

1 **LONDONDERRY, NH PLANNING BOARD**
2 **MINUTES OF THE MEETING OF AUGUST 28, 2013 AT THE MOOSE HILL**
3 **COUNCIL CHAMBERS**
4

5 Members Present: Art Rugg; Mary Soares; Lynn Wiles; Laura El-Azem; Tom
6 Freda, Ex-Officio; Rick Brideau, CNHA, Ex-Officio; John Laferriere, Ex-Officio;
7 Leitha Reilly, alternate member; Maria Newman, alternate member; Al Sypek,
8 alternate member
9

10
11 Also Present: Cynthia May, ASLA, Town Planner and Planning and Economic
12 Development Department Manager and Jaye Trottier, Associate Planner
13

14 A. Rugg called the meeting to order at 7:04 PM. He appointed A. Sypek to vote
15 for Chris Davies and M. Newman to vote for Scott Benson.
16

17 **Administrative Board Work**
18

19 A. Discussions with Town Staff
20

21 Staff had no issues to present to the Board.
22

23 **Continued Plans**
24

25 A. Pillsbury Realty Development, LLC, Map 10, Lots 15, 23, 29C-2A, 29C-2B, 41,
26 41-1, 41-2, 42, 45, 46, 47, 48, 50, 52, 54-1, 57, 58, 59, and 62 – Public
27 hearing for formal review of the Woodmont Commons Planned Unit
28 Development (PUD) Master Plan ***[Continued on August 14, 2013 to August***
29 ***28, 2013].***
30

31 Ari Pollack of Gallagher, Callahan & Gartrell, attorney for Pillsbury Realty
32 Development and the PUD applicants, explained that the Woodmont Commons
33 Development Team (the "Team") was prepared to continue the activity of the
34 August 14 public hearing, i.e. to take input and questions from the Board,
35 Staff, and Public regarding the August, 2013 PUD Master Plan.
36

37 Because the most recent extension of the 65-day approval period per RSA
38 676:4 expires on August 28, 2013, A. Pollack stated that a written request for
39 the Board to extend the timeframe to September 11, 2013 has been submitted
40 to Staff. He also asked that following this presentation, the Board consider a
41 request to continue the public hearing to September 11, 2013.
42

43 **WOODMONT COMMONS PLANNED UNIT DEVELOPMENT MASTER PLAN:**
44

45 A. Rugg stated that in addition to the continued review of the Master Plan, he
46 would also entertain comments and questions afterwards pertaining to the
47 Development Agreement. He explained that he would entertain additional
48 comments and questions from Staff and the Town's third party review
49 consultant, Howard/Stein-Hudson (HSH) as well as the Board, and finally from

1 the public. He added that input from the Board and the public can be
2 submitted to C. May up through September 3, who will then coordinate the
3 information on behalf of the Board. A. Pollack replied that based on this
4 hearing and that of August 14, the Team will attempt to provide a revised
5 Master Plan by September 6 so it may be discussed on September 11.

6
7 A. Rugg asked for questions and comments from the Board. Questions and
8 comments were as follows:

9
10 **1. L. Wiles asked what would be done with input from the Board,**
11 **both from this meeting as well as the August 14 hearing.** T. Goodwin
12 said the Master Plan would be revised to reflect the changes agreed to by
13 the developer. In cases where there is disagreement, the Team will
14 explain the developer's rationale to the Board.

15
16 **2. M. Soares asked if the developer was considering the Board's**
17 **request made on August 14 that preservation of three rows of Apple**
18 **Trees as agreed to on Gilcreast Road would include areas of Hovey**
19 **Road and Pillsbury Road as well.** If three rows are not offered for the
20 two latter roads, she asked what, if any, amount might be offered. A. Rugg
21 echoed the importance of the preservation of a significant hallmark of
22 Londonderry.

23
24 **3. M. Soares reiterated a request made on August 14 that that for**
25 **the aforementioned apple tree buffer, the width equivalent to three**
26 **rows of existing trees be delineated for future knowledge,**
27 particularly since the existing width probably tends to vary.

28 **3a. In light of this question, T. Freda asked for clarification of the**
29 **50-foot Perimeter Buffer** referred to on p. 48 under "Section 3.2 –
30 Performance Standards." A. Rugg explained that an area of 50-feet
31 along the perimeter of any PUD is a requirement of the Town's PUD
32 ordinance. C. May clarified that the trees to be preserved along
33 Gilcreast Road would be a separate area of specific protection within
34 that 50-foot buffer. If three rows of apple trees in one area equated
35 to more than a 50-foot width, the width of those three rows would
36 not be reduced. M. Soares noted that since the width of three rows
37 could conceivably exceed 50 feet and therefore would be an
38 exception to the rule, it is important to include that defined
39 delineation in the Master Plan.

40 **3b. L. Wiles asked how the 50-foot buffer is measured in**
41 **relation to the road.** C. May replied it is measured from the edge
42 of the right of way for that particular road, which extends beyond the
43 edge of pavement.

44
45 **4. M. Soares asked that a maintenance plan be developed for the**
46 **rows of apple trees to be preserved to ensure their aesthetic**
47 **qualities are maintained in perpetuity,** including some form of
48 protection such as a deed restriction when those rows become part of
49 individual house lots.

50 **4a. A. Sypek suggested considering a plan for harvesting of**

1 **any apples produced by those trees**, e.g. to lease the land to
2 orchardists who can harvest and perform necessary maintenance.

