both a sense of a New England village and a sense of a human-scaled environment.



BUILDING MASSING

Figure 94: Break-down the scale of large building masses



Figure 95: Integrate historic structures into the residential development

- a) **Strengthen Prominence of Building Entry** – Building massing should reinforce the purpose and readability of the building. For example, building massing should emphasize and highlight the location of the primary building entrance.
- b) **Visually Reduce Larger Building Scale** – In the ARCPUD and OSRD overlay districts, large building masses should be broken down through variations in the roof lines, bays, setbacks, upper-level setbacks, and other types of architectural articulation. Larger buildings should look like smaller component parts put together.
- c) **Simplify Smaller Buildings** – Smaller building masses should remain simple and not overly complicated.
- d) **Integrate Historic Structures** – Existing historic structures should be integrated into any new development plan. New buildings and additions should complement and reflect the structure and style of any existing older structures. Historic structures should be considered for adaptive reuse, preservation, sensitive rehabilitation or restoration as may be appropriate to the historic structure and nature of its reuse.

- e) **Reinforce Consistency with Residential Context** – Building massing and scale should be developed to be consistent with the surrounding residential context. Building scale must respect the scale of the residential prototype on which it is based. For example a connected farmhouse prototype loses its effectiveness and meaning if it becomes too exaggerated or overinflated.
- f) **Design Variation in Type and Scale** – An eclectic variation is a signature of New England villages, residential development of multiple buildings should vary the building scale and building type. Buildings types should include enough variability in building massing and scale that repetition is not immediately apparent.

2. Façade Composition and Components

Composition of building facades should include architectural features and building components that reduce the scale of large building masses, reinforce the character of the building to reflect a New England village style, and provide detail and articulation of the overall building.

- a) **Emphasize Façade Rhythm and Patterns** – Building façades should be broken into vertical and horizontal parts that reinforce a rhythm and pattern in the architecture. Building facades, pattern of windows and doors and the roof forms should be integrated as a cohesive design. Variation in the façade is encouraged through decorative components, or functional elements such as porches or entryways.
- b) **Highlight Architectural Detail** – Additional architectural detail should be used to reinforce the smaller scale residential character through the use of roof brackets, porches, covered entries, window and door surrounds, or pediment or parapet detail.
- c) **Conceal Garage Doors** – Garage doors should be designed to integrate with the building façade and relate to the aesthetic of carriage doors or barn doors more frequently associated with New England character.



Figure 96: Vary the residential building type to include other features of New England character such as agricultural heritage



Figure 97: A larger building façade is broken into vertical and horizontal patterns with a balanced rhythm



Figure 98: Simple architectural detail reinforces a quality residential development

- d) **Integrate Utilitarian Components into the Façade Design** – All functional, utilitarian, or mechanical components of the building façade should be integrated into the façade or screened so as to be part of the composition of the overall building design. Mechanical vents, service rooms, and similar portions of buildings should be hidden to match other materials and colors of the façade.

3. Historic Structures

When such structures exist, a residential development should integrate and leverage the value of a historic structure within the design and layout of the redevelopment plan. Reuse of existing historic structures should follow the U.S. Secretary of the Interior’s Standards for Rehabilitation.

- a) **Emphasize Compatible Development** – The reuse of the existing historic structure should be compatible with the ability of the structure to accommodate residential uses. New construction or additions should also be compatible with and complementary to the architectural style of the historic structure.
- b) **Pursue Thoughtful Renovation** – When renovation of a historic structure is occurring for reuse, that renovation should be thoughtfully considered to retain the integrity of the historic structure and be sensitive to its underlying design characteristics or historic significance.
- c) **Create Authenticity to Current Time** – New construction or additions should be authentic to the current time in which they are built.

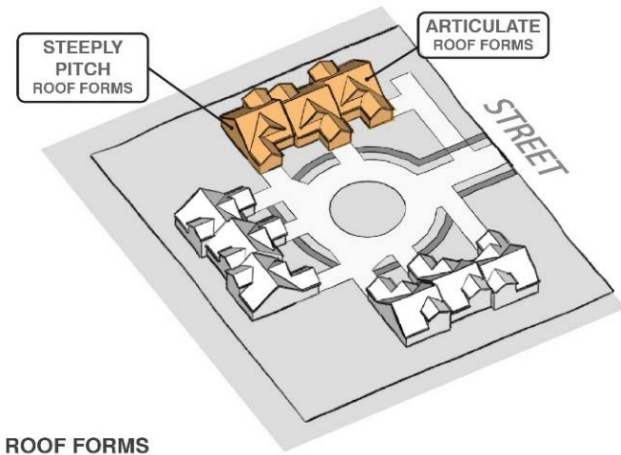
4. Building Roof Forms

Building roof form has a significant impact on the character and style of the architecture. Building roof forms should be both authentic to the type of building they are part of and strive to reinforce a sense of New England village character and scale.



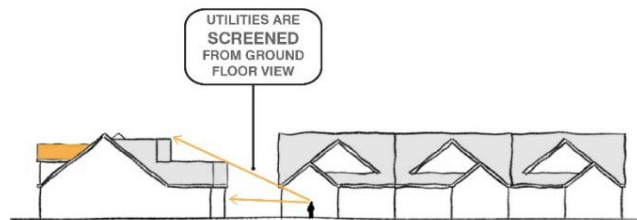
Figure 99: Multiple roof forms reinforce a residential scale to the development

- a) **Reinforce New England Village Character** – Traditional steeply-pitched roof forms are encouraged in order to reinforce a New England sense of place and assist in managing snow loads. Roof slopes should be in the range of 8:12 to 12:12 (vertical: horizontal). Roof styles may include gable, hip, half-hip, mansard, gambrel, saltbox, and shed.
- b) **Develop Roof Variation** – Variation in roof pitch and heights contribute to a New England residential character. Gable, gambrel, hip, mansard, Cape Cod and saltbox style roofs are the most appropriate forms for residential uses. Variation in roof types could be used across a multiple building residential development, but should not be used combined on a single building.
- c) **Reinforce a Human-scale to Buildings** – Large uninterrupted roof forms should be avoided and articulated with roof gables, dormers, brick or stone chimneys or other roof forms that provide variety and interest to the overall building form.
- d) **Integrate and Screen Utilities** – Mechanical equipment on rooftops should be screened from visibility of pedestrians standing at grade on surrounding walkways by means of walls, decorative grilles, or roof parapets. Screening features should be a part of the building composition and design and use materials that complement the overall roof and façade design. Other utilities, such as solar panels should be integrated into the design of the roof.



ROOF FORMS

Figure 100: Articulate roof forms with gables, dormers or other features



INTEGRATE AND SCREEN UTILITIES

Figure 101: Mechanical equipment is screened from visibility of pedestrians.

5. Building Lighting

Building lighting should be used to highlight and emphasize functional and decorative aspects of the building massing and facades. Building lighting should be energy efficient and designed to be minimized and focused on key components of the building. Lighting design must comply with the lighting requirements of the *Zoning Bylaw*.

- a) **Define Hierarchy of Lighting** – Building entries should be a primary focus of building lighting to reinforce safety, security and convenience for access to the building. Lighting to highlight building

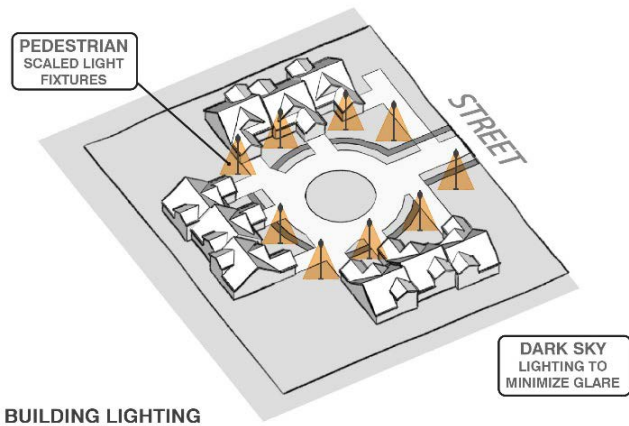


Figure 102: Lighting should be integrated with the site design and reinforce pedestrian safety

features, key architectural elements, accents or signage should be a secondary focus of building lighting.

- b) **Minimize Quantity of Lighting** – Illumination levels should be provided at the minimum level that is required to provide the function desired.
- c) **Coordinate Light Fixture Design** – Lighting fixtures should be selected to contribute to the overall character of the building and site consistent with the overall design and sense of place.

E. Signage Guidelines

The signage guidelines for residential zones provide guidance in the design of signage that is functional and attractive for residential communities. In addition to the Design Review Committee’s (DRC) responsibilities to review signage for the Building Department, the DRC may also provide design assistance for signage applicants that request additional assistance or design resources. Signage design must comply with the dimensional, usage, placement, and other regulations contained in the *Sign Regulations* of the *Zoning Bylaw*. The guidelines focus on well-designed signage in the context of compliance with applicable regulations.

1. Principles and Intentions

Signage for residential communities should identify the community, promote a positive image, harmonize with its surroundings, and respect Medway’s New England village character.

- a) **Reinforce New England Village Character** – Sign design should be appropriate to Medway’s New England village character, through use of historic, muted colors; traditional-style sign face materials such as wood or wood composite; and lettering that is painted, gold foil stamped, carved dimensionally, vinyl cut, sand-blasted or etched, or metal channel-cut.
- b) **Emphasize Legibility and Clarity** – A sign should be readable, simple, and legible, with careful consideration of the proportion of lettered and graphic areas to the overall size and location of the

sign and consideration of the purpose and intended audience of the sign. Signage should be concise and graphically balanced.

2. Sign Harmony

- a) **Reinforce Compatibility with Context** – Signs should be designed to be compatible with neighboring properties, storefronts and buildings. Compatibility should be considered through sign style, height, type, scale and location.
- b) **Reinforce Compatibility with Residential Context** – Where business uses are interspersed with residential uses, signs should be designed and located with sensitivity to the residential areas. Illumination should be designed to minimize impact on adjacent residences.
- c) **Coordinate Compatibility with Building Architecture** – Sign design and placement should relate to and harmonize with the building architecture. Signs should not overwhelm or obscure building features.



Figure 103: Sign design should be compatible for a residential context and follow a traditional New England village character

3. Sign Characteristics

- a) **Focus Signage Design and Readability** – A sign should be readable, simple, and legible, with sign content that should fit comfortably within the space it will occupy on the building. Generally, a sign’s text and graphic elements should not occupy more than two-thirds of the sign panel area.
- b) **Focus Signage Content** – Signage messaging should be simple and brief. Signage should communicate only the name of the residential community and/or wayfinding within the community. Signage may also include home occupation businesses that should be respectful of the residential context.
- c) **Use Signage Color** – Signage color should complement building materials and color palette.
- d) **Coordinate Signage Materials** – Signage materials should be selected for durability, ease of maintenance, and compatibility with building materials and design



Figure 104: Signage for home occupation businesses should be simple and integrated into the residential context

4. Site Signage

- a) **Integrate Signage Design with Landscape** – Site signage should be integrated with site landscape design and be used to reinforce gateway locations and site entry points. Landscape plantings should be included to anchor and integrate signage into the site plan. Refer to *Sign Regulations* in *Zoning Bylaw*.
- b) **Simplify Development Signage** – Signage should indicate the overall residential development and community name or branding features. This type of signage should be balanced and in scale with both the overall scale of the development and the surrounding context. Signage for individual single-family and two-family homes is discouraged unless they are part of an agricultural business.
- c) **Minimize Wayfinding Signage** – Simple directional signage may be provided on the site to inform visitors of entries, parking areas, or other information. Wayfinding signage should be consistent and compatible with other development signage. Wayfinding signage should not obstruct or cause conflict with regulatory or traffic-related signage.



Figure 105: Sign design indicates overall residential community and is integrated with landscaping



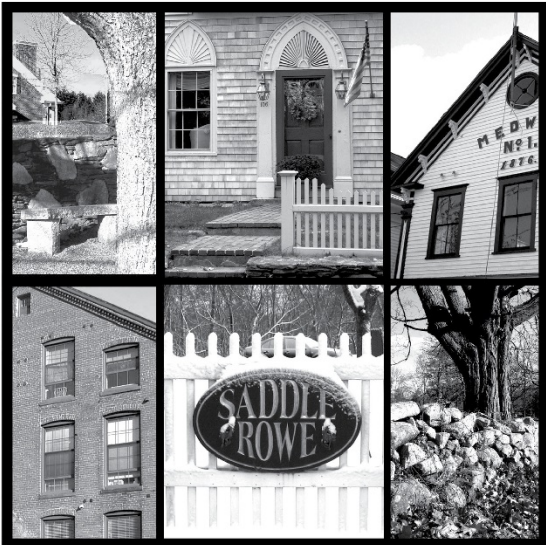
Figure 106: Residential wayfinding signs are consistent with the residential character

5. Sign Illumination

External signage illumination is encouraged and should be targeted only onto the sign, not onto adjacent buildings or towards vehicles or pedestrians.

- a) **Minimize Signage Lighting Fixtures** – External lighting fixtures that project the light from above or below the sign are strongly encouraged. Light fixtures should be simple and unobtrusive, and should not obscure the sign's message and graphics. Raceways, conduits and other electrical components should be concealed from public view.
- b) **Provide Consistent Lighting Levels** – Lighting should provide a consistent and even wash of light across the sign.

Medway, Massachusetts, August 2015



Section 5. Glossary of Terms

**TOWN OF MEDWAY
Design Review Guidelines**

SECTION 5. GLOSSARY OF TERMS

This Glossary has been prepared to explain terms used in the *Design Review Guidelines* that may be technical in nature or specific to Medway. Other terms and definitions are part of the *Zoning Bylaw*.

Adult Retirement Community Planned Unit Development (ARCPUD) – A master-planned development of land as a unified, self-contained residential community, constructed expressly for use and residency by persons who have achieved a minimum age requirement for residency of fifty-five years of age or older and also incorporating the preservation of natural open space areas as an integral element of the development. An ARCPUD shall be permitted only in an Adult Retirement Community Overlay District and only upon the granting of a special permit by the Planning and Economic Development Board.

Applicant – The person or entity having the legal authority and who is seeking a permit or approval from the Town of Medway to construct or use property subject to the provisions of this *Zoning Bylaw*, or the authorized agent of any such person or entity.

Awning – A sheltering or covered frame, often of fabric, either stationary or on a retractable system attached to a structure. The awning does not receive stanchion support as in a canopy.

Awning or Canopy Sign – A sign painted, stamped, perforated, stitched, or otherwise applied on an awning, canopy or marquee, including backlit signs.

Buffer – Landscape or fencing that is used to screen and/or mitigate the impacts of utilitarian elements of a building or site, such as dumpsters, loading areas, or mechanical equipment.

Buffer Zone – Land which is maintained in either a natural or landscaped state, and is used to screen and/or mitigate the impacts of development on surrounding areas, properties or rights-of-way.

Building – An independent structure having a roof supported by columns or walls resting on its own foundations and designed for the shelter, housing, or enclosure of persons, animals, chattels, or property of any kind.

Building Height – The vertical distance from grade to the top of the highest point of the roof or structure.

Business – Any lawful commercial endeavor to engage in the purchase, sale, lease, exchange or provision of goods and for the provision of services or instruction.

Canopy – A sheltering or covered frame, often of fabric, which is attached to a structure at the inner end and receiving stanchion support at the outer end.

Common Driveway – A privately owned driveway, paved or not, providing vehicular access between two or more buildings and a street. A common driveway does not serve as legal frontage for a lot.

Compatible – A visual and aesthetic consideration that allows two parts to exist or occur together without conflict or diminishment of the other part.

Dark-Sky – An international initiative, embraced by the Medway Bylaw, to reduce light pollution intending to increase the number of stars visible at night, reduce the effects of unnatural lighting on the environment and cut down on energy usage. The dark-sky movement encourages the use of full-cutoff light fixtures that cast little or no light upward in public areas.

Design Review Committee (DRC) – A committee appointed by the Planning and Economic Development Board to assist and advise the Board and its applicants with regard to the review of applications for site plans, special permits, sign permits, scenic road work permits and other development proposals.

Development Sign – A permanent, freestanding sign used to display the name and address of a multi-tenant development as defined herein.

Directory Sign – A sign that identifies the names and/or locations of establishments in a multi-tenant building or multi-tenant development. A directory sign may be attached to a building or structure. A directory sign may also be a freestanding sign placed along a road or access way leading to multi-tenant building or through a multi-tenant development, or in a pedestrian area.

Direction Sign – A sign identifying on-premises traffic, parking or other functional activity, which bears no language or symbols for business identification or advertising.

Dormer – A roof-covered projection from a sloped roof. A window set in a small gable projecting from a roof.

Eave – The projecting lower edges of a roof overhanging the walls of a building.

Entablature - The upper panel of moldings and bands which lie horizontally above columns. Entablatures are important elements of classical architecture. They are a common area to provide the most prominent signage for a building.

Establishment – A separate and distinct use, business, enterprise, institution, or organization occupying space within a building.

Façade – The substantially vertical exterior surface of a building or structure exposed to public view.

Fence – Any artificially constructed barrier of any material or combination of materials used as a boundary, or erected to prevent intrusion, or to enclose, buffer or screen areas of land.

Frontage – That portion of a lot which fronts on a street or streets from which physical access to the principal building on the lot can be provided. Frontage is measured as the distance between the points of intersection of the side lot lines with the front lot line. In the case of a corner lot bounding more than one street, the measurement on both streets may be used to determine if the lot meets the minimum frontage requirements of the particular zoning district. With a corner lot, the frontage is measured from the side lot line to the midpoint of the arc that constitutes the corner rounding at the intersection of the two streets.

Full-cutoff Light Fixture – A light fixture that casts little or no light upward.

Gable – The vertical surface that connects two or more sloped roofs. The triangular shaped wall section formed by the two slopes of a roof.

Ground Floor - That building floor which is substantially level with the exterior grade of the lot at the main entrance to a structure.

Human-scaled – The proportional relationship of a particular building, structure or streetscape element to the human form and function.

Impervious Surface – Material or structure on, above, or below the ground that does not allow precipitation or surface water to penetrate directly into the soil.

Landscaped Area – The part or parts of a lot developed and permanently maintained in grass and other plant materials, in which the space is open to the sky and is free of all vehicular traffic, parking, loading and outdoor storage.

Landscape Maintenance Plan – A document that describes the intentions and specifications for maintaining landscape to be installed as part of a development including pest management, irrigation, fertilization, mulching, pruning, staking and seeding requirements to establish and enhance the health of installed landscape.

Lot – A single area of land in one ownership defined by bounds or boundary lines in a recorded deed or shown on a recorded plan.

Lot Frontage – The length of a lot line(s) measured at the street right-of-way line.

Lot Line – A line of record bounding a lot that divides one lot from another lot or from a way or any public space.

- Lot Line, Front – A lot line separating a lot from a street right-of-way
- Lot Line, Rear – A line separating a lot from other lots or from land in a different ownership, being the boundary of a lot which is opposite or approximately opposite the front lot line.
- Lot Line, Side – Any lot line other than a front or rear lot line.

Low Impact Development (LID) – A term used to describe land planning and engineering design approaches that manage stormwater runoff with an emphasis on conservation, use of on-site natural features, and the protection of water quality.

Mansard – A roof having a double slope on all four (4) sides, the lower slope being much steeper. A partial mansard facade consists of the lower slope on one (1) or more sides, with no direct relationship to the upper roof.

Massing – The overall form of a building, its physical bulk and volume as it relates to the site.

Master Signage Plan – A written and graphic document, submitted during the Site Plan process and reviewed by the Design Review Committee that portrays a coordinated signage scheme for all signs for a building that contains two or more establishments, or a multi-tenant development. A Master Signage Plan shall address sign type, design, location, dimensions, surface area, materials, and lighting.

Monument Sign – A sign, other than a pole sign, with a lower overall height in which the entire bottom is in contact with or close to the ground, independent of any other structure.

Multi-Tenant Development – A group of two or more establishments located in one or more buildings on one or more lots of land under single or multiple ownership, that is designed, planned, constructed or managed as a single entity, with customer and employee parking provided on-site. This includes but is not

limited to what is commonly understood and recognized to be a shopping center, office park, or industrial park.

Neck-Down – Also, referred to as a curb extension. A traffic calming measure that extends the curb into the street at an intersection to reduce the pedestrian crossing distance.

Open Space Residential Development (OSRD) – purposes to preserve open space, agricultural and forestry land, viewsheds, wildlife habitat and corridors, wetlands and water resources, and historical and archeological resources; minimize the total amount of disturbance on a site; encourage more efficient development that consume less open land and respects existing topography and natural features; encourage flexibility and creativity in the design of residential developments; and through flexible design and more efficient use of land, facilitate the provision of a variety of housing opportunities in the Town.

Parapet – A low wall or railing that extends above the roof of a building.

Pedestrian-oriented – Describes an approach to circulation or accommodation in which the pedestrian is the primary consideration.

Pedestrian-scale – The relationship between an individual and his or her environment whether natural or built which contributes to an individual’s comfort and sense of accessibility.

Pier - An upright support for a superstructure, such as an arch or bridge. Specific to facades, it often refers to a raised column-like element used to frame windows or bays.

Planning and Economic Development Board (PEDB) – The Planning and Economic Development Board is the approving authority that that reviews subdivision plans, applications for certain special permits and other site plans for future development for consistency with the *2009 Medway Master Plan* and the *Site Plan Rules and Regulations*.

Projecting Sign – A sign, other than a wall sign, affixed to a building or wall in such a manner that its leading edge extends more than eight inches beyond the surface of such building or wall. Projecting signs include but are not limited to awning/canopy sign, banner, marquee sign and suspended sign. Projecting signs are also referred to as blade signs.

Residential Development Sign – A permanent sign positioned at the entrance to a residential neighborhood such as a single-family subdivision, multi-family apartment, or condominium complex.

Roof – The primary outside protective covering of the top of a building. This includes but is not limited to hip, gable, flat, gambrel, mansard, and shed roof types. Roof shall also mean the exterior protective covering affixed to the top of all other elements projecting from a building façade or its roof including but not limited to porches, dormers, or other similar appurtenances.

Setback – The distance between a structure and any lot line.

Sign or Signage – Any object, design, device, display or structure intended for public view from outside a building, used by a private or public entity to identify, announce, advertise or direct attention to any place or location, object, business, institution, organization, profession, merchandise, product, activity, service, event, person, idea or statement, or to communicate information of any kind to the public by any means including words, letters, figures, designs, pictures, symbols, fixtures, colors, and illumination. Sign shall

mean and include any permanent or temporary structure, models, objects, banners, pennants, insignias, trade flags, or other representations that are on a public way or on private property within public view from a public or private street, way or parking area. Any exterior structural surface that is internally or indirectly illuminated or decorated with gaseous tubes or other lights shall be considered a sign.

Site Plan – A scaled illustration depicting the planned layout of buildings, parking, driveways, sidewalks, landscape, stormwater facilities and other features of the lot. The site plan is one element of the required information of a site plan submittal or application.

Street – See definitions below:

- A public way or way which the Town Clerk certifies is maintained and used as a public way.
- A way shown on a definitive subdivision plan approved and endorsed under the Subdivision Control Law and recorded with the Norfolk County Registry of Deeds that is constructed or secured through a covenant or suitable performance guarantee.
- A way already physically in existence on the ground when the Subdivision Control Law become effective in Medway and having, in the opinion of the Planning and Economic Development Board, adequate width, construction, and grades for the needs of vehicular traffic for the existing and future buildings and uses abutting thereon or to be served thereby.

Streetscape – The collection of elements that constitute the physical makeup of a street and that, as a group, define its character including building frontage, street paving, street furniture, landscaping, open space areas and lighting.

Structure – Anything constructed or erected at a fixed location on the ground to give support or to provide shelter.

Vista – A unique view to or from a particular point through a passage or opening in a feature of a building or site.

Wall Sign – A sign which is permanently affixed to the façade of a building or structure, or to its porch, canopy, awning, such that its exposed face and all sign surface areas are parallel or approximately parallel to the plane of the building or wall to which it is attached or mounted.