3
4 **5. M. Soares asked for a clear portrayal in the Master Plan that the**
5 **“like for like” description of the perimeter buffer meant specifically**
6 **that, e.g. a one-acre lot with a residence would be directly across**
7 **from a one-acre lot with a similar residence.**

8
9 **6. A. Rugg confirmed that the maximum size of the proposed pond**
10 **(See Attachment #1 p. 26) is 39 acres and that it could be less,**
11 **depending on any approvals from the NH Department of**
12 **Environmental Sciences (DES).** E. Innes replied that as reflected on
13 p. 66, the conserved green space in subarea WC-3 is a minimum of 39
14 acres, which includes the proposed pond.

15
16 **7. M. Soares confirmed that the trail associated with the pond**
17 **(p. 27) would be configured in such a way with the owners of the**
18 **homes located do not view the trail as an imposition on their**
19 **privacy.** This request was previously made at the August 14 hearing based
20 on M. Soares explanation that homeowners in existing Londonderry
21 subdivisions have objected to walking trails approved for their neighborhood
22 because they were ultimately deemed undesirable, creating the need to
23 subsequently amend the approved plans. T. Goodwin replied that the trail
24 would be identified as either being a sidewalk connected with the street in
25 WC-4 or a trail adjacent to that street but within the open space of WC-3.

26
27 **8. J. Laferriere asked for the rationale behind the requested waiver**
28 **to the Town’s phasing ordinance** (Section 1.3, p. 46). T. Goodwin
29 explained that because Woodmont Commons will be building their
30 infrastructure within the development, capacity will exist prior to any
31 construction, therefore phasing would not be necessary. E. Innes further
32 explained that the number of vehicle trips generated will inherently limit the
33 amount of development occurring at any one time, as will the agreement by
34 the developer to demonstrate annually that the project remains fiscally
35 positive. Additionally, limiting the number of residential units to be built in
36 a mixed use development restricts the developer’s ability to make the
37 commercial aspect economically viable.

38 **8a. J. Laferriere asked if the developer would be willing**
39 **instead to request a waiver of the phasing requirement, when**
40 **justified by that economic viability, at the individual site or**
41 **subdivision plan level, rather than to ask for an overall waiver**
42 **from the regulations.** A. Rugg suggested that would be a
43 **more appropriate approach for the Board to better gauge the**
44 **impacts of distinct proposals to both Woodmont Commons and**
45 **the town in general, particularly if multiple projects are being**
46 **considered at the same time.** T. Goodwin said the idea could be
47 discussed.

48 **8b. L. El-Azem asked how many subareas include mixed use**
49 **development.** E. Innes answered that WC-1, WC-1-GL, WC-10,

1 WC-12 all have mixed use components, as to WC-2 and WC-8 to a
 2 lesser degree.

3 **8c. T. Freda asked if the infrastructure could possible lag**
 4 **behind construction of the properties made exempt from**
 5 **phasing.** E. Innes replied that for each separate development, the
 6 infrastructure would be built prior to construction of any associated
 7 buildings and would have to be approved by the Planning Board as
 8 part of the subdivision plan.
 9

10 **9. J. Laferriere asked for an explanation of the waiver request from**
 11 **Section 3.09(E) of the Londonderry Subdivision Regulation; Streets/**
 12 **Classification** (p. 52). A. Pollack stated that the waiver is from requiring
 13 that streets within Woodmont Commons be built to applicable Town
 14 standards which would allow roads included within the street pallet to be
 15 constructed. The language also acknowledges that the developer does not
 16 have any expectation that the Town will have to accept and/or maintain
 17 such roads, that such roads will be the responsibility of the developer.
 18

19 **10. A. Sypek asked if the reference to a minimum street width**
 20 **within the Master Plan of 12 feet can be restricted to a one-way**
 21 **street or alley.** E. Innes said the request would be considered. C. May
 22 added that it could also refer to the width of a single lane in a divided two
 23 way street, i.e. a boulevard with a median where there is no on-street
 24 parking and the lanes function like one-way streets.
 25

26 **11. A. Rugg requested clarification on of the requested waiver from**
 27 **zoning ordinance Section 3.14.1.2/3.14.1.3 – Fences** (p. 49), i.e. that
 28 "Private fences may be erected within rights-of-way, provided the easement
 29 is of sufficient width to allow safe placement of the fence. Similarly, sight
 30 distance considerations relating to intersection safety shall not prohibit the
 31 location of reasonable fencing in the limited context of signalized
 32 intersections." T. Goodwin stated that as discussed on August 14, the
 33 Team had agreed to review the language to clarify the intent. A. Pollack
 34 reiterated the objective, explaining that private fences within a larger than
 35 average right-of-way or easement could be allowed, provided they do not
 36 pose an obstruction the travel way, including sight distance. The waiver
 37 request is not intended to allow fences in the travel way. A. Pollack gave an
 38 example of a fence erected within a more extensive right-of-way along a
 39 straight section of road or at a safe distance from a signalized intersection.
 40 He added that a visual example could be provided along with the revised
 41 language to illustrate that intent.
 42

43 **11a. L. Reilly confirmed that the waiver request was only from**
 44 **the location of private fences,** not from the aesthetics outlined in
 45 the zoning ordinance.
 46

47 **12. A. Rugg and L. Wiles confirmed that the statement regarding**
 48 **home occupations within Woodmont Commons** (p. 65) **explains that**
 49 **Zoning Board approval of a special exception request to operate a**
 50 **home occupation is not required and that the authority rests with**
the Planning Board to ensure those occupations comply with the

1 **criteria listed in the zoning ordinance.** E. Innes answered that a special
2 exception for a home occupation would not be required for those living
3 within Woodmont Commons, however compliance with performance
4 standards of the zoning ordinance must still be demonstrated. Validation of
5 compliance would take place at the individual site or subdivision plan level
6 and therefore be the jurisdiction of the Planning Board, not the Zoning
7 Board.