Wayfinding – Wayfinding signage refers to a family of signage products created for the purpose of directing people to & from a defined area, all while guiding them through paths, marking destinations reached, and providing both essential and commercial instructions and data along the way.

Yard – Any open space on the same lot with a principal building, unoccupied, and unobstructed from the ground to the sky, except for accessory buildings or structures, or such projections as are expressly permitted in zoning regulations. A yard lies between the principal building and the lot lines.

Zoning District – The basic unit in zoning. A portion of land in a community to which a uniform set of regulations applies, or a uniform set of regulations for a specific use.

Medway, Massachusetts, August 2015



Section 6. References/ Resources

**TOWN OF MEDWAY
Design Review Guidelines**

SECTION 6. REFERENCES AND RESOURCES

A. Town of Medway Resources

1. *Zoning Bylaw* and Zoning Map

The Town of Medway *Zoning Bylaw* and *Zoning Map* are available online at the Town's website. As of the date of this publication, the Medway *Zoning Bylaw* was updated June 2015 and the Medway Zoning Map was updated December 1, 2014. Check the Town of Medway website for the most up-to-date information. The Zoning Map is included on the following page and available at the following website:

http://www.townofmedway.org/Pages/MedwayMA_Bcomm/PlanEcon/ZBL&M

2. Town of Medway Historic Districts

The Town of Medway includes two historic districts that are designated as National Historic Districts, the Rabbit Hill Historic District and the Medway Village Historic District. More information about each district is available at the Medway Historical Society website.

Rabbit Hill Historic District (designated in 1988):

<http://www.medwayhistoricalsociety.org/rabbit-hill.html>

Medway Village Historic District (designated in 2008):

<http://www.medwayhistoricalsociety.org/medway-village.html>

3. Town of Medway Development Handbook

The Town of Medway prepared a guide to the development process for the Town in 2008. Available from the Medway Department of Community and Economic Development.

http://www.townofmedway.org/Pages/MedwayMA_Bcomm/EcDev/handbook.pdf

4. Other Town of Medway Contact Information and Websites

Medway Design Review Committee (DRC)

Email: drc@townofmedway.org

Phone: (508) 533-3291

http://www.townofmedway.org/Pages/MedwayMA_Bcomm/Design/index

Medway Planning and Economic Development Board (PEDB)

Email: planningboard@townofmedway.org

Phone: (508) 533-3291

http://www.townofmedway.org/Pages/MedwayMA_Bcomm/PlanEcon/index

Susan E. Affleck-Childs
Medway Planning and Economic Development Coordinator
Email: sachilds@townofmedway.org
Phone: (508) 533-3291

Department of Community and Economic Development
http://www.townofmedway.org/Pages/MedwayMA_CommEconDev/index

Stephanie Mercandetti
Director of Community and Economic Development
Email: smercandetti@townofmedway.org
Phone: (508) 533-3253

Building Department
http://www.townofmedway.org/Pages/MedwayMA_Build/index

Jack Mee
Building Commissioner
Email: jmee@townofmedway.org
Phone: (508) 533-3253

B. Architectural Styles

The following resources provide a more extensive narrative and cataloguing of the variety of architectural styles that have been historically associated with a New England village style character of buildings:

Poppeliers, John, S. Allen Chambers, and Nancy B. Schwartz. *What Style is it?: A Guide to American Architecture*. Preservation Press: National Trust for Historic Preservation, Washington, D.C., 1983.

Fleming, John and Honor, Hugh and Pevsner, Nikolaus. *The Penguin Dictionary of Architecture & Landscape Architecture*. Penguin Books, 2000.

C. Dark-sky Lighting Resources

The International Dark-Sky Association is a non-profit organization with several resources on their website relating to site and building lighting best practices:

<http://www.darksky.org/>

D. Low Impact Development (LID)

United States Environmental Protection Agency Low Impact Development (LID) resources. Available at: <http://water.epa.gov/polwaste/green>

Massachusetts Smart Growth/Smart Energy Toolkit. Available at: http://www.mass.gov/envir/smart_growth_toolkit/pages/mod-lid.html

E. Rehabilitation and Preservation Standards

United States Department of Interior Regulations, 36 Code of Federal Regulations 67, *Secretary of the Interior's Standards for Rehabilitation*. Available at: <http://www.nps.gov/tps/standards/rehabilitation.htm>

F. Principles of Site Design

The following resource provides practical land use planning and site design techniques to preserve open space and community character.

Arendt, Randall. *Rural by Design: Maintaining Small Town Character*. APA Planners Press, 1994.

G. Principles of Sign Design

The following resources provide practical signage and wayfinding design resources.

Calori, Chris. *Signage and Wayfinding Design: A Complete Guide to Creating Environmental Graphic Design Systems*. John Wiley & Sons, Inc., 2007.

Uebele, Andreas. *Signage Systems and Information Graphics: A Professional Sourcebook*. Thames & Hudson., 2010.

Town of Medway, MA Zoning Map

Zoning District

- AR-I Agricultural-Residential I
- AR-II Agricultural-Residential II
- BI Business-Industrial
- C-I Commercial I
- C-III Commercial III
- C-IV Commercial IV
- C-V Commercial V
- IND-I Industrial I
- IND-II Industrial II
- IND-III Industrial III
- AUOD Adaptive Use Overlay District
- Medway Mill Conversion Subdistrict
- Multifamily Housing Overlay District
- Groundwater Protection District

Other Features

- Medway Parcel Outlines

ZONE	Minimum lot size (sq ft)	Minimum frontage (ft)	Setbacks: front, side, rear (ft)
AR-I	44,000	180	35, 15, 15
AR-II	22,500	150	35, 15, 15
BI	20,000	100	35, 15, 15
C-I	20,000	100	50, 25, 25
C-III	20,000	100	35, 15, 15
C-IV	20,000	100	35, 15, 15
C-V	20,000	100	50, 15, 15
IND-I	20,000	100	30, 20, 30
IND-II	20,000	100	30, 20, 30
IND-III	40,000	100	30, 20, 30

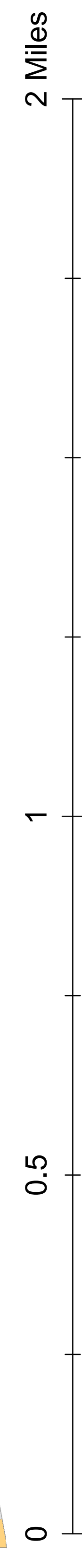
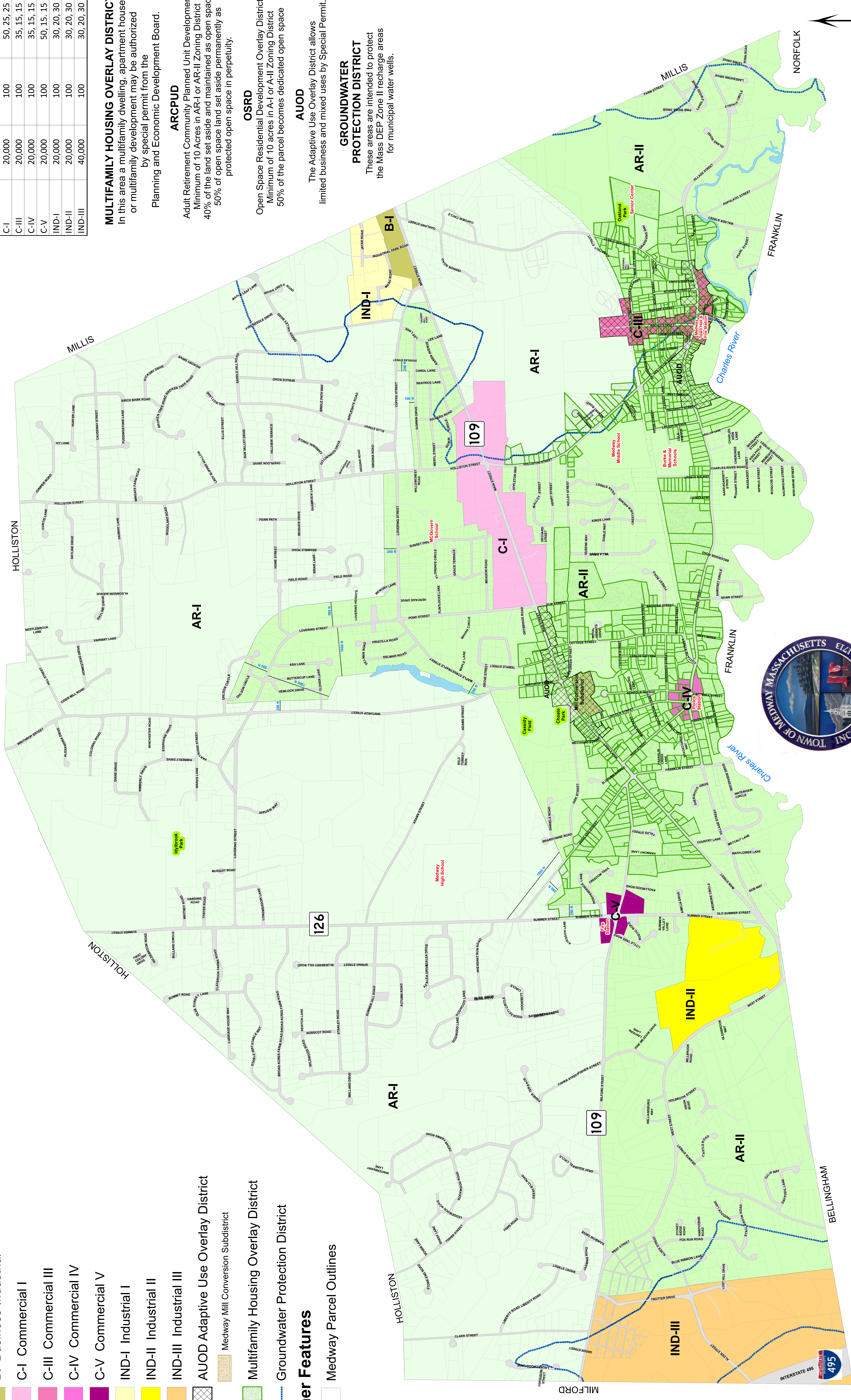
MULTIFAMILY HOUSING OVERLAY DISTRICT
 In this area a multifamily dwelling, apartment house, or multifamily development may be authorized by special permit from the Planning and Economic Development Board.

ARCPUD
 Adult Retirement Community Planned Unit Development
 Minimum of 10 Acres in AR-I or AR-II Zoning District
 40% of the land set aside and maintained as open space
 50% of open space land set aside permanently as protected open space in perpetuity.

OSRD
 Open Space Residential Development Overlay District
 Minimum of 10 acres in A-I or A-II Zoning District
 50% of the parcel becomes dedicated open space

AUOD
 The Adaptive Use Overlay District allows limited business and mixed uses by Special Permit.

GROUNDWATER PROTECTION DISTRICT
 These areas are intended to protect the Mass DEP Zone II recharge areas for municipal water wells.



Prepared for the Medway Planning & Economic Development Board
 155 Village Street, Medway, MA 02053
 508-533-3291
 Data provided by planningboard@townofmedway.org
 Town of Medway and MassGIS

Revised by *JTrazn V. Hutton Lee*, July 9, 2015
 as authorized by the May 11, Medway Town Meeting

The information on this map is believed to be correct but errors in data entry or transmission may occur. The map is not to be used for legal purposes. The information on this map is subject to change or revision at any time.

AGENDA

ITEM #7

**Discussion- Agricultural Committee
(General Bylaws, Art. II)**

Associated back up materials attached:

- Bylaw (approved by 2010 ATM)

Section 2.21 - Agricultural Committee

(a) There shall be established a Medway Agricultural Committee consisting of at least 5 and no more than 9 individuals.

(b) The mission of the Medway Agricultural Committee is to encourage the pursuit of agriculture in Medway by working to preserve, revitalize and sustain Medway's agricultural enterprises, rural character and farming traditions. The Committee may undertake any of the following activities to accomplish this mission:

1. Act as a spokesman on behalf of the Medway farming community.
2. Advise the Town's elected officials, appointed boards/committees, Town staff and the community at large, by request or on its own initiative, on issues, projects and activities relating to agricultural lands and farming in the community, including land acquisitions and other land transactions. Develop, propose and comment on warrant articles, Town policies and procedures, and rules & regulations from the perspective of advocating and promoting agriculture concerns. This advice may include the presentation of oral or written testimony at public meetings and hearings before any Town Board or Committee.
3. Serve as representatives, advocates, educators, facilitators and/or mediators in the resolution of disputes relating to agricultural issues within the community. Any such recommendations shall be advisory and any such agreements are entered into voluntarily by the parties involved.
4. Promote and support agricultural based economic opportunities in the community.
5. Pursue initiatives appropriate to creating, retaining, and sustaining an agricultural community in Medway including but not limited to farm based recreational activities and agritourism opportunities.
6. Work to preserve, protect and improve private and public agricultural lands & resources.
7. Engage in projects and activities to promote farming including educational programs and special community events.
8. Perform such other duties as the Board of Selectmen may determine in response to the need to encourage agriculture in Medway or as may be authorized by other Town bylaws.

(c) The Agricultural Committee shall be appointed by the Board of Selectmen following an active recruitment process. Committee members shall serve 3 year staggered terms with one-third of the first members appointed for a 3 year term, one-third for a 2 year term and the remaining initial members appointed for a 1 year term.

Thereafter, each member shall serve for 3 years or until his successor has been appointed. Vacancies shall be filled by the Board of Selectmen. The Committee may make recommendations for persons to fill vacancies on the Committee. To maintain the staggered cycle of terms, appointments to fill vacancies shall be based on the unexpired term of the open position.

(d) Members of the Agricultural Committee shall either reside in the community or own agricultural property in Medway. A majority of the Agricultural Committee members shall be actively engaged in the business of farming or related agricultural industries. The remaining members shall have a background, expertise, or a demonstrated interest in or an affinity for farming and agricultural traditions. Non-voting associate members who are supporters or friends of Medway farming and agriculture may also be appointed for 1 year terms.

AGENDA

ITEM #8

Discussion with Capital Improvement Planning Committee – FY17-21 Capital Plan

Associated back up materials attached:

- Summary of FY17 requests
- Summary of FY17-21 requests

SUMMARY
FY 2017 ONLY

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Council On Aging

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
BU	RPR	REPAIR WEST DOOR & THRESHOLD	GFD	\$12,000	\$0	\$0	\$0	\$0	\$12,000
Project No.: 1730 Facility: SR CENTER Dept Rank: 5 In-Service Date: 01-Nov-2016 Last Update: 20-Oct-2015			Justification: Door and frame has had damage over the past few winters. The concrete threshold heaves in freezing weather causing damage to the door, door frame, and interior walls. Repairs will be made to stop the heaving and replace the doors.						
Dept. Totals:				\$12,000	\$0	\$0	\$0	\$0	\$12,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

DPS - Building Maintenance

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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INF	RPR	GENERAL TOWN WIDE FACILITY IMPROVEMENTS	GFD	\$50,000	\$25,000	\$25,000	\$25,000	\$25,000	\$150,000
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Project No.: 1217b **Facility:** MUNICIPAL **Dept Rank:** 4 **In-Service Date:** 30-Jun-2016 **Last Update:** 11-Sep-2015

Justification: Similar to strategy used by corporations, this establishes a facilities improvement/replacement budget for nominal unanticipated repair needs that occur during the year. Current list is attached. If not funded, some projects must be done and must be included as separate CIPC items or operating budget.

Dept. Totals: \$50,000 \$25,000 \$25,000 \$25,000 \$25,000 \$150,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

DPS - Highway/Admin

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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BU	RPL	CONSTRUCT NEW DPS FACILITY	MXD	\$10,000,000	\$0	\$0	\$0	\$0	\$10,000,000
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Project No.: 0804a **Facility:** DPS FACILITY **Dept Rank:** 5 **In-Service Date:** 01-Sep-2016 **Last Update:** 02-Sep-2015

Justification: Consolidates all DPW operations into one facility. Current buildings are poor structures that have exceeded useful life. Rear of highway building is collapsing. Base of building is rotting away and is no longer weather tight. Water station is a turn of the century structure in very poor condition. It has no bathroom or locker room facilities. Overall building sizes are too small for current operations. Building support systems have deteriorated beyond effective repair operations. Building support systems have deteriorated beyond effective repair. Direct savings attributed to reduced repair costs for various facilities and energy savings. Additional direct savings from stopping lease agreements. Indirect savings attributed to consolidated operations, better communication and coordination, reduced travel time for equipment staging.

EV	RPL	REPLACE DUMP TRUCK (212-1999 INT)	GFD	\$165,000	\$0	\$0	\$0	\$0	\$165,000
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Project No.: 1420 **Facility:** **Dept Rank:** 5 **In-Service Date:** 30-Jan-2016 **Last Update:** 03-Sep-2015

Justification: TRUCK 2 is 1999 model reaching the end of its useful life. This vehicle is used regularly to perform normal construction and maintenance activities, as well as, snow plowing operations. Maintenance and repair costs are expected to increase and loss of the vehicle would require use of more contractors to supplement it especially in winter months.

EV	NEW	PURCHASE MEDIUM DUMP TRUCK	GFD	\$70,000	\$0	\$0	\$0	\$0	\$70,000
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Project No.: 1615 **Facility:** **Dept Rank:** 5 **In-Service Date:** 30-Jan-2017 **Last Update:** 03-Sep-2015

Justification: To be primarily for the Environmental Services Division for parks maintenance. Another vehicle to support expanded operations is needed. A medium sized truck to haul mulch and loam can increase efficiency by being able to deliver larger loads to the job site. Our large trucks are too heavy and damage the fields. The need for a vehicle in this size range is regular and recurring.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes

TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

FY'2017 ONLY

DPS - Highway/Admin

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EV	RPL	REPLACE FLEET MAINT TRUCK (FM-1)	GFD	\$78,000	\$0	\$0	\$0	\$0	\$78,000
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Project No.: 1213 **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Apr-2016 **Last Update:** 11-Sep-2015

Justification: Vehicle Maintenance is a 2001 model. Truck is reaching the end of its useful life. The existing truck is a repurposed vehicle from the Water Division. A new truck will be purpose built as a fleet field support vehicle allowing staff to perform numerous types of repairs in the field that are now, not possible. Towing charges ranging from \$100 to over \$500 per vehicle will be reduced as we would begin servicing more of these breakdown on location. The vehicle would also be outfitted to provide other infrastructure support such as welding and lift capabilities.

EV	NEW	PURCHASE THREE USED TRUCKS/VANS	GFD	\$39,000	\$0	\$0	\$0	\$0	\$39,000
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Project No.: 1706 **Facility:** **Dept Rank:** 4 **In-Service Date:** 20-Jul-2016 **Last Update:** 03-Sep-2015

Justification: Between new employees on an on-going basis (one in Building Maintenance and two Water/Sewer), seasonal help where six employees are added for the Summer months, and added facilities to cover for the School grounds, DPS does not have enough small light vehicles to perform routine tasks. Addition of three more pick-up trucks or vans will allow us to more efficiently perform our work. These vehicles are not needed for plowing and new vehicles are not required.

INF	RPL	REPLACE STREETLIGHTS W/LED TECHNOLOGY	GFD	\$140,000	\$0	\$0	\$0	\$0	\$60,000
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Project No.: 1551 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Oct-2015 **Last Update:** 11-Sep-2015

Justification: Project is based on economics and "Green Community" status. LED streetlights would replace existing High Pressure Sodium lights throughout the remainder of the Town. Summer, Holliston, Village, Milford, Main, and Winthrop Streets have been changed to LED as part of the Green Community grant work. About 340 lights remain to be changed. The vendor estimates about a 6 year payback on the work saving about 65,000 kwh annually valued at approx. \$14,000. LED technology also reduces maintenance costs as the fixture have a longer time between failure.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

DPS - Highway/Admin

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
EV	NEW	PURCHASE MINI-EXCAVATOR / TRAILER	MXD	\$77,000	\$0	\$0	\$0	\$0	\$77,000
Project No.: 1503 Facility:			Dept Rank: 3	In-Service Date: 01-Sep-2015		Last Update: 11-Sep-2015			
Justification: Provides ability to perform a variety of small dig jobs with low ground pressure and improved efficiency. Most small utility contractors use this type of machine to supplement larger equipment. DPS has a single backhoe and both Water and Highway vie for time. This unit gives both Divisions access to another machine. Low ground pressure with this machine also allows it to provide service where the backhoe cannot such as wet areas, parks in the spring, field use, etc. Over the past two years, the Department has rented a excavator spending about \$10,000 annually to support extra workload.									
EO	NEW	PURCHASE BOOM MOWER	GFD	\$20,000	\$0	\$0	\$0	\$0	\$20,000
Project No.: 1727 Facility:			Dept Rank: 3	In-Service Date: 01-Aug-2016		Last Update: 15-Sep-2015			
Justification: Demands for roadside mowing and maintenance are increasing. DPS currently has two sidewalk tractors and a single boom mower for roadside cutting. Addition of a second boom will allow both machines to be in service during the summer and provide back-up in case of a failed mower.									
LPF	NEW	MEDWAY ARBOR BEAUTIFICATION	GFD	\$15,000	\$15,000	\$15,000	\$15,000	\$15,000	\$75,000
Project No.: 1227 Facility:			Dept Rank: 2	In-Service Date: 30-Jun-2016		Last Update: 11-Sep-2015			
Justification: Medway is noted for its rural charm. Over the past several years, through many natural causes, the Town has lost many trees along its roadways. This project proposes to plant new trees generally in areas of heavy decay or where roadwork is prevalent to maintain/restore Medway's scenic appeal.									
Dept. Totals:				\$10,604,000	\$15,000	\$15,000	\$15,000	\$15,000	\$10,584,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

DPS - Parks

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
EO	RPL	REPLACE LEAF BOX VACUUM	GFD	\$57,000	\$0	\$0	\$0	\$0	\$57,000
Project No.: 1712 Facility: Justification: Existing unit is undersized for new responsibilities in parks, schools, and other facilities. Can also be used by Highway for roadside maintenance.			Dept Rank: 4 In-Service Date: 01-Sep-2016 Last Update: 11-Sep-2015						
LPF	RPL	REPLACE OAKLAND BASKETBALL COURT LIGHTS w/LED	GFD	\$40,000	\$0	\$0	\$0	\$0	\$40,000
Project No.: 1713 Facility: OAKLAND Justification: Energy savings at the park and longer use possible. Remote programming is desirable.			Dept Rank: 3 In-Service Date: 01-Sep-2016 Last Update: 11-Sep-2015						
LPF	RPL	REPLACE SMALL MOWER (KABOTA Z)	GFD	\$17,000	\$0	\$0	\$0	\$0	\$17,000
Project No.: 1507 Facility: Justification: Small landscapers riding lawn mower is used daily for various Town properties where other equipment is too large. Existing unit is a 2007 and is reaching the end of it's useful life.			Dept Rank: 3 In-Service Date: 01-Mar-2016 Last Update: 11-Sep-2015						
EO	RPL	REPLACE LAWN TRACTOR	GFD	\$15,000	\$0	\$0	\$0	\$0	\$15,000
Project No.: 0815 Facility: Justification: Replace 1994 Unit used for mid-size mowing and other functions. This is a multi-purpose tractor with PTO assemble, but on a smaller scale for use on Town properties where the other mowers/tractors are not feasible to use.			Dept Rank: 3 In-Service Date: 01-Apr-2016 Last Update: 11-Sep-2015						
Dept. Totals:				\$129,000	\$0	\$0	\$0	\$0	\$129,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

DPS - Road Repair

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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INF MAJ [VARIOUS ROAD/SIDEWALK IMPROVEMENTS](#) GFD \$750,000 \$750,000 \$750,000 \$750,000 \$750,000 \$3,750,000

Project No.: 0818g **Facility:** **Dept Rank:** 5 **In-Service Date:** 30-Jun-2017 **Last Update:** 11-Sep-2015

Justification: Repair/Reconstruct roads and sidewalks in poor condition as part of the Towns Annual Roadway & Sidewalk Improvement Program

INF RPR [REPAIR WINTHROP ST CULVERT](#) GFD \$90,000 \$0 \$0 \$0 \$0 \$90,000

Project No.: 1705 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Oct-2016 **Last Update:** 03-Sep-2015

Justification: While performing water main installation crossing the existing culvert, several severe deficiencies and voids were found at the culvert. The original corrugated steel culvert is heavily pitted and rotted. Extensions added to widen the street at some point in the past were not well sealed. These areas have voids at the joints with serious leakage. This is eroding the road sub-base and leading to road failure. The proposed repair if possible uses liners that do not require removal or excavation of the existing culvert.