8
9 **13. L. Wiles stated that the project does not provide adequate open**
10 **space, both in its entirety and particular west of I-93.** A. Rugg
11 reminded him that the stated acreage in the document for open space is a
12 minimum requirement. During the individual site/subdivision plan process,
13 the Board will have an additional opportunity to assess the amount and
14 request more if they see fit to. L. Wiles replied that such a request should
15 occur at the Master Plan level and provide direction for subsequent site and
16 subdivision plans. Preserving open space, he said, would preserve one of
17 the most aesthetic features of Londonderry. A. Rugg acknowledged that
18 the Board has requested higher minimums for open space, although
19 defining that for the Master Plan is a difficult task. He also described open
20 space to the Team as a "hallmark" of Londonderry and therefore an
21 important aspect to residents. E. Innes added that L. Wiles' comments
22 would be added to those made on August 14 about increasing the amount
23 of open space.

24
25 **14. T. Freda asked for clarification of the standard on p.69 that**
26 **allows for minimum lot sizes to be waived for Cottage Court**
27 **developments.** T. Goodwin explained that as smaller units designed for
28 affordable housing that are typically clustered and front on common space
29 as opposed to a roadway, the lot size standard can be reduced to maximize
30 their affordability. There are limits, however, on the number of cottage
31 court units that can be included in various subareas. The maxima assigned
32 to Cottage Courts cannot be exceeded; the statement only refers to the
33 placement of units on a lot. When asked how small a cottage court lot can
34 be, T. Goodwin relayed his experience that some are as narrow as 24 feet
35 and 90-100 feet deep, depending on the length of the dwelling. A. Pollack
36 said it would be made clear that the total number of units cannot be
37 exceeded.

38
39 **15. M. Newman reiterated her request from the August 14 meeting**
40 **to require a minima for playgrounds in active open space areas** (see
41 p. 69). A. Rugg noted the importance of flexibility in the PUD, and M.
42 Soares added that in her experience, the Planning Board typically deals with
43 such an issue during the site/subdivision plan level with the number of
44 residential units and amount of open space is more readily apparent. A.
45 Rugg recalled that during the 1980's, the Town had actually required
46 developers to designate 10% of their development specifically for
47 recreation, something which was later determined by the NH Supreme
48 Court to be the equivalent of a land taking and therefore illegal. Consensus
49 amongst Board members was that a requirement for playgrounds in active
50 open space areas was not necessary.

1
2 **16. J. Laferriere asked for clarification about the number of hotels**
3 **and boutique hotels allowed within the PUD and the maximum**
4 **number of rooms allowed in each**, noting that the footnote on p. 66
5 could be interpreted that numerous boutique hotels of 100 rooms each
6 would be allowed, along with three standard hotels of 550 rooms each. E.
7 Innes referred the Board to p. 53 where the definition of a boutique hotel
8 states that such a hotel can only include a limit of 10 guest rooms. While
9 there could be multiple boutique hotels, their aggregate number of rooms
10 cannot exceed 100 within the PUD. Similarly, standard hotels are limited to
11 three total structures within the PUD, whose combined number of rooms
12 cannot exceed 550.

13 **16a. L. Wiles asked if the category of boutique hotels would**
14 **include Bed and Breakfast establishments.** E. Innes replied that Bed
15 and Breakfast Homestays would be considered separate structures in the
16 PUD that would be required to follow the zoning ordinance. T. Goodwin
17 added that Bed and Breakfast Homestays must be owner occupied.

18
19 **17. M. Soares noted that under "Measurement and calculation of**
20 **area" of subdivision signage (p. 163), the language indicates that**
21 **the exception of the 10-foot height maximum is "below," however**
22 **the exception is not discussed for several pages afterward.** E. Innes
23 said the reference will be clarified to indicate the page number where it is
24 located.

25
26 **18. M. Soares asked where a "Gateway Sign" (p. 14) would be**
27 **allowed in proximity to the actual entrances of the PUD since those**
28 **would be allowed a greater height than most other signs.** E. Innes
29 answered that such signs are only permitted at commercial entrances to the
30 PUD and cannot be adjacent to any abutting AR-I zones.

31 **18a. M. Soares asked what the maximum height allowed would**
32 **be for a gateway sign.** C. May noted that the maximum height of
33 freestanding signs (8 feet) can be found on the site plan regulations and
34 standards on p. 208.

35 **18b. M. Soares asked how the square footage of the gateway**
36 **sign would be calculated as the 16 square feet stated is clear when**
37 **looking at the example given.** E. Innes said a more accurate example
38 that reflects the standard gateway sign could be substituted there.

39
40 **19. A. Rugg pointed out that the Town encourages the use of**
41 **external illumination of signs using downcast lights** as opposed to the
42 description on p. 164 that gateway signs "may be illuminated using ground-
43 mounted lights." E. Inness replied that the Team would take note of that.
44 A. Rugg added that internal illumination is permitted by the Town's sign
45 ordinance.