Dept. Totals: \$840,000 \$750,000 \$750,000 \$750,000 \$750,000 \$3,840,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

DPS - Water

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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INF RPL [REPLACE WATER MAINS - BRENTWOOD AREA /HOLLIST](#) EFW \$1,000,000 \$100,000 \$1,100,000 \$0 \$0 \$2,200,000

Project No.: 1614a **Facility:** **Dept Rank:** 4 **In-Service Date:** 30-Sep-2016 **Last Update:** 02-Sep-2015

Justification: Strategic improvements to water system at Brentwood area (Meryl, Gorwin, Virginia, and area streets) require line replacement to provide service reliability, firefighting capabilities and high quality drinking water. Recommended in the Town's Water Master Plan.

EO NEW [PURCHASE GATE VALVE VACUUM BOX](#) EFW \$65,000 \$0 \$0 \$0 \$0 \$65,000

Project No.: 1410a **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Aug-2016 **Last Update:** 03-Sep-2015

Justification: The original authorization for \$15,000 was to provide a tool to clean gate valve stem boxes in right-of-way. Poor cleaning results in valve stem damage and failure over time. Cleaning the stem boxes routinely during annual maintenance of hydrants and before/after road work will lengthen service life of vales and result in fewer emergency replacements of failing gate valves. The unit could also provide a limited amount of service for sewer or catchbasin cleaning, however, it does not replace a full size vactor truck.
 AMENDED-new units are now available that excersize valves and clear sticking valves without the need to excavate. The Department has not had a valve exercise program, although much of this work is performed during annual flushing. A more comprehensive program is needed to ensure system integrity and allow for minimal system outages and disturbances.. During a demonstration, a similar unit opened a stuck valve that was scheduled for dig and replacement. Estimated savings for the single valve in questions was about \$5,000. Eight to ten valves per year are scheduled for replacement. This unit could eliminate at least 1/3 of those replacements saving \$15,000 annually.

EV RPL [REPLACE TRUCK \(415 2004 CHEVY SILVARADO\)](#) EFW \$58,000 \$0 \$0 \$0 \$0 \$58,000

Project No.: 1423 **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Oct-2016 **Last Update:** 03-Sep-2015

Justification: W-5 is 2004 model used daily in DPS operations.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

DPS - Water

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EV	RPL	REPLACE TRUCK (402-2005 CHEVY COLORADO)	EFW	\$38,000	\$0	\$0	\$0	\$0	\$38,000
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Project No.: 1421 **Facility:** **Dept Rank:** 4 **In-Service Date:** 30-Jan-2017 **Last Update:** 03-Sep-2015

Justification: W-4 is 2005 small pick-up, standard transmission and not of great value to support DPS operations. Could be potentially repurposed to Parks, but given limited number of operators, recommend trade in. The Water Division has a need for a larger vehicle with a cap and tool boxes or internal storage for day-to-day operations. Currently looking at Transit vans.

Dept. Totals: \$1,161,000 \$100,000 \$1,100,000 \$0 \$0 \$2,361,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
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 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Econ./Community Development

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
LPF	NEW	OAK GROVE PARK LAND ACQUISITION	OLG	\$74,400	\$785,850	\$0	\$0	\$0	\$1,035,250
Project No.: 1612 Facility: Dept Rank: 5 In-Service Date: 02-Jun-2017 Last Update: 11-Sep-2015				Justification: Total annual tax revenue estimate at build-out = \$1,156,345. Added \$175,000 in legal expenses. No figure available.					
Dept. Totals:				\$74,400	\$785,850	\$0	\$0	\$0	\$1,035,250

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Fire

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EV RPL [REPLACE AMBULANCE \(A-2\)](#) GFD \$260,000 \$0 \$0 \$0 \$0 \$260,000

Project No.: 1542 **Facility:** **Dept Rank:** 5 **In-Service Date:** 31-Dec-2016 **Last Update:** 11-Sep-2015

Justification: Our ambulance are seeing increased service. A-2 presently has over 82,000 miles on it. It is primarily used as our second run ambulance, however, as calls increase we are experiencing multiple calls more frequently. By the time we purchase the new truck we will have in excess of 100,000 miles on the truck and it will have outlived its usefulness without spending a great deal of money to keep it running safely. Additionally, A-1 is on track to have over 80,000 miles on it by the time this truck is purchased. This purchase is part of a regular Capital Improvement Plan by the department.

EO RPL [REPLACE AIRPACKS](#) GFD \$206,000 \$0 \$0 \$0 \$0 \$206,000

Project No.: 1541 **Facility:** **Dept Rank:** 5 **In-Service Date:** 31-Dec-2017 **Last Update:** 03-Sep-2015

Justification: Our present airpacks are 11 years old. The bottles must be replaced every 15 years. Additionally, to bring the current packs up to the current NFPA Standard, it would be cost nearly as much as new packs. Typically, after 2 standard changes manufacturers do not continue to support older models, so parts are no longer manufactured and the packs can no longer be repaired if parts are not available. Also, our present packs are not compatible with surrounding communities so if we need to swap bottles on major emergencies we do not have that capabilities. Our new packs would be interoperable with our surrounding communities.

EV RPR [REPAIR ENGINE 2](#) GFD \$170,000 \$0 \$0 \$0 \$0 \$170,000

Project No.: 1707 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Nov-2016 **Last Update:** 03-Sep-2015

Justification: Engine 2 is a 2002 E-One 1,500 Gallon per Minute Pumper. It has been plagued with issues for the last several years. While they have been relatively minor issues, they have caused the truck to be out of service several times and on 2 occasions have failed at emergency scenes. The pump is showing wear and if this is not addressed catastrophic failure is possible over time. The electrical system has proven to be unreliable and was the cause for the 2 failures on emergency scenes. There is corrosion in several spots on the truck that if left unrepaired will cause more damage to the truck resulting in a higher cost to repair.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Fire

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
INF	PRL	REPLACE RADIO BOX FIRE ALARM SYSTEM	GFD	\$155,000	\$0	\$0	\$0	\$0	\$155,000

Project No.: 1538 **Facility:** **Dept Rank:** 5 **In-Service Date:** 30-Jun-2016 **Last Update:** 11-Sep-2015

Justification: Our present fire alarm reporting system used throughout the town uses 1860's technology. There are presently 4 "zones" in town for our fire alarm system. This means there is wire running though out the town that connects fire alarm system within connected buildings to our dispatch center. In the event a wire is severed this "zone" will be out of service until the broken wire is found and repaired. Quite often these breaks occur is severe weather when trees fall or are a result of a major motor vehicle accident that cause utility poles to be broken. In most cases it can be several hours, if not days, until these wires can be repaired meaning the buildings connected by the broken wire are not being monitored. In the event of a fire in one of these buildings the department would not be notified until someone sees the fire and calls the fire department via telephone. With radio boxes each building is independent of each other and if one building stops working the rest will continue to work without interruption. There is only one "receiver" at our dispatch center, and if it stops working we have no way to monitor the fire alarms connected. The present receiver is very old and often in need of repair. Parts are becoming difficult to find for the receiver. A new one will need to be purchased regardless if we don't go to radio boxes. This new system will have 2 receivers, so if one breaks the other continues to operate. In addition to the antiquated technology for transmitting the alarms, the system presently in use does not let us know if the fire alarm systems within a building are working. Also, if someone disconnects the fire alarm system in a building we will not know that as well. In some cases, for example, if a sprinkler system compressor fails, the department has no way of knowing until the air pressure in the sprinkler system falls below a certain pressure which will cause the fire alarm system to activate, causing disruption in the business and causing a full emergency response by the fire department. These cases generate needless costs to not only the business owners, but to the department as well. While responding to these accidental alarms can be harmful as well if responding personnel are involved in a motor vehicle accident. With radio boxes the department can monitor "troubles" in fire alarm systems and have one person respond non-emergency to find out what the problem is. In the case fore mentioned, we would notify the building owner who can in turn notify their sprinkler company who can rectify the problem before the sprinklers activate causing the fire alarm. There has been occasion that someone has disconnected the buildings fire alarm system from the Town's system therefore it would not transmit that alarm to the dispatcher. With our present system there is no way of knowing this unless we physically open each master box and see if it is connected. These systems are usually disconnected so the fire alarms or sprinklers can be tested without transmitting the alarm to the dispatcher. Presently, to avoid having these systems not be reconnected, the fire department will send a person to disconnect and reconnect the boxes. If there is no one available due to calls this may take quite a while to get accomplished. With the new radio boxes the dispatcher will be able to disconnect the master box from the dispatch center, eliminating the need to send someone, saving fuel and being much more efficient for the businesses. Additionally, if the master box is disconnected with our present system, as I mentioned earlier, there is no way of us knowing unless we physically look in the master box. With radio boxes the dispatcher will know instantly if someone disconnects it. With our present system, the wiring is in constant need of repair. To affect these repairs we must have someone using a bucket truck to make the repairs. Our bucket truck is about 20 years old and will soon need major expensive renovations as it is beginning to rot. With the radio box system we will not need the bucket truck.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Fire

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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Maintenance on the system will be greatly reduced, reducing the man hours required to maintain it. Another advantage to using the radio box system is panic alarms can be installed and connected to the radio boxes for schools. If there is an intruder the panic button is pushed and automatically transmits the alarm to the dispatcher who can immediately dispatch police, fire, and EMS resources. Teachers and administrators do not have to rely on cell signals or land lines to operate, the signal is transmitted immediately to the dispatcher indicating a panic alarm has been pressed.

EV	MAJ	REFURBISH BRUSH 1	GFD	\$18,000	\$0	\$0	\$0	\$0	\$18,000
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Project No.: 1539 **Facility:** **Dept Rank:** 5 **In-Service Date:** 31-Oct-2015 **Last Update:** 11-Sep-2015

Justification: This truck is a 1995 vehicle. The body on this truck is rotted. Due to the nature of the rust it is nearly impossible to fix for the long term. The vehicle itself has very low mileage, less than 10,000 miles, so it has useful life left. The pump is likely to need repair if not replaced due to the nature of the work it does. It is a safety issue. If not replaced, the body could fall off the truck. DPS has agreed to plumb the truck. An original estimate to replace the body and plumb the new pump was \$34,000. This cost will be cut in half by DPS installing and plumbing the pump.

EO	NEW	PURCHASE ENHANCED STATION ALERTING SYSTEM	GFD	\$43,000	\$0	\$0	\$0	\$0	\$43,000
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Project No.: 1543 **Facility:** **Dept Rank:** 3 **In-Service Date:** 31-Dec-2015 **Last Update:** 11-Sep-2015

Justification: Presently the only way Firefighters are alerted to emergency calls is via radio. In the event the radio goes down a phone call would have to be made to the fire station. This system will notify Firefighters to respond to emergencies via a public address system of sorts. Presently if a firefighter has take his portable radio off for whatever reason and is not in proximity of it or the speaker that is presently in use, he has the potential to miss the call. With this system no matter where they are in the station they will receive the notification. Also, this system will automatically turn on lights to illuminate egress for firefighters and the apparatus area which will reduce injuries due to Firefighters tripping because they cannot see. It will also automatically shut off the gas valve to the stove so if Firefighters are cooking while they receive a call and fail to turn the stove off before responding a fire will not start.

Dept. Totals:				\$852,000	\$0	\$0	\$0	\$0	\$852,000
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Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes

TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

FY'2017 ONLY

Information Systems

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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IT	RPL	REPLACE SCHOOL VIDEO SURVEILLANCE SYSTEMS	MXD	\$185,000	\$20,000	\$20,000	\$20,000	\$0	\$185,000
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Project No.: 1711 **Facility:** HS,MCG,MEM **Dept Rank:** 5 **In-Service Date:** 01-Sep-2016 **Last Update:** 09-Sep-2015

Justification: The High School Surveillance system was installed in 2004. The digital storage systems are now 13+ years old, outdated, and failing. Service repairs for 2016 are in excess of \$10,000. This proposal would replace the current system with the same Avigilon IP based system used for the high school fields and provide remote access for review and monitoring by other departments (police). In addition, the McGovern and Memorial surveillance systems provide inadequate coverage by today's standards and no remote capability for monitoring.

IT	RPL	TECHNOLOGY EQUIPMENT	GFD	\$350,000	\$335,000	\$250,000	\$0	\$0	\$690,000
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Project No.: 1110f **Facility:** **Dept Rank:** 4 **In-Service Date:** 30-Jun-2016 **Last Update:** 11-Sep-2015

Justification: The increase in funding is requested to support continued replacement of existng desktop and infrastructure equipment, replacement of the Police Station phone system, and support an increase in mobile devices for the School Department.

EO	NEW	INSTALL REMOTE SECURITY CAMERAS-TOWN WIDE	GFD	\$100,000	\$0	\$0	\$0	\$0	\$100,000
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Project No.: 1549 **Facility:** **Dept Rank:** 3 **In-Service Date:** 30-Sep-2015 **Last Update:** 11-Sep-2015

Justification: The Town parks, buildings, and infrastructure continue to be subject to vandalism. Several areas including the Middle School athletic fields, Memorial Field, Choate Park, Idylbrook Park (potentially) and others have regular reoccurring vandalism attacks. Installation of security cameras will substantially reduce this threat. Vandals that are not deterred will be filmed with the odds of arrest increased.

EO	NEW	PURCHASE TOWN ADMIN COLOR COPIER	TAX	\$9,000	\$0	\$0	\$0	\$0	\$9,000
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Project No.: 1630 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Aug-2015 **Last Update:** 11-Sep-2015

Justification: Currently, Town Hall does not have a color copier; would fill a need for print jobs that are typically outsourced.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes

TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

FY'2017 ONLY

Dept. Totals:	\$644,000	\$355,000	\$270,000	\$20,000	\$0	\$984,000
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Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Library

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST				
BU	RPL	REPLACE FIRE ALARM PANEL	GFD	\$25,000	\$0	\$0	\$0	\$0	\$25,000				
Project No.:		1317	Facility:		LIBRARY	Dept Rank:		5	In-Service Date:	30-Jun-2017	Last Update:		09-Sep-2015
Justification: Reaching the end of fire alarm panel's useful life, and parts are no longer available. Prior year approval of \$6,100 not used as insufficient funds.													
BU	RPL	NEW ROOF SHINGLES & GUTTERS	GFD	\$60,000	\$0	\$0	\$0	\$0	\$60,000				
Project No.:		1262	Facility:		LIBRARY	Dept Rank:		3	In-Service Date:	30-Jun-2017	Last Update:		11-Sep-2015
Justification: Roof shingles are old and past their useful life. Gutters are bent causing water leak at front & side wall													
BU	NEW	CREATE MAKER SPACE IN PART OF BASEMENT	GFD	\$50,000	\$0	\$0	\$0	\$0	\$50,000				
Project No.:		1621	Facility:		LIBRARY	Dept Rank:		3	In-Service Date:	30-Jun-2020	Last Update:		09-Sep-2015
Justification: Would provide more community space, for which there has been increasing demand, as well as exhibit space for art works created by students and other community members, and possibly for historical artifacts													
Dept. Totals:				\$135,000	\$0	\$0	\$0	\$0	\$135,000				

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Planning & Economic Development |

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
BU	NEW	INSTALL COMMUNITY BUILDINGS & WELCOME SIGNS	GFD	\$44,430	\$0	\$0	\$0	\$0	\$44,430
Project No.: 1301a Facility: VARIOUS Dept Rank: 5 In-Service Date: 30-Jun-2016 Last Update: 11-Sep-2015				Justification: Part of a community beautification and public relations effort.					
Dept. Totals:				\$44,430	\$0	\$0	\$0	\$0	\$44,430

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Police

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EO	NEW	PURCHASE POLICE EQUIPMENT - DEFIBS	GFD	\$13,215	\$0	\$0	\$0	\$0	\$13,215
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Project No.: 1709 **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Aug-2016 **Last Update:** 03-Sep-2015

Justification: I would like to purchase a total of 7 new defibrillators for the police dept. We currently have three older model lifepack 500's that were purchased back in 2003 and 2004 that I would like to replace with the newer model lifepack 1000. I also have on defibrillator that is damaged that I would like to replace along with placing the remaining three units in police vehicles that don't currently have defibrillators in them. I have been told by the Physio Control Company that the normal life span for a defibrillator is 8-10 years. We respond, along with the Fire Dept., to all medical emergencies within town. On many occasions we are first on the scene and will start the initial assessment and treatment process of the injured party. I want to be sure that all our vehicles are equipped with the defibrillators, so when we do respond to the cardiac arrest or medical emergency where the defibrillator is needed we have the equipment to start administering the needed lifesaving treatment as soon as possible. I don't want to put our officers or patients into a situation where they have to wait for the needed treatment until the ambulance or an additional police cruiser arrives that is equipped with a defibrillator.

EO	NEW	INSTALL CRUISER LICENSE PLATE RECOGNITION SYSTE	GFD	\$18,000	\$0	\$0	\$0	\$0	\$18,000
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Project No.: 1629 **Facility:** **Dept Rank:** 3 **In-Service Date:** 30-Jun-2016 **Last Update:** 11-Sep-2015

Justification: The Medway Police would like to improve response times and monitoring capabilities of the cruisers by adding an LPR system to one cruiser. This system will be used to identify unregistered, uninsured, and violation vehicles that may pose a safety and security risk for our residents.

Dept. Totals: \$31,215 \$0 \$0 \$0 \$0 \$31,215

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

School

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST				
BU	NEW	REMODEL LOBBY	GFD	\$243,100	\$0	\$0	\$0	\$0	\$243,100				
Project No.:		1701	Facility:		MCGOVERN	Dept Rank:		4	In-Service Date:	15-Aug-2016	Last Update:		02-Sep-2015
Justification: Lobby redesigned would improve both safety and handicap accessibility													

INF	MAJ	RECONSTRUCT MCGOVERN PARKING LOT	GFD	\$224,000	\$0	\$0	\$0	\$0	\$224,000				
Project No.:		1604	Facility:		MCGOVERN	Dept Rank:		4	In-Service Date:	01-Jul-2015	Last Update:		11-Sep-2015
Justification: Road leading to McGovern school has been consistently patched													

EO	RPL	UPGRADE CLASSROOM FURNITURE	OLG	\$200,000	\$0	\$0	\$0	\$0	\$200,000				
Project No.:		1526	Facility:		MIDDLE	Dept Rank:		4	In-Service Date:	01-Sep-2016	Last Update:		11-Sep-2015
Justification: Would replace classroom furniture in Middle School													

INF	RPL	RENOVATE PARKING LOT & PAVING	GFD	\$150,000	\$0	\$0	\$0	\$0	\$150,000				
Project No.:		1230	Facility:		BURKE	Dept Rank:		4	In-Service Date:	01-Sep-2015	Last Update:		11-Sep-2015
Justification: Plan was prepared by MPD Officer Mitchell pre-2009. Would provide safer access to Memorial and Burke campus.													

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

School

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
LPF	RPR	REPAIR TENNIS COURTS	GFD	\$42,000	\$0	\$0	\$0	\$0	\$42,000
Project No.: 1729		Facility: HIGH SCHOOL	Dept Rank: 3	In-Service Date: 01-Sep-2016		Last Update: 13-Oct-2015			
Justification: Courts continue to be used by public and school athletic programs, despite numerous cracks									
BU	RPR	RECONFIGURE LIGHTING FIXTURES	GFD	\$30,000	\$0	\$0	\$0	\$0	\$30,000
Project No.: 1607		Facility: MCGOVERN;MEM	Dept Rank: 3	In-Service Date: 01-Sep-2016		Last Update: 11-Sep-2015			
Justification: New design would provide for better security & flow; new design would provide place for enhanced instruction at McGovern and Memorial									
EO	NEW	PURCHASE THREE SCISSOR LIFTS	GFD	\$42,600	\$0	\$0	\$0	\$0	\$42,600
Project No.: 1521		Facility: MID;HIGH;MEM	Dept Rank: 2	In-Service Date: 01-Jul-2015		Last Update: 11-Sep-2015			
Justification: Purchase would eliminate the need to rent lifts when needed									
EV	RPL	REPLACE PICK-UP TRUCK & SANDER	GFD	\$36,000	\$0	\$0	\$0	\$0	\$36,000
Project No.: 1608		Facility:	Dept Rank: 2	In-Service Date: 01-Sep-2016		Last Update: 11-Sep-2015			
Justification: Existing Equipment 10 years old; rotting to sander even with low mileage									

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

School

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>	
EO	RPL	REPLACE ACID NEUTRALIZER TANK	GFD	\$30,000	\$0	\$0	\$0	\$0	\$30,000	
Project No.:		1525	Facility:	HIGH SCHOOL	Dept Rank:	2	In-Service Date:	01-Sep-2015	Last Update:	11-Sep-2015
Justification: New system should replace original equipment; which is now 10 years old and is subject to frequent odor backups in the building										

EO	RPL	REPLACE FOOD SERVICE EQUIPMENT	OLG	\$100,000	\$0	\$0	\$0	\$0	\$100,000	
Project No.:		1247	Facility:		Dept Rank:	1	In-Service Date:	01-Sep-2016	Last Update:	11-Sep-2015
Justification: Much of the equipment of the food service program is ancient at Middle School & Elementaries										

Dept. Totals:				\$1,097,700	\$0	\$0	\$0	\$0	\$1,097,700	

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

Town Clerk

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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EO	RPL	REPLACE VOTING BOOTHS	GFD	\$10,000	\$0	\$0	\$0	\$0	\$10,000
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Project No.: 1728 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Jul-2016 **Last Update:** 13-Oct-2015

Justification: Booths have not been replaced for at least 35 yrs. Outdated, hard to set up. Franklin offered there older wooden booths last year for free. I took them, not working bulky, falling down and hard to store.

Dept. Totals: \$10,000 \$0 \$0 \$0 \$0 \$10,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

FY'2017 ONLY

	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
Grand Totals:	\$15,799,745	\$2,030,850	\$2,160,000	\$810,000	\$790,000	\$21,380,595

SUMMARY

ALL YEARS

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

Council On Aging

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
BU	RPR	REPAIR WEST DOOR & THRESHOLD	GFD	\$12,000	\$0	\$0	\$0	\$0	\$12,000
Project No.: 1730 Facility: SR CENTER Dept Rank: 5 In-Service Date: 01-Nov-2016 Last Update: 20-Oct-2015 Justification: Door and frame has had damage over the past few winters. The concrete threshold heaves in freezing weather causing damage to the door, door frame, and interior walls. Repairs will be made to stop the heaving and replace the doors.									
BU	RPL	REPLACE SR CENTER ROOF	GFD	\$0	\$35,000	\$0	\$0	\$0	\$35,000
Project No.: 1406 Facility: SR CENTER Dept Rank: 4 In-Service Date: 30-Oct-2017 Last Update: 11-Sep-2015 Justification: Sr Center was originally built in 1997. The roof is showing signs of wear and needs to be replaced.									
Dept. Totals:				\$12,000	\$35,000	\$0	\$0	\$0	\$47,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Building Maintenance

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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INF RPR [GENERAL TOWN WIDE FACILITY IMPROVEMENTS](#) GFD \$50,000 \$25,000 \$25,000 \$25,000 \$25,000 \$150,000

Project No.: 1217b **Facility:** MUNICIPAL **Dept Rank:** 4 **In-Service Date:** 30-Jun-2016 **Last Update:** 11-Sep-2015

Justification: Similar to strategy used by corporations, this establishes a facilities improvement/replacement budget for nominal unanticipated repair needs that occur during the year. Current list is attached. If not funded, some projects must be done and must be included as separate CIPC items or operating budget.