46 **19a. M. Soares asked that staff ensure internally lit signs**
47 **deemed acceptable under the requirements of the PUD Master**
48 **Plan do not included moveable type.** C. May clarified that the
49 intent is simply to allow signs with an internal light source. E. Innes

1 added that the General Sign Guidelines on p. 204 include the
2 prohibition of "animated, moving, flashing, and noise making signs."
3

4 **20. M. Soares asked for the specific definition of "temporary" in the**
5 **instance of temporary signs allowed under the PUD Master Plan.** E.
6 Innes directed the Board to p. 210 where "Temporary Promotional Signs"
7 are defined, but added that the Team could address all temporary signs as
8 well. **A. Rugg suggested the Team review the Town's current sign**
9 **ordinance.**

10
11 **21. M. Soares questioned the description of total sign area for**
12 **Highway Corridor Signage (p. 211) as it states that the maximum**
13 **size could be "350 total square feet per building."** E. Innes
14 responded that the Team took note of the concern at the August 14
15 meeting, adding that signage in that case is restricted to the building itself
16 and does not refer to monument signage. A. Pollack stated that after the
17 August 14 meeting, the Team had intended to add language to the highway
18 corridor signage description that the building signage could not exceed the
19 height of the building to make the intent more apparent.
20

21 **22. L. Wiles asked that a statement be included in the storm water**
22 **management section on p. 215 that there will be no increase in the**
23 **amount of storm water runoff as a result of the PUD.** C. May
24 explained that the requirement is governed by State law and would
25 therefore be a requirement of the development (see also item number 5
26 under public comment). A. Pollack said the Team had already planned on
27 making that statement in the storm water section of the document.
28

29 **23. M. Newman asked for verification under "PUD Landscape" (p.**
30 **168) as to what entity would be responsible for landscaping within**
31 **PUD right-of-ways.** E. Innes replied that the team had taken note of that
32 issue on August 14 and intended to clarify that language. A. Pollack
33 reiterated that unless a street is publicly accepted by the Town, landscaping
34 maintenance would be the responsibility of the developer and any
35 subsequent owner of the land in question.

36 **23a. L. Reilly asked whether the Town could exclude roadway**
37 **landscaping if and when an existing roadway was publicly**
38 **accepted.** C. May offered that an easement could be created
39 whereby the responsibility for that landscaping could be that of the
40 developer or another private landowner. Jimmy D'Angelo of TEC said
41 that such a maintenance agreement could be made a condition of
42 acceptance of the roadway by the Town. A. Pollack further clarified
43 that the issue could be addressed at the individual site/subdivision
44 plan level, not necessarily in terms of the PUD. M. Soares added that
45 a request for the Town to publicly accept a roadway could realistically
46 take place well after approval of an individual plan.
47

48 **24. L. Wiles questioned the appropriateness of the inclusion of the**
49 **Town's winter maintenance requirements (p. 224) when**
50 **considering the Master Plan speaks as a whole to the developer's**

1 **restrictions, requirements and abilities.** A. Pollack explained that the
 2 language was added in conjunction with Town staff in response to their
 3 request to clarify the Town’s existing agreement with the State Department
 4 of Environmental Service (DES) regarding chloride management so as to
 5 better reflect the developer’s responsibilities in relation to that agreement
 6 with the State. He said that Staff had asked that the PUD Master Plan in no
 7 way impede the Town’s contract with the State, therefore the intent was to
 8 first clarify the Town’s obligations to the State and subsequently
 9 demonstrate that the developer would be “part of the solution” and not
 10 frustrate the Town’s efforts to fulfill their DES agreement.

11 **24a. J. Laferriere asked for clarifying language in the**
 12 **introduction of that section about the agreement between the**
 13 **Town and DES.** A. Pollack suggested stating ‘The agreement that
 14 the town has entered into with DES specifies that...’ That was found
 15 an acceptable means of clarification. A. Pollack stated the language
 16 would be revised and reiterated that the intent was not to suggest
 17 any additional obligations on the part of the Town regarding
 18 Woodmont Commons and chloride management.
 19

20 **25. A. Sypek inquired as to the mechanics of “Improved Weather**
 21 **System Monitoring”** on p. 224. J. D’Angelo explained that because the
 22 watershed within the area of Woodmont Commons will be impacted by
 23 winter maintenance of private roads and parking lots within the PUD (i.e.
 24 the use of chloride) yet no specific restrictions can be imposed on private
 25 landowners to prevent excessive chloride use, the items listed in the second
 26 column on p. 224 are designed to educate, train, and State certify
 27 individuals and to employ Best Management Practices, improved technology
 28 and up-to-date information to make the most informed decisions about the
 29 use of road salt.
 30

31 **26. L. Wiles asked if an overall snow removal policy should be**
 32 **included in the PUD Master Plan.** A. Pollack replied that the issue would
 33 be more appropriate at the site/subdivision plan level. C. May added that
 34 at that time, the Board could require that the Master Developer provide a
 35 cumulative review of snow removal up to that point for the entire PUD.
 36

37 A. Rugg asked for input from Staff and HSH. C. May made no additional
 38 comments. **T. Brovitz’s comments were as follows:**
 39

40 **1. The expectation created in the conceptual portion of the Master**
 41 **Plan, i.e. the Information Plan (Section 1.2.2) of a traditional**
 42 **village center with neighborhoods, mixed uses, and strong**
 43 **relationships between open space, buildings, and streets is not**
 44 **readily supported by the Land Use Plan.** This is due in part to the lack
 45 of design specificity and the use of development patters within the Land Use
 46 Plan which in itself is a result of the fact that the Land Use Plan must be
 47 broad to accommodate the many specific outcomes that are possible within
 48 the PUD. **Additional provisions should be established to strengthen**
 49 **the relationship of open space and development as well as the**
 50 **balance of uses within the subareas,** such as additional and more

1 specific composite criteria that would be addressed at the site/subdivision
2 plan level. Another example would be the requirement of ratios in the
3 Density Table to better balance residential uses with those that are
4 commercial/institutional.

5 1a. **M. Soares asked if the flexibility factors in the density table**
6 **would not accomplish that balance.** T. Brovitz answered that the
7 flexibility factors only allow for an increase above and beyond the maximum
8 densities permitted (with a corresponding decrease of the use in another
9 subarea). Establishing ratios, he explained, would ensure the mixed use
10 component which has been portrayed by the developer and its commercial
11 tax base in subareas that could theoretically be developed solely as
12 residential. A. Pollack countered that such ratios would be
13 counterproductive to the market driven economics of the PUD and
14 ultimately restrict the developer to a degree that would render the
15 development economically unviable. The flexible nature of the mixed use
16 PUD, he continued, allows for the market to drive the project while
17 observing maximum densities and open space minimas. A. Rugg agreed,
18 noting that as economic conditions change, development needs to be able
19 to adapt to new conditions and needs. T. Brovitz clarified that the market
20 could still dictate development, however ratios used in conjunction with
21 flexibility factors could push residential or commercial densities to other
22 subareas areas while achieving a balance of mixed uses within the
23 subareas. A. Rugg replied that the Board could consider the issue, however
24 the Board's preference thus far has been for flexibility that would address
25 market fluctuations over the 20 year life of the project.

26
27 **2. An alternative open space plan should be included for subarea**
28 **WC-3 if the proposed pond is not built.** While the existing wetland
29 could still act as a key element of the drainage system, compensation for
30 the potential loss of a prominent open space feature in another area has not
31 been adequately addressed.

32 2a. **A. Sypek asked the Team if the proposed pond would be**
33 **phased in along with the individual developments.** J. D'Angelo
34 answered that if the pond is indeed built in phases, a significant portion,
35 much like infrastructure for the development, would most likely be created
36 first.

37
38 **3. Given that the proposed realignment of Pillsbury Road was a key**
39 **recommendation in the Traffic Impact Assessment (TIA), it should**
40 **be addressed specifically in the Land Use Plan.** A. Rugg agreed, noting
41 that it should at least be stated that it is likely to occur. T. Goodwin said
42 the realignment, while not shown to the level included in the TIA, is still
43 included in the Land Use Plan. He added that the TIA is part of the Master
44 Plan and that any work done regarding the realignment would be reflected
45 in a revised TIA. **E. Innes stated that the TIA is referenced**
46 **throughout the Master Plan, however A. Rugg asked that it be**
47 **identified earlier in the document** to make the correlation more
48 apparent to the reader. **He also asked the same of the possible**
49 **realignment within the Land Use Plan.**

1 3a. **M. Soares asked for confirmation that the realignment**
2 **would definitely take place.** A. Pollack explained that while the success
3 of that portion of the PUD relies in part on the road realignment, an actual
4 commitment would be premature because of various options amongst land
5 rights that must be addressed first and cannot be until the Master Plan is
6 approved. C. May added that an additional reason not to make that
7 guarantee is that traffic technologies could evolve to a point where a more
8 effective alternative can be provided. A. Rugg agreed that it is more
9 important to keep traffic mitigation options open.

10 3b. **J. Laferriere asked if it will be known at what point the**
11 **realignment will be needed and whether some amount of**
12 **development could precede it.** A. Pollack replied that its need would be
13 made more apparent at the site/subdivision plan level since its approval
14 would depend in part on the successful demonstration of mitigating traffic
15 impacts.

16
17 **4. Some institutional, commercial, and home occupation uses**
18 **allowed in subareas such as WC-8 could conflict with abutting**
19 **residential uses.** It should be made clear that home occupations and Bed
20 and Breakfast Homestays must adhere to standards of the current zoning
21 ordinance. For institutional and commercial uses, performance standards
22 could be put in place to reduce impact to residential uses. E. Innes said the
23 point considered. A. Rugg noted the tradition in Londonderry is one of
24 sensitivity to residential areas and the use buffering to even light
25 commercial uses.

26
27 **5. In addition to the implementation of ratios in the land use**
28 **density table, limits within individual subareas would be better**
29 **based on trip generation than development densities.** The table
30 should therefore identify trip thresholds at which point development would
31 stop which would allow for more improvements to be made to address
32 traffic needs. Planned densities may not reveal the appropriate stopping
33 point for development since the use categories are general, while the
34 specific uses realized can produce different trip amounts. E. Innes stated
35 that in response to similar concerns expressed by HSH, maxima have been
36 added to the development standards table (p. 67) based on whether Exit 4A
37 is built. Trips, however, were not included because it was believed their
38 addition to a table of densities could cause confusion. Kevin Dandrade of
39 TEC offered that the density figures were derived from maximum trip data
40 found in the Traffic Sensitivity Analysis and in that sense are represented in
41 the land use density table. J. D'Angelo added that as a result of discussions
42 with HSH, a higher level of conservatism was built into the density
43 associated with those trips to avoid traffic impacts from impeding
44 development. With each site plan, he explained, it will become apparent
45 how development is relating to trip thresholds.