EV RPL [REPLACE 2009 FORD ECONOVAN \(511-BLDG MTN\)](#) GFD \$0 \$0 \$25,000 \$0 \$0 \$25,000

Project No.: 1724 **Facility:** **Dept Rank:** 1 **In-Service Date:** 30-Jun-2019 **Last Update:** 11-Sep-2015

Justification: Normal replacement schedule.

Dept. Totals: \$50,000 \$25,000 \$50,000 \$25,000 \$25,000 \$175,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Highway/Admin

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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BU RPL [CONSTRUCT NEW DPS FACILITY](#) MXD **\$10,000,000** \$0 \$0 \$0 \$0 \$10,000,000

Project No.: 0804a **Facility:** DPS FACILITY **Dept Rank:** 5 **In-Service Date:** 01-Sep-2016 **Last Update:** 02-Sep-2015

Justification: Consolidates all DPW operations into one facility. Current buildings are poor structures that have exceeded useful life. Rear of highway building is collapsing. Base of building is rotting away and is no longer weather tight. Water station is a turn of the century structure in very poor condition. It has no bathroom or locker room facilities. Overall building sizes are too small for current operations. Building support systems have deteriorated beyond effective repair operations. Building support systems have deteriorated beyond effective repair. Direct savings attributed to reduced repair costs for various facilities and energy savings. Additional direct savings from stopping lease agreements. Indirect savings attributed to consolidated operations, better communication and coordination, reduced travel time for equipment staging.

EV RPL [REPLACE DUMP TRUCK \(212-1999 INT\)](#) GFD **\$165,000** \$0 \$0 \$0 \$0 \$165,000

Project No.: 1420 **Facility:** **Dept Rank:** 5 **In-Service Date:** 30-Jan-2016 **Last Update:** 03-Sep-2015

Justification: TRUCK 2 is 1999 model reaching the end of its useful life. This vehicle is used regularly to perform normal construction and maintenance activities, as well as, snow plowing operations. Maintenance and repair costs are expected to increase and loss of the vehicle would require use of more contractors to supplement it especially in winter months.

EV NEW [PURCHASE MEDIUM DUMP TRUCK](#) GFD **\$70,000** \$0 \$0 \$0 \$0 \$70,000

Project No.: 1615 **Facility:** **Dept Rank:** 5 **In-Service Date:** 30-Jan-2017 **Last Update:** 03-Sep-2015

Justification: To be primarily for the Environmental Services Division for parks maintenance. Another vehicle to support expanded operations is needed. A medium sized truck to haul mulch and loam can increase efficiency by being able to deliver larger loads to the job site. Our large trucks are too heavy and damage the fields. The need for a vehicle in this size range is regular and recurring.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Highway/Admin

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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EV RPL [REPLACE FLEET MAINT TRUCK \(FM-1\)](#) GFD \$78,000 \$0 \$0 \$0 \$0 \$78,000

Project No.: 1213 **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Apr-2016 **Last Update:** 11-Sep-2015

Justification: Vehicle Maintenance is a 2001 model. Truck is reaching the end of its useful life. The existing truck is a repurposed vehicle from the Water Division. A new truck will be purpose built as a fleet field support vehicle allowing staff to perform numerous types of repairs in the field that are now, not possible. Towing charges ranging from \$100 to over \$500 per vehicle will be reduced as we would begin servicing more of these breakdown on location. The vehicle would also be outfitted to provide other infrastructure support such as welding and lift capabilities.

EV NEW [PURCHASE THREE USED TRUCKS/VANS](#) GFD \$39,000 \$0 \$0 \$0 \$0 \$39,000

Project No.: 1706 **Facility:** **Dept Rank:** 4 **In-Service Date:** 20-Jul-2016 **Last Update:** 03-Sep-2015

Justification: Between new employees on an on-going basis (one in Building Maintenance and two Water/Sewer), seasonal help where six employees are added for the Summer months, and added facilities to cover for the School grounds, DPS does not have enough small light vehicles to perform routine tasks. Addition of three more pick-up trucks or vans will allow us to more efficiently perform our work. These vehicles are not needed for plowing and new vehicles are not required.

INF RPL [REPLACE STREETLIGHTS W/LED TECHNOLOGY](#) GFD \$140,000 \$0 \$0 \$0 \$0 \$60,000

Project No.: 1551 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Oct-2015 **Last Update:** 11-Sep-2015

Justification: Project is based on economics and "Green Community" status. LED streetlights would replace existing High Pressure Sodium lights throughout the remainder of the Town. Summer, Holliston, Village, Milford, Main, and Winthrop Streets have been changed to LED as part of the Green Community grant work. About 340 lights remain to be changed. The vendor estimates about a 6 year payback on the work saving about 65,000 kwh annually valued at approx. \$14,000. LED technology also reduces maintenance costs as the fixture have a longer time between failure.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Highway/Admin

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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EV NEW [PURCHASE MINI-EXCAVATOR / TRAILER](#) MXD \$77,000 \$0 \$0 \$0 \$0 \$77,000

Project No.: 1503 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Sep-2015 **Last Update:** 11-Sep-2015

Justification: Provides ability to perform a variety of small dig jobs with low ground pressure and improved efficiency. Most small utility contractors use this type of machine to supplement larger equipment. DPS has a single backhoe and both Water and Highway vie for time. This unit gives both Divisions access to another machine. Low ground pressure with this machine also allows it to provide service where the backhoe cannot such as wet areas, parks in the spring, field use, etc. Over the past two years, the Department has rented a excavator spending about \$10,000 annually to support extra workload.

EO NEW [PURCHASE BOOM MOWER](#) GFD \$20,000 \$0 \$0 \$0 \$0 \$20,000

Project No.: 1727 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Aug-2016 **Last Update:** 15-Sep-2015

Justification: Demands for roadside mowing and maintenance are increasing. DPS currently has two sidewalk tractors and a single boom mower for roadside cutting. Addition of a second boom will allow both machines to be in service during the summer and provide back-up in case of a failed mower.

EV RPL [REPLACE SIDEWALK PLOW /BLOWER](#) GFD \$0 \$0 \$160,000 \$0 \$0 \$160,000

Project No.: 0902 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Nov-2018 **Last Update:** 03-Sep-2015

Justification: Requesting a trade of one replacement multi-purpose tractor designed for sidewalk snow removal and general roadside maintenance. New sidewalks added to inventory as part of the Rt. 126 Reconstruction project as well as those recognized to be actively used are now included in the Department's sidewalk plowing program. Two machines now of similar vintage (2007) can provide expected level of response. However, having machines of staggered age (new vs. old) will allow for regular replacement and better service availability.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Highway/Admin

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
EV	RPL	REPLACE STREET SWEEPER	GFD	\$0	\$183,000	\$0	\$0	\$0	\$183,000
Project No.: 1501		Facility:	Dept Rank: 3	In-Service Date: 01-Mar-2018		Last Update: 02-Sep-2015			
Justification: Repair frequency of existing sweeper is growing with associated costs. Vehicle is reaching the end of its useful life. Vehicle is needed to meet EPA stormwater regulations.									
LPF	NEW	MEDWAY ARBOR BEAUTIFICATION	GFD	\$15,000	\$15,000	\$15,000	\$15,000	\$15,000	\$75,000
Project No.: 1227		Facility:	Dept Rank: 2	In-Service Date: 30-Jun-2016		Last Update: 11-Sep-2015			
Justification: Medway is noted for its rural charm. Over the past several years, through many natural causes, the Town has lost many trees along its roadways. This project proposes to plant new trees generally in areas of heavy decay or where roadwork is prevalent to maintain/restore Medway's scenic appeal.									
EO	RPL	REPLACE TRUCK MOUNTED SANDER	GFD	\$0	\$18,600	\$0	\$0	\$0	\$18,600
Project No.: 1618		Facility:	Dept Rank: 2	In-Service Date: 01-Nov-2017		Last Update: 11-Sep-2015			
Justification: Truck mounted sander used every winter as part of normal winter safety protocols. Oldest unit is at the end of its useful life. Must be replaced.									
EV	RPL	REPLACE TRUCK (211-FORD F350)	GFD	\$0	\$65,000	\$0	\$0	\$0	\$65,000
Project No.: 1422		Facility:	Dept Rank: 1	In-Service Date: 30-Oct-2017		Last Update: 03-Sep-2015			
Justification: TRUCK 1 is 2004 model and would be ending its useful life around FY'17. Replace with F550 chassis.									

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Highway/Admin

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
EV	RPL	REPLACE DUMP TRUCK (217-1997 INT)	GFD	\$0	\$165,000	\$0	\$0	\$0	\$165,000
Project No.: 1427		Facility:	Dept Rank: 1	In-Service Date: 30-Nov-2017	Last Update: 02-Sep-2015				
Justification: Current vehicle is a 1997 model. Review fleet status and plan on regular rotation change out due to service life.									
EV	RPL	REPLACE PICK-UP TRUCK (202-2006 FORD F150)	GFD	\$0	\$0	\$45,000	\$0	\$0	\$45,000
Project No.: 1509		Facility:	Dept Rank: 1	In-Service Date: 31-Jul-2018	Last Update: 11-Sep-2015				
Justification: Current vehicle is 2006 and will be 13 Years old at time of replacement. Current vehicle is a light duty small pick-up not suitable for hauling and plowing operations. Propose to replace with a full size pick-up.									
EV	RPL	REPLACE FRONT END LOADER (232 2001 JOHN DEERE)	GFD	\$0	\$0	\$0	\$185,000	\$0	\$185,000
Project No.: 1619		Facility:	Dept Rank: 1	In-Service Date: 01-Nov-2019	Last Update: 11-Sep-2015				
Justification: L-2 is the primary front-end vehicle in DPS. It is in operation every day and cannot be OOS for any extended period of time without severely impacting public service level. Currently planning for a rehabilitation to allow extended life.									
EV	RPL	REPLACE DUMP TRUCK (213-2000 INT)	GFD	\$0	\$165,000	\$0	\$0	\$0	\$165,000
Project No.: 1620		Facility:	Dept Rank: 1	In-Service Date: 30-Jan-2018	Last Update: 11-Sep-2015				
Justification: TRUCK 3 is 20009 model reaching the end of its useful life. This vehicle is used regularly to perform normal construction and maintenance activities, as well as, snow plowing operations. Maintenance and repair costs are expected to increase and loss of the vehicle would require use of more contractors to supplement it especially in winter months.									

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

DPS - Highway/Admin

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
EV	RPL	REPLACE 2001 FORD F250 (221)	GFD	\$0	\$40,000	\$0	\$0	\$0	\$40,000
Project No.:		1715	Facility:	Dept Rank:	1	In-Service Date:	30-Jun-2018	Last Update:	11-Sep-2015
Justification: Normal replacement schedule.									
EV	RPL	REPLACE 2001 MACK RD690S (216-TRUCK 6)	GFD	\$0	\$0	\$0	\$0	\$180,000	\$180,000
Project No.:		1716	Facility:	Dept Rank:	1	In-Service Date:	30-Jun-2021	Last Update:	11-Sep-2015
Justification: Normal replacement schedule.									
EV	RPL	REPLACE 2005 INTERNATIONAL 7400 DUMP (215-TRUCK)	GFD	\$0	\$0	\$0	\$165,000	\$0	\$165,000
Project No.:		1719	Facility:	Dept Rank:	1	In-Service Date:	30-Jun-2020	Last Update:	11-Sep-2015
Justification: Normal replacement schedule.									
EV	RPL	REPLACE 2008 FORD F350 (230)	GFD	\$0	\$40,000	\$0	\$0	\$0	\$40,000
Project No.:		1721	Facility:	Dept Rank:	1	In-Service Date:	30-Jun-2018	Last Update:	11-Sep-2015
Justification: Normal replacement schedule.									

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

DPS - Highway/Admin

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
EV	RPL	REPLACE 2008 MERCURY SABLE (710-TC-1)	GFD	\$0	\$40,000	\$0	\$0	\$0	\$40,000
Project No.: 1722 Facility:				Dept Rank: 1	In-Service Date: 30-Jun-2018	Last Update: 11-Sep-2015			
Justification: Normal replacement schedule.									
EV	RPL	REPLACE 2010 FORD F350 (214-TRUCK 4)	GFD	\$0	\$0	\$0	\$40,000	\$0	\$40,000
Project No.: 1725 Facility:				Dept Rank: 1	In-Service Date: 30-Jun-2020	Last Update: 11-Sep-2015			
Justification: Normal replacement schedule.									
Dept. Totals:				\$10,604,000	\$731,600	\$220,000	\$405,000	\$195,000	\$12,075,600

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Parks

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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EO RPL [REPLACE LEAF BOX VACUUM](#) GFD \$57,000 \$0 \$0 \$0 \$0 \$57,000

Project No.: 1712 **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Sep-2016 **Last Update:** 11-Sep-2015

Justification: Existing unit is undersized for new responsibilities in parks, schools, and other facilities. Can also be used by Highway for roadside maintenance.

LPF RPL [REPLACE OAKLAND BASKETBALL COURT LIGHTS w/LED](#) GFD \$40,000 \$0 \$0 \$0 \$0 \$40,000

Project No.: 1713 **Facility:** OAKLAND **Dept Rank:** 3 **In-Service Date:** 01-Sep-2016 **Last Update:** 11-Sep-2015

Justification: Energy savings at the park and longer use possible. Remote programming is desirable.

LPF RPL [REPLACE SMALL MOWER \(KABOTA Z\)](#) GFD \$17,000 \$0 \$0 \$0 \$0 \$17,000

Project No.: 1507 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Mar-2016 **Last Update:** 11-Sep-2015

Justification: Small landscapers riding lawn mower is used daily for various Town properties where other equipment is too large. Existing unit is a 2007 and is reaching the end of it's useful life.

EO RPL [REPLACE LAWN TRACTOR](#) GFD \$15,000 \$0 \$0 \$0 \$0 \$15,000

Project No.: 0815 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Apr-2016 **Last Update:** 11-Sep-2015

Justification: Replace 1994 Unit used for mid-size mowing and other functions. This is a multi-purpose tractor with PTO assemble, but on a smaller scale for use on Town properties where the other mowers/tractors are not feasible to use.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

DPS - Parks

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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LPF RPL [RENOVATE PLAYGROUNDS](#) GFD \$0 \$25,000 \$15,000 \$0 \$0 \$40,000

Project No.: 1632 **Facility:** PLAYGROUNDS **Dept Rank:** 2 **In-Service Date:** 01-May-2018 **Last Update:** 11-Sep-2015

Justification: All the playgrounds in Town are twenty years old or more. None of these structures are expected to have more than a twenty year life. The DPS has been performing repairs/maintenance on the structures for years, but many main sections are now beyond safe repair. These structures are custom made by the companies that sell them. Replacement parts, if available, are extremely expensive. Unfortunately, outright replacement is even more costly. Playground safety dictates that repairs be made or we begin retiring sections of the play structures as they deteriorate.

EV RPL [REPLACE 1998 KUBOTA TRACTOR \(KUBOTA-LARGET1\)](#) GFD \$0 \$25,000 \$0 \$0 \$0 \$25,000

Project No.: 1714 **Facility:** **Dept Rank:** 1 **In-Service Date:** 30-Jun-2018 **Last Update:** 11-Sep-2015

Justification: Normal replacement schedule.

EV RPL [REPLACE 2003 FORD RANGER \(218-P-4\)](#) GFD \$0 \$40,000 \$0 \$0 \$0 \$40,000

Project No.: 1717 **Facility:** **Dept Rank:** 1 **In-Service Date:** 30-Jun-2018 **Last Update:** 11-Sep-2015

Justification: Normal replacement schedule.

EV RPL [REPLACE 2007 KUBOTA ZD331 MOWER \(KUBOTA RIDER](#) GFD \$0 \$20,000 \$0 \$0 \$0 \$20,000

Project No.: 1720 **Facility:** **Dept Rank:** 1 **In-Service Date:** 30-Jun-2018 **Last Update:** 11-Sep-2015

Justification: Normal replacement schedule.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

DPS - Parks

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
EV	RPL	REPLACE 2009 FORD F350 (312-P-2)	GFD	\$0	\$0	\$40,000	\$0	\$0	\$40,000
Project No.: 1723 Facility:			Dept Rank: 1	In-Service Date: 30-Jun-2019		Last Update: 11-Sep-2015			
Justification: Normal replacement schedule.									
EV	RPL	REPLACE 2011 FORD F250 (313-P-3)	GFD	\$0	\$0	\$0	\$0	\$40,000	\$40,000
Project No.: 1726 Facility:			Dept Rank: 1	In-Service Date: 30-Jun-2021		Last Update: 11-Sep-2015			
Justification: Normal replacement schedule.									
Dept. Totals:				\$129,000	\$110,000	\$55,000	\$0	\$40,000	\$334,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Road Repair

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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INF MAJ [VARIOUS ROAD/SIDEWALK IMPROVEMENTS](#) GFD **\$750,000** \$750,000 \$750,000 \$750,000 \$750,000 \$3,750,000

Project No.: 0818g **Facility:** **Dept Rank:** 5 **In-Service Date:** 30-Jun-2017 **Last Update:** 11-Sep-2015

Justification: Repair/Reconstruct roads and sidewalks in poor condition as part of the Towns Annual Roadway & Sidewalk Improvement Program

INF RPR [REPAIR WINTHROP ST CULVERT](#) GFD **\$90,000** \$0 \$0 \$0 \$0 \$90,000

Project No.: 1705 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Oct-2016 **Last Update:** 03-Sep-2015

Justification: While performing water main installation crossing the existing culvert, several severe deficiencies and voids were found at the culvert. The original corrugated steel culvert is heavily pitted and rotted. Extensions added to widen the street at some point in the past were not well sealed. These areas have voids at the joints with serious leakage. This is eroding the road sub-base and leading to road failure. The proposed repair if possible uses liners that do not require removal or excavation of the existing culvert.

INF NEW [VARIOUS STORM WATER IMPROVEMENTS](#) GFD **\$0** \$500,000 \$0 \$0 \$0 \$500,000

Project No.: 1307 **Facility:** **Dept Rank:** 2 **In-Service Date:** 30-Jun-2018 **Last Update:** 02-Sep-2015

Justification: Provide funds for general infrastructure improvements and repairs. New NPDES storm water permit requirements will mandate several repairs and modifications to be identified through the water resources management program.

Dept. Totals: **\$840,000** \$1,250,000 \$750,000 \$750,000 \$750,000 \$4,340,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Sewer

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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EV	NEW	PURCHASE PRE-OWNED SEWER JETTING TRUCK	MXD	\$100,000	\$0	\$0	\$0	\$0	\$100,000
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Project No.: 1202 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Jun-2016 **Last Update:** 11-Sep-2015

Justification: This sewer maintenance service is required for proper operation. Currently using contractor at \$130 per hour. Two hours are common every two weeks. Needed and appropriate levels of system maintenance requires regular cleaning activities. This equipment will allow for a scheduled preventive maintenance program as well as provide for emergency response. Cleaning and clog removal. Also can be used for storm drain blockages and maintenance done several times a year. These vehicles are very expensive. A pre-owned vehicle is proposed commensurate with our workload.

INF	NEW	VARIOUS SEWER COLLECTION PROJECTS	EFS	\$15,000	\$0	\$0	\$0	\$0	\$15,000
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Project No.: 1626 **Facility:** **Dept Rank:** 3 **In-Service Date:** 30-Jun-2016 **Last Update:** 11-Sep-2015

Justification: Provides a small reserve of funds to handle emergent/necessary repairs and upgrades to the sewer system that fall outside of normal operating expenditures.

INF	RPR	SEWER INFLOW & INFILTRATION REPAIRS	ETS	\$0	\$75,000	\$200,000	\$75,000	\$200,000	\$550,000
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Project No.: 1426b **Facility:** **Dept Rank:** 2 **In-Service Date:** 01-Nov-2017 **Last Update:** 02-Sep-2015

Justification: Sanitary sewers are susceptible to infiltration of ground water resulting in large volumes of water being processed for treatment unnecessarily and limiting the capacity of the system to handle actual sewer flows. This on-going effort will produce a report leading to infrastructure improvements to reduce the level of groundwater and stormwater entering the Town's sanitary sewer system. The desired outcome of the work is to allow the Town to make further reductions in its levels of infiltration and inflow which will open the system for authorized flows associated with future development. Per our permit with the Charles River Pollution Control District, we are required to perform routine evaluations and repairs of the system.

Dept. Totals: \$115,000 \$75,000 \$200,000 \$75,000 \$200,000 \$665,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

DPS - Solid Waste

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
EV	RPL	REPLACE FRONT END LOADER (231-2003 CAT)	GFD	\$0	\$160,000	\$0	\$0	\$0	\$160,000
Project No.: 1401 Facility:				Dept Rank: 1 In-Service Date: 01-Oct-2017		Last Update: 02-Sep-2015			
Justification: Replace L-1, a 2003 Caterpillar front end loader. Machine is used daily and is at the end of its service life.									
Dept. Totals:				\$0	\$160,000	\$0	\$0	\$0	\$160,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Water

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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INF RPL [REPLACE WATER MAINS - BRENTWOOD AREA /HOLLIST](#) EFW \$1,000,000 \$100,000 \$1,100,000 \$0 \$0 \$2,200,000

Project No.: 1614a **Facility:** **Dept Rank:** 4 **In-Service Date:** 30-Sep-2016 **Last Update:** 02-Sep-2015

Justification: Strategic improvements to water system at Brentwood area (Meryl, Gorwin, Virginia, and area streets) require line replacement to provide service reliability, firefighting capabilities and high quality drinking water. Recommended in the Town's Water Master Plan.

EO NEW [PURCHASE GATE VALVE VACUUM BOX](#) EFW \$65,000 \$0 \$0 \$0 \$0 \$65,000

Project No.: 1410a **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Aug-2016 **Last Update:** 03-Sep-2015

Justification: The original authorization for \$15,000 was to provide a tool to clean gate valve stem boxes in right-of-way. Poor cleaning results in valve stem damage and failure over time. Cleaning the stem boxes routinely during annual maintenance of hydrants and before/after road work will lengthen service life of vales and result in fewer emergency replacements of failing gate valves. The unit could also provide a limited amount of service for sewer or catchbasin cleaning, however, it does not replace a full size vactor truck. AMENDED-new units are now available that excersize valves and clear sticking valves without the need to excavate. The Department has not had a valve exercise program, although much of this work is performed during annual flushing. A more comprehensive program is needed to ensure system integrity and allow for minimal system outages and disturbances.. During a demonstration, a similar unit opened a stuck valve that was scheduled for dig and replacement. Estimated savings for the single valve in questions was about \$5,000. Eight to ten valves per year are scheduled for replacement. This unit could eliminate at least 1/3 of those replacements saving \$15,000 annually.

EV RPL [REPLACE TRUCK \(415 2004 CHEVY SILVARADO\)](#) EFW \$58,000 \$0 \$0 \$0 \$0 \$58,000

Project No.: 1423 **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Oct-2016 **Last Update:** 03-Sep-2015

Justification: W-5 is 2004 model used daily in DPS operations.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Water

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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EV RPL [REPLACE TRUCK \(402-2005 CHEVY COLORADO\)](#) EFW \$38,000 \$0 \$0 \$0 \$0 \$38,000

Project No.: 1421 **Facility:** **Dept Rank:** 4 **In-Service Date:** 30-Jan-2017 **Last Update:** 03-Sep-2015

Justification: W-4 is 2005 small pick-up, standard transmission and not of great value to support DPS operations. Could be potentially repurposed to Parks, but given limited number of operators, recommend trade in. The Water Division has a need for a larger vehicle with a cap and tool boxes or internal storage for day-to-day operations. Currently looking at Transit vans.

EV RPL [REPLACE DUMP TRUCK CHASSIS \(412-2001 INTER\) & B](#) MXD \$0 \$136,000 \$0 \$0 \$0 \$136,000

Project No.: 1319 **Facility:** **Dept Rank:** 3 **In-Service Date:** 30-Oct-2017 **Last Update:** 03-Sep-2015

Justification: Replace W-2, a 2001 International dump truck. Move W-2 chassis to Parks service, replace body for Parks, and install current stainless steel body on new International chassis. W-2 is used periodically for routine service work year round. It provides large capacity for response to large water breaks. Because it is a manual transmission, it is used sparingly for plowing. Current chassis is frequently overloaded and a safety concern. In Parks, added staff and crew needs makes the addition of a mid-sized truck a priority for the Department. Most park and field related work cannot use a large truck due to weight concerns and resulting turf damage. Smaller trucks in regular use have proven too small to handle the higher loads of materials used for the parks, e.g. pallets of fertilizer or seed. Highway and Water also have a need from time to time of a niche size vehicle that crews can use. Funding would be \$118,000 from Water Enterprise and \$18,000 from General Fund.

INF NEW [INSTALL WATER FILTRATION SYSTEM](#) EFW \$0 \$300,000 \$3,100,000 \$0 \$0 \$3,400,000

Project No.: 1403 **Facility:** POPULATIC **Dept Rank:** 2 **In-Service Date:** 01-Oct-2018 **Last Update:** 11-Sep-2015

Justification: Latest Sanitary Survey conducted by the DEP shows Oakland and Village St. Wells to be in non-compliance for manganese and iron levels. Viable solutions are to seek new well sites, install filters at each well, or move to a centralized filter process. New well sites require proper hydrology, water that is not contaminated, and land to protect the aquifer. Very limited number of sites that meet this criteria. Ever increasing regulation will likely cause advanced water treatment requirements at all wells within a few years. Centralized water treatment will reduce long term operating costs by consolidating mechanics and operations. Raw water would be piped to Village St. well site where it can be centrally treated and pumped into the distribution system. This phase would include Oakland, Populatic, and Village St. Wells. Industrial could be tied in from Oakland St. at a later date when needed.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

DPS - Water

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
INF	NEW	VARIOUS WATER SYSTEM MAINTENA PROJECTS	EFW	\$0	\$15,000	\$0	\$0	\$0	\$15,000
Project No.: 1505		Facility:	Dept Rank: 2	In-Service Date: 01-Jun-2018		Last Update: 03-Sep-2015			
Justification: Provides a small reserve of funds to handle emergent/necessary repairs and upgrades to distribution system that fall outside of normal operating expenditures.									
INF	RPL	REPLACE WATER MAINS - HOLLISTON STREET	EFW	\$0	\$0	\$100,000	\$1,100,000	\$0	\$1,200,000
Project No.: 1308		Facility:	Dept Rank: 1	In-Service Date: 30-Sep-2019		Last Update: 11-Sep-2015			
Justification: Strategic improvements to water system at Holliston St. require line replacement to provide service reliability, firefighting capabilities and high quality drinking water. Recommended in the Town's Water Master Plan.									
EV	RPL	REPLACE 2004 FORD F250 (419)	EFW	\$0	\$40,000	\$0	\$0	\$0	\$40,000
Project No.: 1718		Facility:	Dept Rank: 1	In-Service Date: 30-Jun-2018		Last Update: 11-Sep-2015			
Justification: Normal replacement schedule.									
Dept. Totals:				\$1,161,000	\$591,000	\$4,300,000	\$1,100,000	\$0	\$7,152,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

Econ./Community Development

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
LPF	NEW	OAK GROVE PARK LAND ACQUISITION	OLG	\$74,400	\$785,850	\$0	\$0	\$0	\$1,035,250
Project No.: 1612 Facility:				Dept Rank: 5 In-Service Date: 02-Jun-2017		Last Update: 11-Sep-2015			
Justification: Total annual tax revenue estimate at build-out = \$1,156,345. Added \$175,000 in legal expenses. No figure available.									
Dept. Totals:				\$74,400	\$785,850	\$0	\$0	\$0	\$1,035,250

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Fire

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EV RPL [REPLACE AMBULANCE \(A-2\)](#) GFD \$260,000 \$0 \$0 \$0 \$0 \$260,000

Project No.: 1542 **Facility:** **Dept Rank:** 5 **In-Service Date:** 31-Dec-2016 **Last Update:** 11-Sep-2015

Justification: Our ambulance are seeing increased service. A-2 presently has over 82,000 miles on it. It is primarily used as our second run ambulance, however, as calls increase we are experiencing multiple calls more frequently. By the time we purchase the new truck we will have in excess of 100,000 miles on the truck and it will have outlived its usefulness without spending a great deal of money to keep it running safely. Additionally, A-1 is on track to have over 80,000 miles on it by the time this truck is purchased. This purchase is part of a regular Capital Improvement Plan by the department.

EO RPL [REPLACE AIRPACKS](#) GFD \$206,000 \$0 \$0 \$0 \$0 \$206,000

Project No.: 1541 **Facility:** **Dept Rank:** 5 **In-Service Date:** 31-Dec-2017 **Last Update:** 03-Sep-2015

Justification: Our present airpacks are 11 years old. The bottles must be replaced every 15 years. Additionally, to bring the current packs up to the current NFPA Standard, it would be cost nearly as much as new packs. Typically, after 2 standard changes manufacturers do not continue to support older models, so parts are no longer manufactured and the packs can no longer be repaired if parts are not available. Also, our present packs are not compatible with surrounding communities so if we need to swap bottles on major emergencies we do not have that capabilities. Our new packs would be interoperable with our surrounding communities.

EV RPR [REPAIR ENGINE 2](#) GFD \$170,000 \$0 \$0 \$0 \$0 \$170,000

Project No.: 1707 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Nov-2016 **Last Update:** 03-Sep-2015

Justification: Engine 2 is a 2002 E-One 1,500 Gallon per Minute Pumper. It has been plagued with issues for the last several years. While they have been relatively minor issues, they have caused the truck to be out of service several times and on 2 occasions have failed at emergency scenes. The pump is showing wear and if this is not addressed catastrophic failure is possible over time. The electrical system has proven to be unreliable and was the cause for the 2 failures on emergency scenes. There is corrosion in several spots on the truck that if left unrepaired will cause more damage to the truck resulting in a higher cost to repair.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

Fire

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
INF	PRL	REPLACE RADIO BOX FIRE ALARM SYSTEM	GFD	\$155,000	\$0	\$0	\$0	\$0	\$155,000
Project No.: 1538 Facility:				Dept Rank: 5	In-Service Date: 30-Jun-2016	Last Update: 11-Sep-2015			

Justification: Our present fire alarm reporting system used throughout the town uses 1860's technology. There are presently 4 "zones" in town for our fire alarm system. This means there is wire running though out the town that connects fire alarm system within connected buildings to our dispatch center. In the event a wire is severed this "zone" will be out of service until the broken wire is found and repaired. Quite often these breaks occur is severe weather when trees fall or are a result of a major motor vehicle accident that cause utility poles to be broken. In most cases it can be several hours, if not days, until these wires can be repaired meaning the buildings connected by the broken wire are not being monitored. In the event of a fire in one of these buildings the department would not be notified until someone sees the fire and calls the fire department via telephone. With radio boxes each building is independent of each other and if one building stops working the rest will continue to work without interruption. There is only one "receiver" at our dispatch center, and if it stops working we have no way to monitor the fire alarms connected. The present receiver is very old and often in need of repair. Parts are becoming difficult to find for the receiver. A new one will need to be purchased regardless if we don't go to radio boxes. This new system will have 2 receivers, so if one breaks the other continues to operate. In addition to the antiquated technology for transmitting the alarms, the system presently in use does not let us know if the fire alarm systems within a building are working. Also, if someone disconnects the fire alarm system in a building we will not know that as well. In some cases, for example, if a sprinkler system compressor fails, the department has no way of knowing until the air pressure in the sprinkler system falls below a certain pressure which will cause the fire alarm system to activate, causing disruption in the business and causing a full emergency response by the fire department. These cases generate needless costs to not only the business owners, but to the department as well. While responding to these accidental alarms can be harmful as well if responding personnel are involved in a motor vehicle accident. With radio boxes the department can monitor "troubles" in fire alarm systems and have one person respond non-emergency to find out what the problem is. In the case fore mentioned, we would notify the building owner who can in turn notify their sprinkler company who can rectify the problem before the sprinklers activate causing the fire alarm. There has been occasion that someone has disconnected the buildings fire alarm system from the Town's system therefore it would not transmit that alarm to the dispatcher. With our present system there is no way of knowing this unless we physically open each master box and see if it is connected. These systems are usually disconnected so the fire alarms or sprinklers can be tested without transmitting the alarm to the dispatcher. Presently, to avoid having these systems not be reconnected, the fire department will send a person to disconnect and reconnect the boxes. If there is no one available due to calls this may take quite a while to get accomplished. With the new radio boxes the dispatcher will be able to disconnect the master box from the dispatch center, eliminating the need to send someone, saving fuel and being much more efficient for the businesses. Additionally, if the master box is disconnected with our present system, as I mentioned earlier, there is no way of us knowing unless we physically look in the master box. With radio boxes the dispatcher will know instantly if someone disconnects it. With our present system, the wiring is in constant need of repair. To affect these repairs we must have someone using a bucket truck to make the repairs. Our bucket truck is about 20 years old and will soon need major expensive renovations as it is beginning to rot. With the radio box system we will not need the bucket truck.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Fire

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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Maintenance on the system will be greatly reduced, reducing the man hours required to maintain it. Another advantage to using the radio box system is panic alarms can be installed and connected to the radio boxes for schools. If there is an intruder the panic button is pushed and automatically transmits the alarm to the dispatcher who can immediately dispatch police, fire, and EMS resources. Teachers and administrators do not have to rely on cell signals or land lines to operate, the signal is transmitted immediately to the dispatcher indicating a panic alarm has been pressed.

EV MAJ [REFURBISH BRUSH 1](#) GFD \$18,000 \$0 \$0 \$0 \$0 \$18,000

Project No.: 1539 **Facility:** **Dept Rank:** 5 **In-Service Date:** 31-Oct-2015 **Last Update:** 11-Sep-2015

Justification: This truck is a 1995 vehicle. The body on this truck is rotted. Due to the nature of the rust it is nearly impossible to fix for the long term. The vehicle itself has very low mileage, less than 10,000 miles, so it has useful life left. The pump is likely to need repair if not replaced due to the nature of the work it does. It is a safety issue. If not replaced, the body could fall off the truck. DPS has agreed to plumb the truck. An original estimate to replace the body and plumb the new pump was \$34,000. This cost will be cut in half by DPS installing and plumbing the pump.

BU RPL [REPLACE WINDOWS & DOORS](#) GFD \$0 \$70,000 \$0 \$0 \$0 \$70,000

Project No.: 1613 **Facility:** STATION 1 **Dept Rank:** 5 **In-Service Date:** 01-Nov-2017 **Last Update:** 03-Sep-2015

Justification: Original doors and windows are in poor condition. These should be replaced for building integrity and energy efficiency.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Fire

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EO	NEW	PURCHASE ALS EQUIPMENT	ETA	\$0	\$60,000	\$0	\$0	\$0	\$60,000
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Project No.: 1624 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-May-2016 **Last Update:** 11-Sep-2015

Justification: The Fire Department is presently transitioning its ambulance service from a Basic Life Support service to an Advanced Life Support service. Presently our EMTs cannot administer life saving drugs, place breathing tubes, or perform surgical procedures that may mean the difference between life and death. Currently the department contracts with private ambulance services to provide those services. Three members of the department are currently completing a Paramedic course, and two others are scheduled to begin in September of 2014. This equipment will be used to diagnose heart issues, defibrillate, and provide many other pieces of diagnostic information such as oxygen levels, carbon monoxide levels, blood pressure, pulse rate etc. This will ensure our EMTs are able to begin life saving measures immediately upon arrival to an emergency scene. Furthermore, it will enable the Department to increase ambulance revenues. Several other departments who have switched from a BLS to an ALS service have increased revenue by about 30%. If the same holds true here I expect our revenue will increase by at least \$100,000 annually. In addition to added revenue, with changes in health care due to the Affordable Care Act, many service providers will need to provide new home services and change the way they operate. What has been termed by some as Community Paramedicine is quickly becoming a reality. Fire Departments who provide ambulance services will need to have trained Paramedics to provide home visits and seek alternate health care service destinations other than hospitals. This has the potential to reduce ambulance revenues for ambulance services that are not prepared to offer these services. Departments that are prepared and provide the services will benefit from cost savings sharing with health care facilities and insurance providers. Those estimates are not presently available.

EO	NEW	PURCHASE ENHANCED STATION ALERTING SYSTEM	GFD	\$43,000	\$0	\$0	\$0	\$0	\$43,000
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Project No.: 1543 **Facility:** **Dept Rank:** 3 **In-Service Date:** 31-Dec-2015 **Last Update:** 11-Sep-2015

Justification: Presently the only way Firefighters are alerted to respond to emergency calls is via radio. In the event the radio goes down a phone call would have to be made to the fire station. This system will notify Firefighters to respond to emergencies via a public address system of sorts. Presently if a firefighter has taken his portable radio off for whatever reason and is not in proximity of it or the speaker that is presently in use, he has the potential to miss the call. With this system no matter where they are in the station they will receive the notification. Also, this system will automatically turn on lights to illuminate egress for firefighters and the apparatus area which will reduce injuries due to firefighters tripping because they cannot see. It will also automatically shut off the gas valve to the stove so if firefighters are cooking while they receive a call and fail to turn the stove off before responding a fire will not start.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Fire

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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EV	RPL	REPLACE COMMAND VEHICLE	GFD	\$0	\$54,000	\$0	\$0	\$0	\$54,000
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Project No.: 1545 **Facility:** **Dept Rank:** 3 **In-Service Date:** 31-Oct-2017 **Last Update:** 11-Sep-2015

Justification: Our present Command Vehicle will be 5 years old and have around 100,000 miles on it. It will be placed into a reserve status for use as a fire prevention vehicle and general purpose utility vehicle for another 5 years.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

Fire

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EV	RPL	REPLACE ENGINE 5	GFD	\$0	\$0	\$650,000	\$0	\$0	\$650,000
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Project No.: 1546 **Facility:** **Dept Rank:** 2 **In-Service Date:** 30-Jun-2019 **Last Update:** 11-Sep-2015

Justification: Engine 5 is presently 23 years old. NFPA Standard 1901 suggests any apparatus over 15 years old be refurbished and placed in reserve status and any apparatus over 25 years old be replaced. D.1 General. To maximize fire fighter capabilities and minimize risk of injuries, it is important that fire apparatus be equipped with the latest safety features and operating capabilities. In the last 10 to 15 years, much progress has been made in upgrading functional capabilities and improving the safety features of fire apparatus. Apparatus manufactured prior to 1991 usually included only a few of the safety upgrades required by the recent editions of the NFPA fire department apparatus standards or the equivalent Underwriters Laboratories of Canada (ULC) standards. Because the changes, upgrades, and fine tuning to NFPA 1901, Standard for Automotive Fire Apparatus, have been truly significant, especially in the area of safety, fire departments should seriously consider the value (or risk) to fire fighters of keeping fire apparatus older than 15 years in first-line service. It is recommended that apparatus greater than 15 years old that have been properly maintained and that are still in serviceable condition be placed in reserve status and upgraded in accordance with NFPA 1912, Standard for Fire Apparatus Refurbishing, to incorporate as many features as possible of the current fire apparatus standard (see Section D.3). This will ensure that, while the apparatus might not totally comply with the current edition of the automotive fire apparatus standards, many of the improvements and upgrades required by the recent versions of the standards are available to the fire fighters who use the apparatus. Apparatus that were not manufactured to the applicable NFPA fire apparatus standards or that are over 25 years old should be replaced. Engine 2 will be 17 years old. It will be a candidate for refurbishment if fiscally feasible and prudent. The new engine will also be equipped with extrication tools that are pre-connected so when firefighters arrive on a scene they can immediately put the tools to work removing trapped victims from vehicles rather than have to take the time to set up the extrication equipment as we currently do. This can save upwards to 5 minutes on the scene. I had two vendors look at this truck and one sent a letter saying it is not a candidate for refurbishment due to its age and the other gave me a quote of \$350,000 in FY 14. That price would likely be closer to \$450,000 by the time the work is done. Refurbishing the truck would also mean we are still using an engine, transmission, and pump that will be 29 years old.

Dept. Totals: **\$852,000** \$184,000 \$650,000 \$0 \$0 \$1,686,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes

TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

Information Systems

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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IT RPL [REPLACE SCHOOL VIDEO SURVEILLANCE SYSTEMS](#) MXD **\$185,000** \$20,000 \$20,000 \$20,000 \$0 \$185,000

Project No.: 1711 **Facility:** HS,MCG,MEM **Dept Rank:** 5 **In-Service Date:** 01-Sep-2016 **Last Update:** 09-Sep-2015

Justification: The High School Surveillance system was installed in 2004. The digital storage systems are now 13+ years old, outdated, and failing. Service repairs for 2016 are in excess of \$10,000. This proposal would replace the current system with the same Avigilon IP based system used for the high school fields and provide remote access for review and monitoring by other departments (police). In addition, the McGovern and Memorial surveillance systems provide inadequate coverage by today's standards and no remote capability for monitoring.

IT RPL [TECHNOLOGY EQUIPMENT](#) GFD **\$350,000** \$335,000 \$250,000 \$0 \$0 \$690,000

Project No.: 1110f **Facility:** **Dept Rank:** 4 **In-Service Date:** 30-Jun-2016 **Last Update:** 11-Sep-2015

Justification: The increase in funding is requested to support continued replacement of existing desktop and infrastructure equipment, replacement of the Police Station phone system, and support an increase in mobile devices for the School Department.

EO NEW [INSTALL REMOTE SECURITY CAMERAS-TOWN WIDE](#) GFD **\$100,000** \$0 \$0 \$0 \$0 \$100,000

Project No.: 1549 **Facility:** **Dept Rank:** 3 **In-Service Date:** 30-Sep-2015 **Last Update:** 11-Sep-2015

Justification: The Town parks, buildings, and infrastructure continue to be subject to vandalism. Several areas including the Middle School athletic fields, Memorial Field, Choate Park, Idylbrook Park (potentially) and others have regular reoccurring vandalism attacks. Installation of security cameras will substantially reduce this threat. Vandals that are not deterred will be filmed with the odds of arrest increased.

EO NEW [PURCHASE TOWN ADMIN COLOR COPIER](#) TAX **\$9,000** \$0 \$0 \$0 \$0 \$9,000

Project No.: 1630 **Facility:** **Dept Rank:** 3 **In-Service Date:** 01-Aug-2015 **Last Update:** 11-Sep-2015

Justification: Currently, Town Hall does not have a color copier; would fill a need for print jobs that are typically outsourced.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

Dept. Totals: **\$644,000** \$355,000 \$270,000 \$20,000 \$0 \$984,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Library

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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BU RPL [REPLACE FIRE ALARM PANEL](#) GFD \$25,000 \$0 \$0 \$0 \$0 \$25,000

Project No.: 1317 **Facility:** LIBRARY **Dept Rank:** 5 **In-Service Date:** 30-Jun-2017 **Last Update:** 09-Sep-2015

Justification: Reaching the end of fire alarm panel's useful life, and parts are no longer available. Prior year approval of \$6,100 not used as insufficient funds.

BU RPL [NEW ROOF SHINGLES & GUTTERS](#) GFD \$60,000 \$0 \$0 \$0 \$0 \$60,000

Project No.: 1262 **Facility:** LIBRARY **Dept Rank:** 3 **In-Service Date:** 30-Jun-2017 **Last Update:** 11-Sep-2015

Justification: Roof shingles are old and past their useful life. Gutters are bent causing water leak at front & side wall

BU NEW [CREATE MAKER SPACE IN PART OF BASEMENT](#) GFD \$50,000 \$0 \$0 \$0 \$0 \$50,000

Project No.: 1621 **Facility:** LIBRARY **Dept Rank:** 3 **In-Service Date:** 30-Jun-2020 **Last Update:** 09-Sep-2015

Justification: Would provide more community space, for which there has been increasing demand, as well as exhibit space for art works created by students and other community members, and possibly for historical artifacts

BU RPR [EXISTING FURNITURE REFURBISHED](#) GFD \$0 \$0 \$0 \$10,000 \$0 \$10,000

Project No.: 1259 **Facility:** LIBRARY **Dept Rank:** 2 **In-Service Date:** 30-Jun-2020 **Last Update:** 11-Sep-2015

Justification: Fabric all torn & old- need to replace fabric & save wood frame

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

Library

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
BU	RPL	REPLACE LOWER LEVEL HVAC UNIT	GFD	\$0	\$10,000	\$0	\$0	\$0	\$10,000
Project No.: 1318 Facility: LIBRARY Dept Rank: 2 In-Service Date: 30-Jun-2018 Last Update: 11-Sep-2015									
Justification: HVAC unit at end of useful life									
Dept. Totals:				\$135,000	\$10,000	\$0	\$10,000	\$0	\$155,000

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Planning & Economic Development |

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>		
BU	NEW	INSTALL COMMUNITY BUILDINGS & WELCOME SIGNS	GFD	\$44,430	\$0	\$0	\$0	\$0	\$44,430		
Project No.:		1301a	Facility:	VARIOUS		Dept Rank:	5	In-Service Date:	30-Jun-2016	Last Update:	11-Sep-2015
Justification: Part of a community beautification and public relations effort.											

INF	NEW	UPDATE MEDWAY MASTER PLAN - CONSULTANT SERVIC	GFD	\$0	\$0	\$50,000	\$0	\$0	\$50,000		
Project No.:		1710	Facility:			Dept Rank:	5	In-Service Date:	01-Jul-2019	Last Update:	03-Sep-2015
Justification: The Medway Master Plan was last adopted by the Town in 2009. This proposal is to begin preparations for the development of an updated master plan during the fall of 2017 with the intent of having it adopted by the Planning and Economic Development Board and Town Meeting in May 2019. A community's master plan provides overall direction to the development of land in the municipality and guidance on zoning to accomplish the Town's land use goals.											

Dept. Totals:				\$44,430	\$0	\$50,000	\$0	\$0	\$94,430		

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Police

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EO New [PURCHASE SPEED MESSAGE RADAR TRAILER](#) GFD \$0 \$17,900 \$0 \$0 \$0 \$17,900

Project No.: 1513 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Aug-2017 **Last Update:** 11-Sep-2015

Justification: This unit would strength our traffic enforcement, by placing the speed trailer in various locations in town, where motorist would view a digital reading of the speed they were traveling. The speed trailer would provide positive feedback to the motorist of its speed and brings its speed to a higher level of attention, letting the motorist know to slow down if they are speeding. The unit serves as a warning to motorist that their speed is being watched. As many times motorist are not aware they are speeding. The benefit of using the speed trailer is it reduces speeds. Many times the residents complain of motorist speeding will have a false sense of vehicle speeds. The unit provides positive feedback to the residents to the speeds that motorists are traveling and gives them a good feel of vehicle traffic speeds on their street. The unit will also be capable of collecting and recording traffic stats, while set up at various locations, to assist the police department in planning successful enforcement strategies

EO RPL [REPLACE BALLISTIC VESTS](#) GFD \$0 \$0 \$21,725 \$0 \$0 \$21,725

Project No.: 1514 **Facility:** **Dept Rank:** 5 **In-Service Date:** 03-Dec-2018 **Last Update:** 11-Sep-2015

Justification: The ballistic vests must be replaced every five years per manufactures guidelines

BU NEW [CONSTRUCT 3 BAY HEATED GARAGE W/ 2ND FLR STOR/](#) GFD \$0 \$70,000 \$320,000 \$0 \$0 \$390,000

Project No.: 1602 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Sep-2018 **Last Update:** 11-Sep-2015

Justification: To construct a 3 bay garage with a second floor within the police department property to store our vehicles, trailers, lighting plant, radar board, ATV, dirt bike, bicycles, two police motorcycles and various other equipment out of the adverse weather conditions and to provide better care and security for this equipment. The second floor would be used for records and storage of various types of police equipment and records. We are currently starting to outgrow the storage and record capacity of our building. Our record room is almost full and we are presently storing various equipment and records on the floor, in the basement hallway, electrical room, second floor kitchen area, furnace room and garage area. The building is becoming very cluttered and I am concerned within the next couple of years we are going to run out of storage room.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Police

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EO NEW [PURCHASE POLICE EQUIPMENT - DEFIBS](#) GFD \$13,215 \$0 \$0 \$0 \$0 \$13,215

Project No.: 1709 **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Aug-2016 **Last Update:** 03-Sep-2015

Justification: I would like to purchase a total of 7 new defibrillators for the police dept. We currently have three older model lifepack 500's that were purchased back in 2003 and 2004 that I would like to replace with the newer model lifepack 1000. I also have on defibrillator that is damaged that I would like to replace along with placing the remaining three units in police vehicles that don't currently have defibrillators in them. I have been told by the Physio Control Company that the normal life span for a defibrillator is 8-10 years. We respond, along with the Fire Dept., to all medical emergencies within town. On many occasions we are first on the scene and will start the initial assessment and treatment process of the injured party. I want to be sure that all our vehicles are equipped with the defibrillators, so when we do respond to the cardiac arrest or medical emergency where the defibrillator is needed we have the equipment to start administering the needed lifesaving treatment as soon as possible. I don't want to put our officers or patients into a situation where they have to wait for the needed treatment until the ambulance or an additional police cruiser arrives that is equipped with a defibrillator.