46
47 **6. Larger block sizes included in the plan, e.g. 600 feet x 600 feet,**
48 **have a tendency to encourage higher vehicle speeds on wider**
49 **streets which runs contrary to the key notion in the PUD of**
50 **walkability.** Instead of simply allowing them, exceptions can be made for

1 larger block sizes where they are deemed appropriate. E. Innes said that
 2 while the Team is confident in the inclusion of a 600 x 600 block type in the
 3 village corridor whereas HSH advocated for a 400 x 400 block (see pp. 112-
 4 113), the Team has addressed pedestrian safety concerns through speed
 5 management and crossing standards. T. Goodwin explained that the need
 6 for a wider range of block sizes is to accommodate the different uses within
 7 the PUD, especially uses whose dimensions have changed since original
 8 New England villages were developed and the use of vehicles and the
 9 associated parking needs have replaced that of the horse and buggy. C.
 10 May clarified that the 600 x 600 block size is a maximum size, not a
 11 standard one. A. Rugg stated that block sizes the Board deems excessive
 12 can be addressed at the site/subdivision plan level.

13
 14 **7. Regarding proposed parking standards, the table of parking**
 15 **spaces required for various uses should be based on current Town**
 16 **standards, but allow for the ability to amend those standards at the**
 17 **site plan level.** Offset factors such as reduction measures, on-street
 18 parking, satellite employee parking, etc. should be employed instead of
 19 having standards that are too permissive.

20
 21 **8. It is unclear what active open space is.** If it is simply shared open
 22 space, that can provide for public access, but there is no assurance of public
 23 amenities that would make the land more useable for recreation.

24
 25 A. Rugg stated that additional comments from HSH, such as T. Brovitz's
 26 stated need to integrate intersection improvements could be forwarded to
 27 C. May. He asked that Staff and the Woodmont Commons Team continue
 28 to work with HSH to address these concerns and suggestions. L. Reilly
 29 expressed her confidence in the validity of T. Brovitz's comments based on
 30 the fact that HSH was selected as the Town's third party Woodmont
 31 Commons consultant in part because of their ability to look at the overview
 32 of a large project and predict possible areas of concerns before discovering
 33 them during the site/subdivision plan process. J. Laferriere agreed, noting
 34 the input given by HSH will be helpful when reviewing individual
 35 development plans. T. Brovitz said his goal was to better refine certain
 36 aspects of the plan.

37
 38 A. Rugg asked for public input. Comments and questions were as follows:

39
 40 1. **Mike Brown, 5 Carousel Court, read a prepared statement to**
 41 **the Board (see Attachment #2). He asked that the Board require the**
 42 **Woodmont Commons development is subject to both the phasing**
 43 **and growth management regulations of the zoning ordinance in**
 44 **order to ensure development does not exceed the Town's ability to**
 45 **provide adequate services to all residents.** Both before and after
 46 Woodmont Commons became a possibility, M. Brown asked Staff if all PUDs
 47 would have to comply with phasing and growth management ordinances
 48 and he was told they would. He also conveyed his concern that the PUD
 49 Master Plan seeks to be exempt from Conditional Use Permits and variances
 50 from the zoning ordinance. A. Rugg replied that the Development

1 Agreement, which is part of the Master Plan, addresses several of the
2 concerns expressed in the letter, particularly growth management since the
3 development must remain tax positive on an annual basis during its
4 complete buildout. This is expected to provide a means of pacing
5 development in the PUD so construction does not exceed infrastructure
6 capacity. M. Brown replied that being tax positive is not the same as
7 actually limiting the number of units that can be built in a given year as
8 required by the Growth Management Ordinance (GMO). A. Rugg said the
9 spirit and intent of the GMO is still achieved because the ability to remain
10 tax positive will require restricting development when it poses the possibility
11 to exceed capacity.

12 1a. M. **Brown asked if the Board had come to a consensus to**
13 **waive the phasing and growth management regulations of the**
14 **zoning ordinance.** T. Freda explained that in terms of the GMO, the
15 Development Agreement states that other than what regulations are stated
16 in the Master Plan, the developer shall become vested as to all other laws,
17 standards, and regulations in place at the time of the approval of the Master
18 Plan, which includes the GMO. Since the GMO has a sunset clause of
19 January 1, 2015, once it expires, it will no longer apply to Woodmont
20 Commons. M. Brown noted that the wording in the zoning ordinance about
21 the GMO expiring includes the provision "until such time as it is reenacted
22 by the Town Council." L. El-Azem asked for clarification as to whether the
23 Board had discussed reenactment of the GMO. A. Rugg answered that the
24 Board will need to confer further with the Town Attorney on the matter. T.
25 Freda offered his recollection that once the GMO sunsets, Woodmont
26 Commons would no longer be held to that ordinance or any reenactment of
27 it by the Town Council. M. Brown asked if Woodmont Commons is exempt
28 from phasing under the Development Agreement. M. Soares said phasing
29 will be addressed by having to keep the development tax positive on an
30 annual basis. M. Brown asked if the specific waiver on p. 46 from limiting
31 the number of units that can be built in a year will be granted by the Board.
32 A. Rugg said the Board is considering that request. M. Brown asked that
33 the Board not grant the wavier.
34