EO NEW [PURCHASE POLICE EQUIPMENT - RADARS](#) GFD \$0 \$12,000 \$0 \$0 \$0 \$12,000

Project No.: 1708 **Facility:** **Dept Rank:** 4 **In-Service Date:** 01-Aug-2017 **Last Update:** 03-Sep-2015

Justification: I am requesting the funding to purchase three mobile dash mounted radar units along with three hand held radar guns for the police dept. I would like to replace and add the dash units to our front line patrol vehicles along with the hand held units that would be used by both our officers in their police vehicles along with our officers that do radar enforcement on the two police motorcycles. The units we currently have are in need of replacement. With the equipment it will allow us to get more traffic enforcement done as the officers will be able to run moving radar while they are out on patrol with the new dash mount units and they will also have the ability to do some enforcement in areas where they can get out of the car and use the hand held units for traffic enforcement. As we all know excessive speed increases the likelihood of accident and causes a high risk to public safety. With this new and updated equipment it will give us the additional availability to work on reducing speed and making the community a little safer for all who travel through town.

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

Police

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
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EO	NEW	INSTALL CRUISER LICENSE PLATE RECOGNITION SYSTE	GFD	\$18,000	\$0	\$0	\$0	\$0	\$18,000
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Project No.: 1629 **Facility:** **Dept Rank:** 3 **In-Service Date:** 30-Jun-2016 **Last Update:** 11-Sep-2015

Justification: The Medway Police would like to improve response times and monitoring capabilities of the cruisers by adding an LPR system to one cruiser. This system will be used to identify unregistered, uninsured, and violation vehicles that may pose a safety and security risk for our residents.

Dept. Totals: \$31,215 \$99,900 \$341,725 \$0 \$0 \$472,840

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

School

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
BU	RPL	REPLACE CLASSROOM WINDOWS	GFD	\$0	\$0	\$880,000	\$0	\$0	\$880,000
Project No.: 1245 Facility: BURKE Dept Rank: 5 In-Service Date: 01-Sep-2018 Last Update: 11-Sep-2015 Justification: Burke School windows will be over 55 Years old and are in bad condition from an energy-efficiency, safety and aesthetics perspective.									
INF	RPL	REPAIR TENNIS COURTS	GFD	\$0	\$0	\$0	\$240,000	\$0	\$240,000
Project No.: 1253 Facility: MIDDLE Dept Rank: 5 In-Service Date: 01-Sep-2019 Last Update: 11-Sep-2015 Justification: Courts continue to be used by public and school athletic programs, despite numerous cracks									
BU	MAJ	REPLACE HEATING BOILER	GFD	\$0	\$450,000	\$0	\$0	\$0	\$450,000
Project No.: 1609 Facility: MCGOVERN;MEM Dept Rank: 5 In-Service Date: 01-Sep-2017 Last Update: 11-Sep-2015 Justification: Boiler at McGovern School Currently used as emergency backup only; Boiler at Memorial School is the Original and will be 20 years old									
BU	RPL	REPLACE ROOF	GFD	\$0	\$651,250	\$0	\$0	\$0	\$651,250
Project No.: 1702 Facility: MCGOVERN Dept Rank: 5 In-Service Date: 15-Aug-2017 Last Update: 02-Sep-2015 Justification: Current warranty expires June 13, 2018; July 2015 review classified its condition as "poor-nearing the end of its service life"									

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

School

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
BU	RPL	REPLACE ROOF	GFD	\$0	\$0	\$0	\$0	\$1,207,750	\$1,207,750
Project No.: 1703 Facility: BURKE/MEM Dept Rank: 5 In-Service Date: 15-Aug-2020 Last Update: 02-Sep-2015 Justification: Memorial warranty expired September 11, 2012; Burke warranty expires October 8, 2018; July 2015 review classified its condition as "fair-serviceable yet requiring repairs and routine maintenance required"									
BU	RPL	REPLACE ROOF	GFD	\$0	\$0	\$0	\$0	\$1,615,742	\$1,615,742
Project No.: 1704 Facility: MIDDLE Dept Rank: 5 In-Service Date: 15-Aug-2020 Last Update: 02-Sep-2015 Justification: Current warranty expires August 30, 2019; July 2015 review classified its condition as "fair-serviceable yet requiring repairs and routine maintenance required"									
BU	NEW	REMODEL LOBBY	GFD	\$243,100	\$0	\$0	\$0	\$0	\$243,100
Project No.: 1701 Facility: MCGOVERN Dept Rank: 4 In-Service Date: 15-Aug-2016 Last Update: 02-Sep-2015 Justification: Lobby redesigned would improve both safety and handicap accessibility									
INF	MAJ	RECONSTRUCT MCGOVERN PARKING LOT	GFD	\$224,000	\$0	\$0	\$0	\$0	\$224,000
Project No.: 1604 Facility: MCGOVERN Dept Rank: 4 In-Service Date: 01-Jul-2015 Last Update: 11-Sep-2015 Justification: Road leading to McGovern school has been consistently patched									

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

School

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
EO	RPL	UPGRADE CLASSROOM FURNITURE	OLG	\$200,000	\$0	\$0	\$0	\$0	\$200,000
Project No.: 1526 Facility: MIDDLE Dept Rank: 4 In-Service Date: 01-Sep-2016 Last Update: 11-Sep-2015 Justification: Would replace classroom furniture in Middle School									
INF	RPL	RENOVATE PARKING LOT & PAVING	GFD	\$150,000	\$0	\$0	\$0	\$0	\$150,000
Project No.: 1230 Facility: BURKE Dept Rank: 4 In-Service Date: 01-Sep-2015 Last Update: 11-Sep-2015 Justification: Plan was prepared by MPD Officer Mitchell pre-2009. Would provide safer access to Memorial and Burke campus.									
IT	RPL	UPGRADE FIRE ALARM SYSTEM	GFD	\$0	\$75,000	\$0	\$0	\$0	\$75,000
Project No.: 1610 Facility: MCGOVERN Dept Rank: 4 In-Service Date: 01-Sep-2017 Last Update: 11-Sep-2015 Justification: Proposal recommended by Fire Chief to bring building up to code at McGovern (only one remaining)									
LPF	RPR	REPAIR TENNIS COURTS	GFD	\$42,000	\$0	\$0	\$0	\$0	\$42,000
Project No.: 1729 Facility: HIGH SCHOOL Dept Rank: 3 In-Service Date: 01-Sep-2016 Last Update: 13-Oct-2015 Justification: Courts continue to be used by public and school athletic programs, despite numerous cracks									

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

School

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
BU	RPR	RECONFIGURE LIGHTING FIXTURES	GFD	\$30,000	\$0	\$0	\$0	\$0	\$30,000
Project No.: 1607 Facility: MCGOVERN;MEM Dept Rank: 3 In-Service Date: 01-Sep-2016 Last Update: 11-Sep-2015 Justification: New design would provide for better security & flow; new design would provide place for enhanced instruction at McGovern and Memorial									
BU	RPL	REPLACE CLASSROOM FLOORS	GFD	\$0	\$80,000	\$0	\$0	\$0	\$80,000
Project No.: 1248 Facility: BURKE Dept Rank: 3 In-Service Date: 01-Sep-2017 Last Update: 11-Sep-2015 Justification: Original floors at Burke; heavy use and have buckled. Safety issue for staff and students									
BU	RPL	REPLACE CLASSROOM BLINDS	GFD	\$0	\$20,000	\$20,000	\$0	\$0	\$40,000
Project No.: 1249 Facility: BURKE Dept Rank: 3 In-Service Date: 01-Sep-2018 Last Update: 11-Sep-2015 Justification: Proposing to do project in two phases split over two years.									
INF	RPL	REPLACE DOMESTIC WATER HEATER	GFD	\$0	\$0	\$40,000	\$0	\$0	\$40,000
Project No.: 1611 Facility: MCGOVERN Dept Rank: 3 In-Service Date: 01-Sep-2018 Last Update: 11-Sep-2015 Justification: The current water heater was purchased in 1991, making it 25 years old. The request is based on the assumption that due to a normal preventive replacement schedule that it will need to be replaced in the next several years.									

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes

BU BUILDING
 EO EQUIP. OTHER
 EV EQ. VEHICLES
 INF INFRASTRUCTURE
 IT TECHNOLOGY
 LPF LAND/PARK/FIELDS

Funding Codes

TAX TAX LEVY/FREE CASH
 GFD GENERAL FUND DEBT
 DE DEBT EXCLUSION
 DIF DISTRICT IMPROVEMENT FINANCING
 EFS SOLID WASTE ENTERPRISE FUND
 EFW WATER ENTERPRISE FUND
 ETA AMBULANCE ENTERPRISE FUND
 OLG OTHER LOAN/GRANT
 ETS SEWER ENTERPRISE FUND
 MXD MIXED ENTERPRISE & GEN FUNDS

Type Codes

NEW NEW
 MAJ MAJOR REPAIR / RENOVATION
 RPL RECONSTRUCT / REPLACEME
 RPR REPAIR

School

CAT.	TYPE	DESCRIPTION	SOURCE FUND	2017 COST	2018 COST	2019 COST	2020 COST	2021 COST	TOTAL COST
EO	NEW	PURCHASE THREE SCISSOR LIFTS	GFD	\$42,600	\$0	\$0	\$0	\$0	\$42,600
Project No.: 1521 Facility: MID;HIGH;MEM Dept Rank: 2 In-Service Date: 01-Jul-2015 Last Update: 11-Sep-2015 Justification: Purchase would eliminate the need to rent lifts when needed									
EV	RPL	REPLACE PICK-UP TRUCK & SANDER	GFD	\$36,000	\$0	\$0	\$0	\$0	\$36,000
Project No.: 1608 Facility: Dept Rank: 2 In-Service Date: 01-Sep-2016 Last Update: 11-Sep-2015 Justification: Existing Equipment 10 years old; rotting to sander even with low mileage									
EO	RPL	REPLACE ACID NEUTRALIZER TANK	GFD	\$30,000	\$0	\$0	\$0	\$0	\$30,000
Project No.: 1525 Facility: HIGH SCHOOL Dept Rank: 2 In-Service Date: 01-Sep-2015 Last Update: 11-Sep-2015 Justification: New system should replace original equipment; which is now 10 years old and is subject to frequent odor backups in the building									
EO	RPL	REPLACE FOOD SERVICE EQUIPMENT	OLG	\$100,000	\$0	\$0	\$0	\$0	\$100,000
Project No.: 1247 Facility: Dept Rank: 1 In-Service Date: 01-Sep-2016 Last Update: 11-Sep-2015 Justification: Much of the equipment of the food service program is ancient at Middle School & Elementaries									
Dept. Totals:				\$1,097,700	\$1,276,250	\$940,000	\$240,000	\$2,823,492	\$6,377,442

Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

Town Clerk

<i>CAT.</i>	<i>TYPE</i>	<i>DESCRIPTION</i>	<i>SOURCE FUND</i>	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
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EO	RPL	REPLACE VOTING BOOTHS	GFD	\$10,000	\$0	\$0	\$0	\$0	\$10,000
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Project No.: 1728 **Facility:** **Dept Rank:** 5 **In-Service Date:** 01-Jul-2016 **Last Update:** 13-Oct-2015

Justification: Booths have not been replaced for at least 35 yrs. Outdated, hard to set up. Franklin offered there older wooden booths last year for free. I took them, not working bulky, falling down and hard to store.

Dept. Totals:				\$10,000	\$0	\$0	\$0	\$0	\$10,000
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Town of Medway
FY 2017 to FY 2021 Capital Improvement Plan (CIP) - By Department

Category Codes	
BU	BUILDING
EO	EQUIP. OTHER
EV	EQ. VEHICLES
INF	INFRASTRUCTURE
IT	TECHNOLOGY
LPF	LAND/PARK/FIELDS

Funding Codes	
TAX	TAX LEVY/FREE CASH
GFD	GENERAL FUND DEBT
DE	DEBT EXCLUSION
DIF	DISTRICT IMPROVEMENT FINANCING
EFS	SOLID WASTE ENTERPRISE FUND
EFW	WATER ENTERPRISE FUND
ETA	AMBULANCE ENTERPRISE FUND
OLG	OTHER LOAN/GRANT
ETS	SEWER ENTERPRISE FUND
MXD	MIXED ENTERPRISE & GEN FUNDS

Type Codes	
NEW	NEW
MAJ	MAJOR REPAIR / RENOVATION
RPL	RECONSTRUCT / REPLACEME
RPR	REPAIR

	<i>2017 COST</i>	<i>2018 COST</i>	<i>2019 COST</i>	<i>2020 COST</i>	<i>2021 COST</i>	<i>TOTAL COST</i>
Grand Totals:	\$15,799,745	\$5,688,600	\$7,826,725	\$2,625,000	\$4,033,492	\$35,763,562

AGENDA ITEM #9

Discussion – Five-Year Revenue and Expense Forecast

Associated back up materials attached:

- Forecast

**TOWN OF MEDWAY - FINANCE TEAM BUDGET PROJECTIONS FY17 - FY21
REVENUE ASSUMPTIONS**

1. Property Tax	ATM Budget	Actual Recap					
	FY2016	FY2016	FY2017	FY2018	FY2019	FY2020	FY2021
Base	30,055,013	30,055,012	31,323,049	32,306,126	33,313,779	34,346,623	35,405,289
2.5% levy increase	751,375	751,375	783,076	807,653	832,844	858,666	885,132
Debt Exclusion	2,170,598	2,173,679	1,845,132	1,801,889	1,753,934	1,705,443	1,655,804
Est. New Growth	200,000	516,662	200,000	200,000	200,000	200,000	200,000
ATM Change							
Unused Capacity	(381,750)	(751,728)	(375,000)				
Total	32,795,236	32,745,000	33,776,258	35,115,668	36,100,557	37,110,732	38,146,225
2. State Aid	11,727,818	11,836,814	11,836,814	11,836,814	11,836,814	11,836,814	11,836,814
3. Local Receipts	1,992,000	1,992,000	2,100,000 *	2,100,000	2,100,000	2,100,000	2,100,000
4. Stabilization Transfers	43,684	43,684	38,546	34,396	34,396	7,997	
TOTAL	46,558,738	46,617,498	47,751,618	49,086,878	50,071,767	51,055,543	52,083,039

* Includes an estimated \$100k in Meals Tax Revenue

**TOWN OF MEDWAY - FINANCE TEAM BUDGET PROJECTIONS FY17 - FY21
EXPENDITURE ASSUMPTIONS**

**Forecasted Expenses are LEVEL
except for line items noted below:**

		FY17 Increases/(Savings)
Town base salaries	Contract Increases FY17-18; 2% increase/yr. FY19-21	\$82,806.00
School base salaries	FY17 - 21 = 2%	\$391,417.00
Expenses	1% per year	\$35,851.00
Tri-County VocTech	FY17 = estimated \$250k increase; FY18-21 = 4.18% per year (5 yr. ave)	\$250,000.00
Property/Liability Insurance	10% increase per year	\$24,700.00
Medicare	2% per year	\$11,323.00
Retirement	FY17 estimated actual; FY18-21 10.5% per year	\$237,885.00
Health Insurance	FY17 = 2.5% increase; FY18-21 5% per year	\$117,236.00
IDC Reimbursement	2% per year	(\$11,355.00)
Legal	\$120,000 per year (FY16 @ \$145k)	(\$25,000.00)
Town Administrators Office	Position replacement at \$50k (FY16 @ \$70,563)	(\$20,563.00)
Town Accountants Office	Increase of \$6,500 for Finance Director (FY16 @ \$109k)	\$6,500.00
Treasurer/Collector Office	Position replacement at \$79k (FY16 @ \$114,555 + \$8k FTM transfer)	(\$43,555.00)
MIS/Technology	Director of Communications at \$60k (partially funded in FY16 @ \$35k)	\$25,000.00
Elections	FY17 = \$30k; FY18-21 2% increase over FY16	\$20,923.00
Energy Management	Budgeted for FY17 only	
Economic Development	Administrative Assistant at \$41k (partially funded in FY16 @ \$23,076)	\$17,924.00
Fire Department	Promotion to Assistant Chief plus replacement position = \$80k	\$80,000.00
Police Department	New Police Officer position at \$54k	\$54,000.00
Senior Center	Director replacement position at \$75k (FY16 = \$40,165)	\$34,835.00
Debt Service	Estimated Actuals	(\$84,693.00)
Salary Reserve	FY19 = \$120,000	
Overlay	FY16 = \$551,000; FY17-21 = \$500,000 per year	(\$51,000.00)

Town of Medway - General Fund
Finance Team Budget Projections
FY17 - FY21

	FY16 Town Mtg	FY17	FY18	FY19	FY20	FY21
Revenues	Budgeted Revenue	Projected Revenue	Projected Revenues	Projected Revenues	Projected Revenues	Projected Revenues
<i>Property Tax Levy</i>	\$ 32,795,236	\$ 33,776,258	\$ 35,115,668	\$ 36,100,557	\$ 37,110,732	\$ 38,146,225
<i>State Aid</i>	\$ 11,727,818	\$ 11,836,814	\$ 11,836,814	\$ 11,836,814	\$ 11,836,814	\$ 11,836,814
<i>Local Receipts</i>	\$ 1,992,000	\$ 2,100,000	\$ 2,100,000	\$ 2,100,000	\$ 2,100,000	\$ 2,100,000
<i>Stabilization Funds</i>	\$ 43,684	\$ 38,546	\$ 34,396	\$ 34,396	\$ 7,997	\$ -
Subtotal:	\$ 46,558,738	\$ 47,751,618	\$ 49,086,878	\$ 50,071,767	\$ 51,055,543	\$ 52,083,039
Expenses	FY16 Budgeted Expenditures	FY17 Projected Expenditures	FY18 Projected Expenditures	FY19 Projected Expenditures	FY20 Projected Expenditures	FY21 Projected Expenditures
EDUCATION - 300						
<i>Salaries</i>	\$ 19,570,842	\$ 19,962,259	\$ 20,361,504	\$ 20,768,734	\$ 21,184,109	\$ 21,607,791
<i>Expenses</i>	\$ 5,325,552	\$ 5,378,808	\$ 5,432,596	\$ 5,486,922	\$ 5,541,791	\$ 5,597,209
Total	\$ 24,896,394	\$ 25,341,066	\$ 25,794,100	\$ 26,255,656	\$ 26,725,900	\$ 27,205,000
<i>Tri-County VocTech</i>	\$ 633,567	\$ 883,567	\$ 920,500	\$ 958,977	\$ 999,062	\$ 1,040,823
<i>Norfolk County Agricultural</i>	\$ 18,000	\$ 18,000	\$ 18,000	\$ 18,000	\$ 18,000	\$ 18,000
EDUCATION TOTAL	\$ 25,547,961	\$ 26,242,633	\$ 26,732,600	\$ 27,232,633	\$ 27,742,962	\$ 28,263,823
SELECTMEN - 122						
<i>Salaries</i>	\$ 346,592	\$ 332,550	\$ 339,201	\$ 345,985	\$ 352,904	\$ 359,962
<i>Expenses</i>	\$ 30,325	\$ 30,628	\$ 30,935	\$ 31,244	\$ 31,556	\$ 31,872
Total	\$ 376,917	\$ 363,178	\$ 370,135	\$ 377,228	\$ 384,461	\$ 391,834
HUMAN RESOURCES - 125						
<i>Salaries</i>	\$ 157,969	\$ 161,128	\$ 164,351	\$ 167,638	\$ 170,991	\$ 174,411
<i>Expenses</i>	\$ 61,486	\$ 62,101	\$ 62,722	\$ 63,349	\$ 63,983	\$ 64,622
Total	\$ 219,455	\$ 223,229	\$ 227,073	\$ 230,987	\$ 234,973	\$ 239,033
FINANCE COMMITTEE - 132						
<i>Salaries</i>	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
<i>Expenses</i>	\$ 2,000	\$ 2,000	\$ 2,000	\$ 2,000	\$ 2,000	\$ 2,000
<i>Reserve Fund - 131</i>	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000
Total	\$ 102,000	\$ 102,000	\$ 102,000	\$ 102,000	\$ 102,000	\$ 102,000
TOWN ACCOUNTANT - 135						
<i>Salaries</i>	\$ 186,484	\$ 206,466	\$ 210,596	\$ 214,808	\$ 219,104	\$ 223,486
<i>Expenses</i>	\$ 55,032	\$ 55,582	\$ 56,138	\$ 56,700	\$ 57,267	\$ 57,839
Total	\$ 241,516	\$ 262,049	\$ 266,734	\$ 271,507	\$ 276,370	\$ 281,325
ASSESSORS - 141						
<i>Salaries</i>	\$ 207,468	\$ 211,617	\$ 215,850	\$ 220,167	\$ 224,570	\$ 229,061
<i>Expenses</i>	\$ 36,750	\$ 19,948	\$ 20,147	\$ 20,348	\$ 20,552	\$ 20,757
Total	\$ 244,218	\$ 231,565	\$ 235,997	\$ 240,515	\$ 245,122	\$ 249,819
TREASURER/COLLECTOR - 145						
<i>Salaries</i>	\$ 262,940	\$ 223,773	\$ 228,248	\$ 232,813	\$ 237,469	\$ 242,219
<i>Expenses</i>	\$ 62,000	\$ 62,620	\$ 63,246	\$ 63,879	\$ 64,517	\$ 65,163
Total	\$ 324,940	\$ 286,393	\$ 291,494	\$ 296,692	\$ 301,987	\$ 307,381