35 2. **Mike Speltz, 18 Sugarplum Drive, suggested that being tax positive**
36 **is not the same as providing sufficient infrastructure.** The
37 development could prove to be tax positive while still creating excessive
38 demands on Town services, something the GMO would prevent by limiting
39 the number of units that can be built. An influx of children caused by the
40 development, for example, can create a demand years before a school can
41 be designed and built. Like M. Brown, he asked that the GMO not be
42 waived. A. Rugg disagreed that remaining tax positive would not produce
43 the same effect as the GMO, adding that the Town's annual budgeting
44 process will serve to keep infrastructure in line with development. M.
45 Soares added that the Planning Board could not approve a plan if they see,
46 under M. Speltz's example, that the possible number of children associated
47 with a proposed development would require an additional school. M. Speltz
48 questioned whether the Planning Board would have the authority in that
49 situation to deny approval of a plan if all regulations applicable to the PUD
50 are met.

1
2 **3. M. Speltz asked if the Planning Board would approve the Master**
3 **Plan and recommend approval of the Development Agreement to**
4 **the Town Council at the same time.** A. Rugg said the decisions could be
5 rendered separately with the contingency that they must both be adopted
6 by the Planning Board and Town Council respectively.

7
8 **4. M. Speltz asked if there was any intent to distribute the current**
9 **submission to the Town's Design Review Committee (DRC) since the**
10 **original Woodmont Commons submission was in 2011.** A. Rugg said
11 he was not aware that the first submission had been circulated to the DRC,
12 adding that the public hearings have provided the opportunity for comments
13 and that the document has been available to the public both in hard copy
14 and on the Town's website. M. Speltz asked that the Planning Board not
15 deviate from the practice of sending submittals through the Design Review
16 process. A. Rugg noted that the Design Review process is standard for
17 reviewing site and subdivision plans specifically.

18
19 **5. M. Speltz referred to C. May's earlier statement that under State**
20 **law, there can be no increase in runoff created by a proposed**
21 **development, but noted that DES only has authority over individual**
22 **site and subdivision plans, not PUDs.** He acknowledged the developer's
23 commitment that there will be no cumulative increase in runoff from the
24 PUD as a whole and described that obligation as significant, however he
25 clarified that there is no State law requiring the developer to do so. A.
26 Rugg noted the offer from the developer is a matter of public record. M.
27 Speltz also explained the impact of the offer because it refers to the
28 collective runoff of the PUD as a whole which can be a better indication of
29 potential flooding impacts to areas downstream from Woodmont Commons,
30 a scope not obvious when reviewing individual development plans. **He**
31 **asked that the developer's offer be made a requirement of the PUD**
32 **Master Plan.**

33
34 **6. M. Speltz expressed his agreement with the comments made**
35 **earlier by T. Brovitz of HSH, particularly that of the addition of**
36 **vehicle trips to the land use table.**

37
38 **7. M. Speltz asked that when a waiver is requested in the Master**
39 **Plan, it be accompanied by the justification for the request and an**
40 **alternate method to offset any resulting impacts.** If, for example, a
41 waiver is requested from the entire Conservation Overlay District ordinance,
42 a rationalization be provided, along with contingency plan to protect
43 wetlands and streams from the negative effects of development.

44
45 **8. M. Speltz read a prepared statement to the Board (see #3). He**
46 **asked that the Board give consideration to density issues and hold**
47 **the Master Plan to all five general considerations and all 15 specific**
48 **objectives in Section 2.8.8 of the Town's PUD ordinance.** He voiced
49 particular concern for the issues of limited proposed agricultural uses,
50 limited availability for outdoor recreation, preservation of agricultural and

1 other natural resources, preservation of open space other than
2 undevelopable areas, and the demonstration of sustainable design that will
3 promote energy conservation. He questioned the ability of the PUD Master
4 Plan to fully provide the fundamental concepts of the rural urban transect
5 and pedestriansheds that are key elements of the New Urban design on
6 which Woodmont Commons is based. He also asked whether the PUD
7 Master Plan can observe the goals of the Town's 2013 Master Plan, which in
8 itself recommends adherence to the 2010/2011 Open Space Plan. He asked
9 that the Board consider balancing the market value of Woodmont Commons
10 with the protection of the Town's natural resources.
11

12 **9. Martin Srugis, 17 Wimbledon Drive, expressed apprehension over**
13 **the effects of Woodmont Commons on traffic, schools, police and**
14 **fire services, etc. in Londonderry, pointing to the negative effects on**
15 **Town caused by the unchecked growth experienced in the 1980's.**
16 **He also communicated concern for the need of existing taxpayers to**
17 **provide services to Woodmont Commons before the development**
18 **accrues the revenue to pay for the necessary improvements.** He
19 asked that the GMO not be waived and that the PUD be integrated into the
20 rest of Londonderry rather than allowing it to exist as a town unto itself. M.
21 Srugis asked that if a period of unsustainable growth was determined and
22 all other developments in town were limited to the number of building
23 permits that could be issued in a year, whether that limitation would apply
24 to building permits associated with Woodmont Commons. A. Rugg replied
25 that Woodmont Commons would be governed by the Development
26 Agreement.