Town of Medway - General Fund
Finance Team Budget Projections
FY17 - FY21

Expenses		FY16	FY17	FY18	FY19	FY20	FY21
		Budgeted Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures
LEGAL - 151							
	<i>Expenses</i>	\$ 145,000	\$ 120,000	\$ 120,000	\$ 120,000	\$ 120,000	\$ 120,000
	Total	\$ 145,000	\$ 120,000	\$ 120,000	\$ 120,000	\$ 120,000	\$ 120,000
MIS/TECHNOLOGY - 155							
	<i>Salaries</i>	\$ 208,683	\$ 238,357	\$ 243,124	\$ 247,986	\$ 252,946	\$ 258,005
	<i>Expenses</i>	\$ 233,450	\$ 235,785	\$ 238,142	\$ 240,524	\$ 242,929	\$ 245,358
	Total	\$ 442,133	\$ 474,141	\$ 481,266	\$ 488,510	\$ 495,875	\$ 503,363
TOWN CLERK - 161							
	<i>Salary Elected Official</i>	\$ 68,285	\$ 69,651	\$ 71,044	\$ 72,465	\$ 73,914	\$ 75,392
	<i>Salaries Other</i>	\$ 42,693	\$ 43,547	\$ 44,418	\$ 45,306	\$ 46,212	\$ 47,137
	<i>Expenses</i>	\$ 2,682	\$ 2,709	\$ 2,736	\$ 2,763	\$ 2,791	\$ 2,819
	Total	\$ 113,660	\$ 115,906	\$ 118,197	\$ 120,534	\$ 122,917	\$ 125,347
ELECTIONS - 162							
	<i>Salaries</i>	\$ 2,821	\$ 15,000	\$ 2,877	\$ 2,935	\$ 2,994	\$ 3,054
	<i>Expenses</i>	\$ 6,256	\$ 15,000	\$ 6,319	\$ 6,382	\$ 6,446	\$ 6,510
	Total	\$ 9,077	\$ 30,000	\$ 9,196	\$ 9,317	\$ 9,439	\$ 9,564
REGISTRAR - 163							
	<i>Salaries</i>	\$ 450	\$ 450	\$ 450	\$ 450	\$ 450	\$ 450
	<i>Expenses</i>	\$ 4,255	\$ 4,298	\$ 4,341	\$ 4,384	\$ 4,428	\$ 4,472
	Total	\$ 4,705	\$ 4,748	\$ 4,791	\$ 4,834	\$ 4,878	\$ 4,922
CONSERVATION - 171							
	<i>Salaries</i>	\$ 48,638	\$ 49,611	\$ 50,603	\$ 51,615	\$ 52,647	\$ 53,700
	<i>Expenses</i>	\$ 2,404	\$ 2,428	\$ 2,452	\$ 2,477	\$ 2,502	\$ 2,527
	Total	\$ 51,042	\$ 52,039	\$ 53,055	\$ 54,092	\$ 55,149	\$ 56,227
PLANNING BOARD - 175							
	<i>Salaries</i>	\$ 71,500	\$ 72,930	\$ 74,389	\$ 75,876	\$ 77,394	\$ 78,942
	<i>Expenses</i>	\$ 7,860	\$ 7,939	\$ 8,018	\$ 8,098	\$ 8,179	\$ 8,261
	Total	\$ 79,360	\$ 80,869	\$ 82,407	\$ 83,975	\$ 85,573	\$ 87,203
ZONING BOARD OF APPEALS - 176							
	<i>Salaries</i>	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
	<i>Expenses</i>	\$ 2,050	\$ 2,250	\$ 2,250	\$ 2,250	\$ 2,250	\$ 2,250
	Total	\$ 2,050	\$ 2,250	\$ 2,250	\$ 2,250	\$ 2,250	\$ 2,250
ENERGY COMMITTEE - 178							
	<i>Salaries</i>	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
	<i>Expenses</i>	\$ 400	\$ 800	\$ 800	\$ 800	\$ 800	\$ 800
	Total	\$ 400	\$ 800	\$ 800	\$ 800	\$ 800	\$ 800
ENERGY MANAGEMENT - 177							
	<i>Salaries</i>	\$ 15,907	\$ 15,907	\$ -	\$ -	\$ -	\$ -
	<i>Expenses</i>	\$ 1,000	\$ 1,000	\$ -	\$ -	\$ -	\$ -
	Total	\$ 16,907	\$ 16,907	\$ -	\$ -	\$ -	\$ -
ECONOMIC DEVELOPMENT - 182							
	<i>Salaries</i>	\$ 112,511	\$ 133,044	\$ 135,705	\$ 138,419	\$ 141,187	\$ 144,011
	<i>Expenses</i>	\$ 4,345	\$ 4,388	\$ 4,432	\$ 4,477	\$ 4,521	\$ 4,567
	Total	\$ 116,856	\$ 137,432	\$ 140,137	\$ 142,895	\$ 145,708	\$ 148,577
ANNUAL TOWN REPORT - 195							
	<i>Expenses</i>	\$ 850	\$ 850	\$ 850	\$ 850	\$ 850	\$ 850
	Total	\$ 850	\$ 850	\$ 850	\$ 850	\$ 850	\$ 850
GENERAL GOVERNMENT TOTALS		\$ 2,491,086	\$ 2,504,355	\$ 2,506,382	\$ 2,546,986	\$ 2,588,352	\$ 2,630,496

Town of Medway - General Fund
Finance Team Budget Projections
FY17 - FY21

Expenses	FY16 Budgeted Expenditures	FY17 Budgeted Expenditures	FY18 Projected Expenditures	FY19 Projected Expenditures	FY20 Projected Expenditures	FY21 Projected Expenditures
DEBT SERVICE - 700						
<i>Long Term Principal</i>	\$ 2,620,486	\$ 2,408,316	\$ 2,843,916	\$ 2,828,916	\$ 2,733,916	\$ 2,708,916
<i>Long Term Interest</i>	\$ 838,574	\$ 916,051	\$ 1,177,622	\$ 1,079,259	\$ 980,499	\$ 886,596
<i>Short Term Interest</i>	\$ 50,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000
Total	\$ 3,509,060	\$ 3,424,367	\$ 4,121,538	\$ 4,008,175	\$ 3,814,415	\$ 3,695,512
UNCLASSIFIED						
<i>Property/Liability Insurance - 194</i>	\$ 247,000	\$ 271,700	\$ 298,870	\$ 328,757	\$ 361,633	\$ 397,796
<i>Medicare - 910</i>	\$ 416,172	\$ 424,495	\$ 432,985	\$ 441,645	\$ 450,478	\$ 459,488
<i>Life Insurance - 910</i>	\$ 8,480	\$ 8,480	\$ 8,480	\$ 8,480	\$ 8,480	\$ 8,480
<i>Retirement - 911</i>	\$ 1,902,885	\$ 2,140,770	\$ 2,365,551	\$ 2,613,934	\$ 2,888,397	\$ 3,191,678
<i>Workers Comp - 912</i>	\$ 117,500	\$ 117,500	\$ 117,500	\$ 117,500	\$ 117,500	\$ 117,500
<i>Unemployment Insurance - 913</i>	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000
<i>Health Insurance - 914</i>	\$ 4,689,432	\$ 4,806,668	\$ 5,047,001	\$ 5,299,351	\$ 5,564,319	\$ 5,842,535
<i>Salary Reserve</i>	\$ 8	\$ -	\$ -	\$ 120,000	\$ -	\$ -
Total	\$ 7,471,477	\$ 7,859,613	\$ 8,360,387	\$ 9,019,667	\$ 9,480,806	\$ 10,107,477
TOWN WIDE GENERAL GOVERNMENT TOTAL	\$ 10,980,537	\$ 11,283,980	\$ 12,481,925	\$ 13,027,842	\$ 13,295,221	\$ 13,802,989

Town of Medway - General Fund
Finance Team Budget Projections
FY17 - FY21

Expenses	FY16	FY17	FY18	FY19	FY20	FY21
	Budgeted Expenditures	Budgeted Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures
DEPT OF PUBLIC SERVICES						
DPS Salaries - 422	\$ 565,659	\$ 576,972	\$ 588,512	\$ 600,282	\$ 612,287	\$ 624,533
DPS Expenses - 422	\$ 298,150	\$ 301,132	\$ 304,143	\$ 307,184	\$ 310,256	\$ 313,359
Street Lighting Expenses - 424	\$ 30,200	\$ 30,502	\$ 30,807	\$ 31,115	\$ 31,426	\$ 31,741
Road Repair Expenses - 421	\$ 166,000	\$ 167,660	\$ 169,337	\$ 171,030	\$ 172,740	\$ 174,468
Building Maintenance Salaries - 192	\$ 76,714	\$ 78,248	\$ 79,813	\$ 81,410	\$ 83,038	\$ 84,698
Building Maintenance Expenses - 192	\$ 137,150	\$ 138,522	\$ 139,907	\$ 141,306	\$ 142,719	\$ 144,146
Traffic Signals Expenses - 293	\$ 1,700	\$ 1,700	\$ 1,700	\$ 1,700	\$ 1,700	\$ 1,700
Tree/Moth Agent Salary - 294	\$ 1,500	\$ 1,500	\$ 1,500	\$ 1,500	\$ 1,500	\$ 1,500
Tree/Moth Agent Expense - 294	\$ 1,000	\$ 1,000	\$ 1,000	\$ 1,000	\$ 1,000	\$ 1,000
Total	\$ 1,278,073	\$ 1,297,235	\$ 1,316,718	\$ 1,336,526	\$ 1,355,667	\$ 1,376,145
SNOW AND ICE REMOVAL - 423						
Salaries	\$ 41,500	\$ 41,500	\$ 41,500	\$ 41,500	\$ 41,500	\$ 41,500
Expenses	\$ 384,377	\$ 384,377	\$ 384,377	\$ 384,377	\$ 384,377	\$ 384,377
Total	\$ 425,877	\$ 425,877	\$ 425,877	\$ 425,877	\$ 425,877	\$ 425,877
CEMETERY COMMISSION - 491						
Expenses	\$ 100	\$ 100	\$ 100	\$ 100	\$ 100	\$ 100
Total	\$ 100	\$ 100	\$ 100	\$ 100	\$ 100	\$ 100
PUBLIC WORKS TOTAL	\$ 1,704,050	\$ 1,723,212	\$ 1,742,695	\$ 1,762,503	\$ 1,781,644	\$ 1,802,122
FIRE - 220						
Salaries	\$ 614,379	\$ 708,267	\$ 722,432	\$ 736,881	\$ 751,618	\$ 766,651
Expenses	\$ 99,500	\$ 100,495	\$ 101,500	\$ 102,515	\$ 103,540	\$ 104,575
Total	\$ 713,879	\$ 808,762	\$ 823,932	\$ 839,395	\$ 855,158	\$ 871,226
POLICE - 210						
Salaries	\$ 2,030,436	\$ 2,136,836	\$ 2,179,573	\$ 2,223,164	\$ 2,267,627	\$ 2,312,980
Expenses	\$ 225,862	\$ 228,121	\$ 230,402	\$ 232,706	\$ 235,033	\$ 237,383
Total	\$ 2,256,298	\$ 2,364,957	\$ 2,409,974	\$ 2,455,870	\$ 2,502,660	\$ 2,550,363
POLICE/FIRE COMMUNICATIONS - 215						
Salaries	\$ 243,151	\$ 248,014	\$ 252,974	\$ 258,034	\$ 263,194	\$ 268,458
Expenses	\$ 13,080	\$ 13,211	\$ 13,343	\$ 13,476	\$ 13,611	\$ 13,747
Total	\$ 256,231	\$ 261,225	\$ 266,317	\$ 271,510	\$ 276,806	\$ 282,206
ANIMAL CONTROL - 292						
Expenses	\$ 46,475	\$ 45,484	\$ 45,484	\$ 45,484	\$ 45,484	\$ 45,484
Total	\$ 46,475	\$ 45,484	\$ 45,484	\$ 45,484	\$ 45,484	\$ 45,484
BUILDING INSPECTOR - 241						
Salaries	\$ 181,660	\$ 185,293	\$ 188,999	\$ 192,779	\$ 196,635	\$ 200,567
Expenses	\$ 5,300	\$ 5,353	\$ 5,407	\$ 5,461	\$ 5,515	\$ 5,570
Total	\$ 186,960	\$ 190,646	\$ 194,406	\$ 198,240	\$ 202,150	\$ 206,138
PUBLIC SAFETY TOTAL	\$ 3,459,843	\$ 3,671,073	\$ 3,740,113	\$ 3,810,499	\$ 3,882,258	\$ 3,955,416

Town of Medway - General Fund
Finance Team Budget Projections
FY17 - FY21

Expenses	FY16	FY17	FY18	FY19	FY20	FY21
	Budgeted Expenditures	Budgeted Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures
BOARD OF HEALTH - 510						
Salaries	\$ 101,213	\$ 103,237	\$ 105,302	\$ 107,408	\$ 109,556	\$ 111,747
Expenses	\$ 27,975	\$ 28,255	\$ 28,537	\$ 28,823	\$ 29,111	\$ 29,402
Total	\$ 129,188	\$ 131,492	\$ 133,839	\$ 136,231	\$ 138,667	\$ 141,149
COUNCIL ON AGING - 541						
Salaries	\$ 86,001	\$ 123,253	\$ 125,718	\$ 128,232	\$ 130,797	\$ 133,413
Expenses	\$ 53,061	\$ 53,592	\$ 54,128	\$ 54,669	\$ 55,215	\$ 55,768
Total	\$ 139,062	\$ 176,844	\$ 179,845	\$ 182,901	\$ 186,012	\$ 189,180
VETERANS SERVICES - 543						
Salaries	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
Expenses	\$ 97,054	\$ 97,054	\$ 97,054	\$ 97,054	\$ 97,054	\$ 97,054
Total	\$ 97,054	\$ 97,054	\$ 97,054	\$ 97,054	\$ 97,054	\$ 97,054
HEALTH & HUMAN SERVICES TOTAL	\$ 365,304	\$ 405,390	\$ 410,739	\$ 416,186	\$ 421,733	\$ 427,384
PARKS DEPT - 650						
Salaries	\$ 223,188	\$ 227,652	\$ 232,205	\$ 236,849	\$ 241,586	\$ 246,418
Expenses	\$ 100,102	\$ 101,103	\$ 102,114	\$ 103,135	\$ 104,167	\$ 105,208
Total	\$ 323,290	\$ 328,755	\$ 334,319	\$ 339,984	\$ 345,752	\$ 351,626
LIBRARY - 610						
Salaries	\$ 231,582	\$ 236,214	\$ 240,938	\$ 245,757	\$ 250,672	\$ 255,685
Expenses	\$ 110,282	\$ 111,385	\$ 112,499	\$ 113,624	\$ 114,760	\$ 115,907
Total	\$ 341,864	\$ 347,598	\$ 353,437	\$ 359,380	\$ 365,432	\$ 371,593
SUNSHINE GROUP - 654						
Salaries	\$ 32,621	\$ 33,273	\$ 33,939	\$ 34,618	\$ 35,310	\$ 36,016
Expenses	\$ 14,040	\$ 14,180	\$ 14,322	\$ 14,465	\$ 14,610	\$ 14,756
Total	\$ 46,661	\$ 47,454	\$ 48,261	\$ 49,083	\$ 49,920	\$ 50,772
DISABILITY COMMISSION - 545						
Expenses	\$ 500	\$ 500	\$ 500	\$ 500	\$ 500	\$ 500
Total	\$ 500	\$ 500	\$ 500	\$ 500	\$ 500	\$ 500
MEMORIAL COMMISSION - 670						
Expenses	\$ 2,000	\$ 600	\$ 600	\$ 600	\$ 600	\$ 600
Total	\$ 2,000	\$ 600	\$ 600	\$ 600	\$ 600	\$ 600
HISTORICAL COMMISSION - 691						
Expenses	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
Total	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
CULTURE AND RECREATION TOTAL	\$ 714,315	\$ 724,907	\$ 737,117	\$ 749,547	\$ 762,204	\$ 775,091
Total Town Meeting Appropriation:	\$ 45,263,096	\$ 46,555,552	\$ 48,351,570	\$ 49,546,196	\$ 50,474,375	\$ 51,657,320

Town of Medway - General Fund
 Finance Team Budget Projections
 FY17 - FY21

	FY16	FY17	FY18	FY19	FY20	FY21
Expenditures Not requiring Appropriation	Budgeted Expenditures	Budgeted Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures	Projected Expenditures
<i>Tax Title</i>	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000
<i>Cherry Sheet Offsets</i>	\$ 421,788	\$ 410,001	\$ 410,001	\$ 410,001	\$ 410,001	\$ 410,001
<i>Cherry Sheet Charges</i>	\$ 701,122	\$ 714,743	\$ 714,743	\$ 714,743	\$ 714,743	\$ 714,743
<i>Overlay</i>	\$ 551,000	\$ 500,000	\$ 500,000	\$ 500,000	\$ 500,000	\$ 500,000
<i>Enterprise IDC Offset</i>	\$ (567,768)	\$ (579,123)	\$ (590,706)	\$ (602,520)	\$ (614,570)	\$ (626,862)
Total	\$ 1,116,142	\$ 1,055,621	\$ 1,044,038	\$ 1,032,224	\$ 1,020,174	\$ 1,007,882
Total Operating Budget	\$ 46,379,238	\$ 47,611,172	\$ 49,395,608	\$ 50,578,421	\$ 51,494,548	\$ 52,665,202
MONETARY ARTICLES						
<i>Ambulance Subsidy</i>	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000
<i>Thayer House Revolving Fund</i>	\$ 20,000	\$ 20,000				
<i>Medway Family Day</i>	\$ 9,500	\$ 9,500				
<i>Transfer to OPEB Trust</i>		\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000
Total Monetary Articles	\$ 179,500	\$ 279,500	\$ 250,000	\$ 250,000	\$ 250,000	\$ 250,000
Total Appropriated and Non-Appropriated Budget	\$ 46,558,738	\$ 47,890,672	\$ 49,645,608	\$ 50,828,421	\$ 51,744,548	\$ 52,915,202
<i>Revenues (from Above)</i>	\$ 46,558,738	\$ 47,751,618	\$ 49,086,878	\$ 50,071,767	\$ 51,055,543	\$ 52,083,039
General Fund Surplus/(Deficit)	\$ -	\$ (139,055)	\$ (558,731)	\$ (756,653)	\$ (689,006)	\$ (832,163)

AGENDA ITEM #10

**Approval – One-Day Liquor License
Applications – Hithendra Pothu, 12/31/14
and Linda Spiller, 7/30/16**

Associated back up materials attached:

- Hithendra Pothu's application
- Linda Spiller's application and Police Chief's recommendation

Proposed motion: I move that the Board approve one-day liquor licenses for Hithendra Pothu and Linda Spiller for their events at the Thayer Homestead on December 31, 2015 and July 30, 2016 respectively subject to fulfillment of the Police Chief's recommendations and evidence of appropriate insurance coverage.



Town of Medway

BOARD OF SELECTMEN

155 Village Street, Medway MA 02053
Ph. (508) 533-3264 Fax: (508) 321-4899

APPLICATION FOR SPECIAL ONE-DAY LIQUOR LICENSE

MGL c.138, §14

Application for the purpose of selling or dispensing the following beverages permitted by law. A Section 12 license holder may not also be granted a Section 14 (one-day) license unless event is held at a separate location. A person holding a Section 14 license cannot purchase alcoholic beverages from a package store. Purchase must be made from a licensed wholesaler/importer, manufacturer, farmer-winery/brewery, or special permit holder.

For Profit Businesses are eligible for wine and malt license only.

Application must be submitted at least two weeks prior to event.

Fee: \$50 (May be waived at Board of Selectmen's discretion)

All Alcohol Wine and Malt

Event FAMILY REUNION FOR NEW YEAR

Name of Organization/Applicant HITHENDRA POTHU

Address 4 CHARLES VIEW LN, MEDWAY, 02053

FID# _____

Phone _____

Non-Profit Organization Y N

Attach non-profit certificate of exemption

Event Location THAYER HOMESTEAD

Event Date 12/31/05

Event Hours (No later than 1:00 AM; Last call 12:30 AM) 9 PM - 1 AM

Is event open to the general public? Y N

Estimated attendance 20-25

Will there be an age restriction? Y N

Minimum age allowed:

How, where and by whom will ID's be checked? EVENT ORGANIZER
AT DOOR

Is there a charge for the beverages? Y _____ N X
Price structure: _____

Alcohol server(s) _____
Attach Proof of Alcohol Server Training _____

Provisions for Security, Detail Officer _____

Does the applicant have knowledge of State liquor laws? Y X N

Experience _____

The following may be required:
Police Dept. – Detail; Fire Dept. – Detail; Board of Health – Food Permit; Building Dept. – Tent Permit

Date of Application 12/17/15

Applicant's Signature R. Hithe

Applicant's Name HITHENDRA POTHU

Address 4, CHARLES VIEW LN, MEDWAY, MA-02053

Phone _____

The Board of Selectmen's Office will forward this application to the Police, Fire, and Building Departments and the Board of Health for approval and recommendations.

Police Department _____
315 Village St _____ Date _____

Fire Department _____
44 Milford St _____ Date _____

Board of Health _____
Town Hall, 2nd Fl _____ Date _____

Building Department _____
Town Hall, 1st Fl _____ Date _____



Town of Medway

BOARD OF SELECTMEN

155 Village Street, Medway MA 02053
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APPLICATION FOR SPECIAL ONE-DAY LIQUOR LICENSE

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For Profit Businesses are eligible for wine and malt license only.

Application must be submitted at least two weeks prior to event.

Fee: \$50 (May be waived at Board of Selectmen's discretion)

All Alcohol _____ Wine and Malt

Event Bridal Shower

Name of Organization/Applicant Linda Spiller

Address 10 Ellis St. Medway MA 02053

FID#

Pho:

Non-Profit Organization Y _____ N

Attach non-profit certificate of exemption

Event Location Thayer Homestead

Event Date 7-30-16

Event Hours (No later than 1:00 AM; Last call 12:30 AM)

Is event open to the general public? Y _____ N

Estimated attendance 40

Will there be an age restriction? Y _____ N

Minimum age allowed:

How, where and by whom will ID's be checked? Applicant

Is there a charge for the beverages? Y _____ N

Price structure: _____
Alcohol server(s)
Attach Proof of Alcohol Server Training

Provisions for Security, Detail Officer _____

Does the applicant have knowledge of State liquor laws? Y N _____

Experience Ex waitress

The following may be required:
Police Dept. – Detail; Fire Dept. – Detail; Board of Health – Food Permit; Building Dept. – Tent Permit

Date of Application 12-8-15

Applicant's Signature Linda Spiller

Applicant's Name Linda Spiller

Address 10 Ellis St. Medway

Pho: _____

The Board of Selectmen's Office will forward this application to the Police, Fire, and Building Departments and the Board of Health for approval and recommendations.

Police Department _____
315 Village St _____ Date _____

Fire Department _____
44 Milford St _____ Date _____

Board of Health _____
Town Hall, 2nd Fl _____ Date _____

Building Department _____
Town Hall, 1st Fl _____ Date _____



Medway Police Department

315 Village Street
Medway, MA 02053

Phone: 508-533-3212
FAX: 508-533-3216
Emergency: 911

Allen M. Tingley
Chief of Police

December 11, 2015

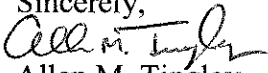
To: Michael Boynton
Town Administrator

From: Allen M. Tingley
Chief of Police

Re: One day liquor license- Thayer Property- Bridal Shower

I have reviewed the request from Linda Spiller for a one day wine/malt license for a bridal shower, to be held at the Thayer House, 2B Oak Street, on August 9, 2016. I approve of the issuance of this one day liquor license with the stipulation that the wine/malt will be purchased from a licensed alcohol wholesale distributor, as indicated on the license application and the Town of Medway's Alcohol Policy and there will be no on-street parking on Mechanic Street and Oak Street and a responsible adult with some knowledge of Mass. Liquor laws will be checking ID's of individuals being served alcohol at the party.

Sincerely,


Allen M. Tingley
Chief of Police

AGENDA

ITEM #11

Town Counsel Appointment

Associated back up materials attached:

- Correspondence from Petrini & Assoc.
- Kopelman & Paige proposal

December 15, 2015

BY EMAIL AND FIRST CLASS MAIL

Board of Selectmen
Town of Medway
155 Village Street
Medway, MA 02053

Re: **Town Counsel Services**

Dear Members of the Board of Selectmen:

This letter is to notify you that as of January 8, 2016, Attorney Barbara J. Saint André will no longer be practicing with the firm of Petrini and Associates, P.C. and will be joining Kopelman and Paige, P.C. Be advised that established case law obligates a firm and a departing attorney to notify clients in writing of the change in their relationship before contacting the clients about the change. This letter serves as that notice.

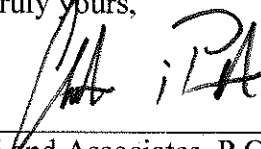
As the client, you have the right to choose which attorney will handle your legal matters. You can choose to have Petrini and Associates, P.C., Attorney Saint André, or other counsel, work on any or all of the items listed on the attachment, or on none of them, at your discretion. Attached please find a list of matters on which we have been representing the Town of Medway.

Should you choose to transfer any of the matters at issue to Attorney Saint André or other counsel, arrangements will be made to transfer files in an efficient and timely manner so as to protect your interests in ongoing matters. Please inform us in writing of any election you may make to transfer matters and specifically authorize the transfer of related files. For your convenience, the attached list of matters includes columns allowing you to indicate how you would like such matters to be handled, and completion of the same will constitute notice of any determination to transfer matters and authorization to transfer the related files.

December 15, 2015
Page 2

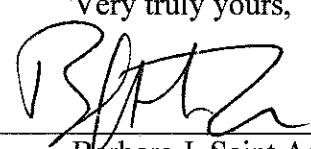
Should you wish to have Petrini and Associates continue to handle such matters, Attorney Christopher J. Petrini or Peter Mello will handle these matters, to be determined mutually based on the town's preference. Until a decision is made as described herein, the firm will continue to represent your legal interests.

Very truly yours,



Petrini and Associates, P.C.
By: Christopher J. Petrini, Esq.

Very truly yours,



Barbara J. Saint André, Esq.

To: Christopher J. Petrini, Esq.
 Petrini and Associates, P.C.
 372 Union Avenue
 Framingham, MA 01702
cpetrini@petrinilaw.com

Matter Name	Transfer to Attorney Saint André	Remain with Petrini and Associates	Transfer to Other Counsel
All Matters	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
OR			
Oak Grove Redevelopment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Maritime Housing Fund, LLC v. PEDB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Maritime Housing Fund v. ZBA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Certification:

This is to inform you, in connection with your letter of December 15, 2015, that the above-completed document specifies those matters, if any, that the Client wishes to: continue to be handled by Petrini and Associates; transfer to Attorney Barbara J. Saint André; and/or, transfer to other counsel. If the Client elects to have matter(s) transferred to another counsel, that person or firm's name and address is set forth on this form or on a sheet physically attached hereto.

It is hereby recognized that return of this form to Attorney Petrini, by regular mail, facsimile or e-mail, at the address or numbers specified above, will be shared with Attorney Saint André or other named counsel, in the event another is named.

I certify by affixing my signature below that the appropriate appointing authority has taken the action specified herein, and if matters will be transferred to Attorney Saint André or another named counsel, authorized the transfer of any files related to such matters to Attorney Saint André or other counsel as specified.

Dated: _____

Signed: _____



KOPELMAN AND PAIGE, P.C.
The Leader in Public Sector Law

101 Arch Street
Boston, MA 02110
T: 617.556.0007
F: 617.654.1735
www.k-plaw.com

December 17, 2015

Lauren F. Goldberg
lgoldberg@k-plaw.com

BY ELECTRONIC MAIL ONLY (mboynton@townofmedway.org)

Mr. Michael Boynton
Town Administrator
Town Hall
155 Village Street
Medway, MA 02053

Re: Price Proposal – Town Counsel Services

Dear Mr. Boynton:

I am writing as Managing Attorney at Kopelman and Paige, P.C. You have requested a price proposal for the provision of Town Counsel services. As you know, Attorney Barbara J. St. André is returning to Kopelman and Paige, P.C. as a Shareholder shortly after the new year. As you have been informed, the Town may choose to make no change with respect to Town Counsel, may follow Attorney St. André to Kopelman and Paige, P.C., or may select a different attorney or firm. In connection with this information, you have asked whether the firm would be willing to honor the terms of the Town's current contract for legal services. The firm will, of course, be pleased to do so, although we talked about possibly refining the financial agreement somewhat. Although you did not share the current contract with me, you indicated that there are two main components of the contract – the first is a retainer amount of \$62,000 per year, which amount covers general Town Counsel services. The second component is for legal services provided outside the retainer, for which billing is done on an hourly basis.

Description of Services Under the Retainer Agreement

We would propose that the retainer amount of \$62,000 be paid in 12 equal installments of \$5,166.67, to cover basic Town Counsel services, as outlined below. Of course, for purposes of tracking and assessing legal services provided under the retainer, such services would be itemized and accounted for as if the Town was being billed on an hourly basis. The retainer would cover the following:

- Unlimited telephone consultation with the Town Administrator or such other department heads or board chairpersons as maybe authorized;
- Research and writing of legal opinion e-mails and letters;
- Attendance at all sessions of Annual and Special Town Meeting as well as a pre-town meeting held in connection therewith and preparation/review of town meeting warrant articles and motions;

Mr. Michael Boynton
Town Administrator
December 17, 2015
Page 2

- Attendance at board and committee meetings (up to two per month);
- Review and drafting of contracts;
- Review of routine legal instruments, such as restrictive covenants, conservation restrictions, releases and easements;
- Review of subdivision documents; and
- Review and drafting of decisions of multiple-member bodies.

Of course, any travel time spent in performing these services would be covered under the retainer and would not be separately charged to the Town. Two client seminars would also be provided within the retainer at no charge.

For services provided outside the retainer, the Town will pay only for services it utilizes, billed at the agreed-upon hourly rate, including travel time. Such matters would be itemized separately on the Town's bill so that they could be easily tracked. Matters outside the retainer would include:

- Adversarial proceedings such as litigation, administrative appeals and complaints, collective bargaining, and arbitration and grievance hearings;
- Significant matters not falling within the description of basic Town Counsel services above, including but not limited to:
 - Real estate transactions, including drafting of , complete recodification of zoning or general bylaws (as compared to review and drafting of particular bylaws, or reviewing technical work completed by others);
 - Comprehensive permits;
 - Negotiation of host community agreements; and
 - Development projects.

Legal services to be paid for by other parties, including but not limited to assistance with permitting matters for which the applicant pays for legal services, street acceptances for which the developer pays for legal services, or insured claims for which fees are paid by the insurer, will be billed outside the retainer even if they would otherwise fall within the retainer.

In addition to the retainer and charges for legal services outside thereof, the Town will be billed at cost for out-of pocket disbursements, such as copying charges, filing fees, service of process, deposition transcription fees, and court fees. Travel time for work outside the retainer would be billed on a portal to portal basis from our Boston office to the Town, or such closer location actually travelled by the attorney. Please note, however, that many of the courts and administrative agencies in which the Town may have matters pending are within a short walking distance of our Boston office, reducing travel time expenses.

Mr. Michael Boynton
Town Administrator
December 17, 2015
Page 3

Hourly Rate

I understand that Attorney St. André's billable hour rate includes a particular rate for services provided by her, as well as a somewhat lower rate for associates and paralegals at the firm. We would be happy to honor that rate structure, with shareholders at the firm being billed at the same rate as Attorney St. André's rate under the existing contract, and associates and paralegals at the lower quoted rate.

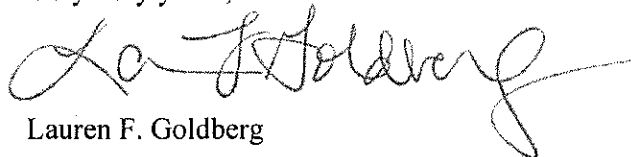
In the alternative, we offer the Town a single blended rate of \$185.00 per hour, billed in 1/10th of an hour increments, for all work provided outside of the retainer. Note further that under this alternative, paralegal time would be billed at ½ the attorney rate, or \$92.50.

Conclusion

As you may know, over 98% of our firm's work is municipal legal work. We are thus keenly aware that our revenue derives from public sector funds. We understand the cost constraints and service expectations facing municipalities. The firm draws heavily on the economies of scale stemming from our depth of experience and expertise to limit the costs of our services. We pride ourselves on the fact that our clients can utilize our services for just what they need, when and as they need them. While this fee proposal was developed based on our understanding of the Town's current needs and expectations, we are always available to discuss terms that best meet the Town's ability to manage its limited resources.

Please let me know if I am able to provide you with any additional information about the firm. Thank you for the opportunity to provide this proposal.

Very truly yours,

A handwritten signature in cursive script, appearing to read "Lauren F. Goldberg".

Lauren F. Goldberg

LFG/

AGENDA ITEM #12

Action Items from Previous Meeting

Associated back up materials attached:

- Action item list

	DATE	ACTION ITEMS BOS	WHO	COMPLETED
1	7/6/2010	Street acceptance progress	S. Affleck-Childs	Ongoing
2	9/20/2010	Route 109 Project	DPS	Ongoing
3	2/4/2013	Brentwood Project	DPS	Ongoing
4	2/3/2014	Cable license renewal process; Mtg of Cable Advisory Com	BOS	Verizon & Comcast notice received; further action Fall 2015
5	7/28/2014	DPS Facility Building Project	DPS/TA/Committee	Ongoing
6	1/20/2015	Playground concepts for Idylbrook and existing locations	G. Trindade	Future Town Meeting
7	2/24/2015	\$1.1 mil environmental bond bill; Choate Improvements; prepare technical proposal for state funding in FY17	TA/BOS	Ongoing
8	2/28/2015	Database of searchable minutes/Update Town Website	TA/IS	Fall 2015
9	6/1/2015	Road and Sidewalk Repair and Construction Strategy/Plan	DPS	Winter 2016
10	10/13/2015	Policy on Land Acceptances - BOS v ConCom	BOS/ PEBD	January 2016
11	11/2/2015	Discussion - solid waste and recycling fees	BOS/DPS	Winter 2016

AGENDA

ITEM #13

Approval of Warrants

Warrants to be provided at meeting.

AGENDA

ITEM #14

Approval of Minutes

Associated back up materials attached:

- December 7, 2015 draft minutes

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Board of Selectmen's Meeting
Monday, December 7, 2015 – 7:00 PM
Sanford Hall
155 Village Street

Present: John Foresto, Chair; Maryjane White, Vice Chair; Dennis Crowley, Member; Glenn Trindade, Member.

Absent: Richard D'Innocenzo, Clerk.

Staff Present: Michael Boynton, Town Administrator; Allison Potter, Assistant Town Administrator; Carol Pratt, Finance Director; Tom Holder, Director, Department of Public Services; Susy Affleck-Childs, Planning and Economic Development Coordinator; Stephanie Mercandetti, Director, Community and Economic Development.

At 7:00 PM Chairman Foresto called the meeting to order and led the Pledge of Allegiance.

At 7:01 PM Selectman Trindade moved that the Board enter Executive Session under Exemption 6 to consider the purchase, exchange, lease or value of real property if the chair declares that an open meeting may have a detrimental effect on the negotiating position of the public body [4-5 Kingson Ln]; Selectman White seconded. The chair did so declare. No discussion. Roll call vote: 4-0-0 (Crowley, aye; Foresto, aye; Trindade, aye; White, aye).

At 7:16 Chairman Foresto reconvened Public Session.

Public Comments:

There were none.

Vote – Assignment of Right to Purchase 4-5 Kingson Ln to Affordable Housing Trust

The Board reviewed a document entitled CHAPA notification RE: Town's 30-day right of first refusal.

Chairman Foresto reported that the Board discussed this in executive session this evening, briefly reviewing the highlights of the discussion. He noted further that the Board voted as follows:

To assign the right of first refusal to purchase contained in the rider of the deed for Unit 4-5 Kingson Lane, Medway, recorded with the Norfolk District Registry of Deeds in Book 16760, Page 205, to the Trustees of the Town of Medway's Affordable Housing Trust. Motion passed unanimously.

Authorization of Chairman to Execute Contract with Beals and Thomas for Trail Design and Engineering Services - \$35,000

The Board reviewed a Proposed Contract.

Present: Susy Affleck-Childs, Planning and Economic Development Coordinator; Jim Wieler; Michael Murphy (neighborhood representative).

1
2 Ms. Affleck-Childs stated that this group will do a segment of the trail. While the Town did not have to
3 go through a formal bid process, it solicited bids from six different firms. This particular one has a great
4 reputation for trail design. She is asking for authorization to execute the contract at this time.

5
6 Mr. Michael Murphy stated he is serving as the Iarussi Way neighborhood representative on the task
7 force, noting his ten years of history with this area. He stated that he hopes the engineering study can
8 provide information on the best placement, most usability, least cost, etc.

9
10 Selectman Crowley asked about the other abutters and their representation. Mr. Wieler responded that there
11 are not any other abutters, adding that the intent of the task force is to assure that the work associated with
12 the contract stays on track.

13
14 Selectman Crowley asked if the dollar amount includes costs for plans, design, maps, etc. Ms. Affleck-
15 Childs responded that it does, noting that a lot of the physical work will be done by volunteers while
16 some of the more specialized work will be contracted out. A bid process for some work will be used. Mr.
17 Wieler added that they are also applying for some grant funds.

18
19 Brief discussion followed on whether bikes will be using this trail and it was clarified that it will be a
20 natural trail and stone dust will not be part of the design. Selectman Crowley asked about public
21 meetings. Ms. Affleck-Childs responded that none are specifically listed in this contract, but they do
22 public outreach at the appropriate times. She stated that they felt the best use of this individual's time was
23 in the design work. Mr. Wieler noted that Item 6 in the contract references an outreach to Iarussi Way
24 residents. Discussion followed.

25
26 **Selectman Trindade moved that the Board authorize the Chairman to execute a contract in the**
27 **amount of \$30,500 with Beals and Thomas as presented; Selectman White seconded. No discussion.**
28 **VOTE: 4-0-0.**

29
30 **Change of Manager Request – Medway Veterans Building Assoc., Inc. d/b/a Medway Post 1526 VFW:**
31 *The Board reviewed application materials required by the Alcohol Beverages Control Commission (ABCC).*

32
33 Present: John Larney, proposed Manager.

34
35 It was noted that any change in Manager for an establishment with a liquor license must be approved by
36 the Board of Selectmen.

37
38 **Selectman Trindade moved that the Board approve a change of manager for Medway Post #1526**
39 **VFW from Lawrence Landry to John Larney, as proposed; Selectman White seconded. No**
40 **discussion. VOTE: 4-0-0.**

41
42 **Approval – Purchase of 54R Adams St:**

43 *The Board reviewed the following information: (1) Amended P&S signed by the Lees; and (2) Quitclaim*
44 *deed signed by the Lees.*

45
46 Mr. Boynton stated the documents had to be amended due to an incorrect parcel reference, emphasizing
47 that the actual property is the same. Discussion followed on whether a hazardous waste survey needed to
48 be done. A site walk revealed that there was little concern for hazardous waste as the land is mostly
49 “wet”, though there is an abandoned vehicle. It was a fairly inactive farming site.

1 **Selectman Trindade moved that the Board of Selectmen vote to ratify and execute the Amended**
2 **Purchase and Sale Agreement between the Town of Medway and George F. Lee and Charlotte A.**
3 **Lee dated October 15, 2015, and amended December 7, 2015, to purchase the land situated at 54R**
4 **Adams Street, Medway, Norfolk County, MA and to accept the deed from George F. Lee and**
5 **Charlotte A. Lee to the Town as authorized by, and pursuant to, the terms of the vote under Article**
6 **7 of the November 16, 2015 Fall Town Meeting; Selectman White seconded. No discussion. VOTE:**
7 **3-0-1 (Crowley abstain).**

8
9 **Authorization to Expend Grant Funds – Sustainable Materials Recovery Program Grant - \$16,000:**
10 *The Board reviewed the following information: (1) Grant correspondence; and (2) Grant expenditure*
11 *authorization form.*

12
13 Present: Tom Holder, Director, Department of Public Works.

14
15 Mr. Holder reported that the grant funds are from Mass DEP and are intended to augment costs incurred
16 in recycling mattresses. They will be collected in a separate container. Responding to a question from
17 the Board, Mr. Holder clarified that the department receives the funds only as a reimbursement after
18 invoices are submitted and processed.

19
20 **Selectman Trindade moved that the Board accept and authorize grant funds from Mass DEP as a**
21 **Sustainable Materials Recovery Program Grant in the amount of \$16,000; Selectman White seconded.**
22 **It was clarified that this was only for mattresses. No further discussion. VOTE: 4-0-0.**

23
24 Mr. Holder announced that Medway was being recognized by Mass Recycling for its recycling efforts.

25
26 **Authorization of Chairman to Execute Contract for Urban Renewal Plan – BSC Group - \$78,400:**
27 *The Board reviewed the following information: (1) Proposed contract; (2) RFP – electronic packet only;*
28 *and (3) BCS's proposal – electronic packet only.*

29
30 Present: Stephanie Mercandetti, Director, Community and Economic Development.

31
32 Ms. Mercandetti stated a Request for Proposals was issued with lots of interest but only three bids.
33 Interviews were held, and Redevelopment Authority voted to recommend award of the contract to BSC
34 Group.

35
36 **Selectman Trindade moved that the Board authorize the Chairman to execute a contract in the**
37 **amount of \$78,400 with BSC Group for the development of an urban renewal plan following the**
38 **approval as to form by Town Counsel and the acknowledgement of sufficient funds by the Town**
39 **Accountant; Selectman White seconded. Brief discussion followed on the progress of determining**
40 **property owners in the Oak Grove area and why the Redevelopment Authority was created.**
41 **VOTE: 4-0-0.**

42
43 **Change of Sunday Hours Request – Medway Beverages d/b/a Keystone Liquors:**
44 *The Board reviewed the following information: (1) ABCC Retail Alcoholic Beverages License*
45 *Application; and (2) Corporate Vote.*

46
47 **Selectman Trindade moved that the Board approve a change of Sunday opening time for Keystone**
48 **Liquors to 10 AM; Selectman White seconded. No discussion. VOTE: 4-0-0.**

49
50 **Presentation – Five Year Budget Forecast:**
51 *The Board reviewed Five-Year Budget information.*

1
2 Present: Carol Pratt, Finance Director.

3
4 Mr. Boynton reported that, while this forecast does not include any revenues from Exelon, the budget
5 situation looks pretty good. What is needed this evening is the go-ahead to distribute the policy to
6 department heads. Brief discussion followed.

7
8 **Discussion/Vote – Board of Selectmen Fiscal Year 2017 Budget Policy**

9 *The Board reviewed a document entitled “Board of Selectmen Fiscal Year 2017 Budget Policy” (draft).*

10
11 **Selectman Trindade moved that the Board approve the Fiscal Year 2017 budget policy as**
12 **presented; Selectman White seconded. Chairman Foresto noted that this is a fresh way of**
13 **presenting this information. Briefly discussion followed. VOTE: 4-0-0.**

14
15 Mr. Boynton stated the target for the first draft of the budget is mid-January. The Governor’s budget
16 comes out about the third week of January.

17
18 **Approval – Continuation of Membership in MetroWest Veterans’ District and Appointment**
19 **Of Representative to Board of Directors:**

20 *The Board reviewed the following information: (1) Proposed statement by Medway re: its continuation*
21 *with the MetroWest Veterans’ District; and (2) FY2014 Amendment to District Agreement, including*
22 *amendments, 2011 District Agreement.*

23
24 **Selectman Trindade moved that Medway continue its membership in the MetroWest Veterans’**
25 **Services District and that Michael Boynton remain the Board of Selectmen’s representative on the**
26 **District’s Board of Directors; Selectman White seconded. No discussion. VOTE: 4-0-0.**

27
28 **Approval – One-Day Liquor License Applications:**

29 *The Board received the following applications for One-Day Liquor Licenses to be exercised at the Thayer*
30 *Homestead on the following dates: (1) James Centola, 12/19/15; (2) Teresa Rice, 12/27/15; (3) Allison*
31 *Warren and Katherine Fleck, 1/3/16; and (4) Jeanne O’Byrne, 1/9/16. The Police Chief submitted*
32 *recommendations for all applications.*

33
34 **Selectman Trindade moved that the Board approve one-day liquor licenses for James Centola,**
35 **Teresa Rise, Allison Warren and Katherine Fleck, and Jeanne O’Byrne for their events at the**
36 **Thayer Homestead on the dates requested subject to fulfillment of the Police Chief’s**
37 **recommendations and evidence of appropriate insurance coverage; Selectman White seconded. No**
38 **discussion. VOTE: 4-0-0.**

39
40 **Annual License Renewals [see list following agenda items]**

41 *The Board reviewed a list of Annual License Renewals with approval status as presented on the agenda.*
42 *The list will be attached to these minutes.*

43
44 **Selectman Trindade moved that the Board approve license renewals for the establishments**
45 **provided in the Board’s packet conditioned upon the receipt of all necessary documentation,**
46 **departmental approvals and payments to renew the specific licenses; Selectman White seconded.**
47 **No discussion. VOTE: 4-0-0.**

48
49 **Action Items from Previous Meeting:**

50 *The Board reviewed the Action Items List.*

1
2 Mr. Boynton announced that the new communications director would start next week. Additionally, he
3 anticipates an update from the DPS Facility Building Project in a few weeks. Brief discussion followed
4 on other items.

5
6 **Approval of Warrants:**

7 *The Board reviewed Warrants 16-24 and 16-24S.*

8
9 Substituting for the Clerk, Selectman Trindade read aloud Warrants 16-24 and 16-24S presented for
10 approval:

11			
12	16-24	Town Expenses	\$307,297.62
13	16-24S	School Expenses	<u>\$215,639.93</u>
14		TOTAL:	\$522,937.55

15
16 **Selectman Trindade moved that the Board approve the Warrant as read; Selectman White**
17 **seconded. No discussion. VOTE: 4-0-0.**

18
19 **Town Administrator's Report:**

20 Mr. Boynton began his report by updating the Board on the Exelon hearings with the Siting Board, noting
21 that there is a proposed amendment to the Host Community Agreement which provides additional details
22 and information to Section 12, part d, and Medway-V-3 as follows:

23
24 *Generally, it is expected that natural screenings, plantings, berms, and fencing as required/desired*
25 *will be utilized. Final screening plans should be included in the Planning Board site plan process.*

26
27 **Selectman Trindade moved that the Board approve the additional language for Section 12, part d**
28 **and Medway –V-3 of the Host Community Agreement between Exelon and the Town of Medway,**
29 **as proposed; Selectman White seconded. No further discussion. VOTE: 4-0-0.**

30
31 Mr. Boynton updated the Board on the Middle School Renovation Project. At this time, he requested
32 authorization to approve change orders that may come up instead of coming to the Board each time.
33 Brief discussion followed.

34
35 **Selectman Trindade moved that the Board authorize the Town Administrator to approve any**
36 **change order for the Middle School project in amounts up to \$10,000 with reporting provided to**
37 **the Board at the end of the project; Selectman White seconded. No discussion. VOTE: 4-0-0. It**
38 **was clarified that the result of a change order cannot exceed the amount appropriated at Town**
39 **Meeting.**

40
41 Mr. Boynton announced that Health Agent, Stephanie Bacon, will be leaving Medway for a position with
42 the Town of Northborough. The job will be much closer to home for Ms. Bacon.

43
44 Next, he updated the Board on a number of appointments:

- 45 New Communications Director, Mary Becotte, starts on Monday, December 14;
- 46 Joanne Russo will be promoted to Treasurer/Collector;
- 47 Michael Fasolino will be coming in as Assistant Fire Chief on January 4;
- 48 New Police Officers William Freitas and Anthony Nigro start on January 1.

49
50 At this time, Mr. Boynton stated that he has worked with different boards for over 20 years. He expressed
51 appreciation for the Board members, noting that they all bring something unique to the Board. "Respect

1 is something that is waning these days. This Board has gone above and beyond to do what's right for
2 Medway, and it's clear how much you all love this community.”
3

4 **Selectmen's Reports:**

5 Selectman Crowley asked if CIPC could come in to present an overview in order to provide some
6 direction for the Board. After brief discussion, it was agreed to see if they can come on Dec. 21.
7

8 Chairman Foresto reported that he attended a Leadership meeting with the school superintendent. He
9 stated that the number of students opting for vocational school has increased with over 50 students
10 participating. The Town pays an amount per student to the vocational school.
11

12 Chairman Foresto expressed appreciation for the excellent Christmas Parade. He announced that it was
13 put together without the benefit of Town funds. The entire parade was done with donations.
14

15
16 **At 8:20 PM Selectman Trindade moved to adjourn; Selectman White seconded. No discussion.**
17 **VOTE: 4-0-0.**
18

19
20 Respectfully submitted,
21 Jeanette Galliard
22 Night Board Secretary

AGENDA

ITEM #15

Town Administrator's Report

AGENDA

ITEM #16

Selectmen's Reports