27 **9a. T. Freda asked if the Woodmont Commons Team could**
28 **explain any plans for phasing that may exist at this time.** A. Pollack
29 replied that it would be impractical to describe any specific phasing until
30 specific demands are determined, but added that the developer expects to
31 employ various mitigation strategies to be reviewed at the site/subdivision
32 plan level if and when the potential for development to exceed
33 infrastructure and services becomes a possibility. The developer, he said, is
34 also prepared to review and balance any drain on Town resources by
35 remaining tax positive on an annual bases. **T. Freda asked if the**
36 **developer believes phasing is something that applies to Woodmont**
37 **Commons.** A. Pollack answered that phasing as described in the PUD
38 ordinance applies to this PUD.
39

40 **10. Ann Chiampa, 28 Wedgewood Drive, asked for the definition of a**
41 **"block" and if the 600 x 600 block type discussed earlier would be**
42 **included in both the Village Center and neighborhoods.** E. Innes
43 provided the definition found on p. 53 and T. Brovitz replied that 600 x 600
44 size blocks are intended for the Village Center. A. Chiampa confirmed that
45 residential blocks could be up to 750 feet x 750 feet in size.
46

47 **11. A. Chiampa reiterated her concern stated previously over the**
48 **number of proposed intersections on Hovey Road (i.e. 17).**
49

1 **12. A. Chiampa asked if the square featured in the middle of the**
2 **straight portion of Hovey Road was necessary, considering the**
3 **imposition to existing residents who use the road regularly.** A. Rugg
4 said the goal is to slow traffic in a straight section of roadway. A. Chiampa
5 noted that a square previously shown on Pillsbury Road had been removed
6 from subsequent illustrations, but A. Rugg explained that Pillsbury Road is a
7 major thoroughfare in the plan, unlike Hovey Road. K. Dandrade said that
8 as a result of the analysis of the area with and without Exit 4A, there is not
9 an expectation of significant volumes of traffic on Hovey Road since it is not
10 planned as a direct route to and from Woodmont Commons. The attempt
11 to slow traffic with the square is a means of integrating the road into the
12 walkable aspects of the PUD. As development within Woodmont Commons
13 progresses, he said the Hovey Road area will continue to be examined in
14 order to restrict its potential use as a main road into the PUD. M. Soares
15 noted that the plan is still at a conceptual level. A. Chiampa asked that the
16 particular concept of the square on Hovey Road not be considered for the
17 sake of the residents on Hovey Road.

18
19 **13. A. Chiampa asked if uses can be added to subareas if they**
20 **overflow from abutting subareas as a result of the flexibility factors.**
21 M. Soares said the use has to be allowed in a subarea to accept such a
22 resulting influx. A. Chiampa confirmed that only an additional 15% of
23 commercial and institutional uses allowed in subarea WC-10 could occur
24 because of the flexibility factor.

25
26 **14. A. Chiampa verified that the Board is familiar with the**
27 **appendices included in the Master Plan,** particularly the TIA, the
28 infrastructure memo, the Fiscal Impact Analysis (FIA), and the Development
29 Agreement. She stated that according to a statement on p. 6 of 25 in the
30 TIA regarding Exit 4A that 100% of the traffic currently going from Folsom
31 Road in Derry to I-93 via Londonderry Road would be mitigated and would
32 not go to exit 4 if Exit 4A is built. Unlike exits 4 and 5, however, **WC-12**
33 **does not appear to provide adequate travel amenities such as gas**
34 **stations.** If the 100% figure is accurate, she explained that motorists
35 would have to travel to Crystal Ave or Route 28 in Derry after getting off at
36 Exit 4A to purchase gas. C. May said the TIA was thoroughly evaluated by
37 HSH, including all of the figures provided. T. Brovitz verified that,
38 describing the figures are "highly informed estimates." K. Dandrade stated
39 that information from the engineering firm of CLD Consultants (the firm
40 consulting on Exit 4A for both Derry and Londonderry) was also used and is
41 based on a series of long term assumptions about traffic diversions that will
42 result from Exit 4A.

43
44 **15. A. Chiampa asked that the Board assign specific amounts of**
45 **active open space to more dense residential developments** (e.g.
46 playgrounds) to compensate those residents with smaller lot sizes.

47
48 **16. A. Chiampa asked when the Planning Board might consider the**
49 **final approval of the PUD Master Plan.** A. Rugg said that is unknown,
50 adding that if the Board approves this evening's request by the developer to

1 extend the 65-day review period to September 11, an additional extension
2 beyond that would have to be considered on that date. She confirmed the
3 September 3 deadline for additional comments and questions to be sent to
4 C. May and **asked when the public would be able to review responses**
5 **from the developer.** A. Rugg answered that when the revised PUD Master
6 Plan is delivered to the Board as anticipated on September 6, it would be
7 posted on the Town website that day as well.

8
9 **17. M. Soares read into the record a letter from Walter and Marilyn**
10 **Stocks, 39 Gordon Drive (see Attachment #4) which was addressed to the**
11 **Planning Board, Town Council and Town Manager, which expressed**
12 **concerns over additional traffic created by Woodmont, the scale of the**
13 **development and its proposed buildings, and the impact on taxpayers to**
14 **financially support any necessary improvements to existing roads. They**
15 **urged the public to attend public hearings concerning Woodmont.**

16
17 **M. Soares made a motion to extend the 65-day review period to**
18 **September 11, 2013 as requested by the applicant. L. Wiles seconded**
19 **the motion. No discussion. Vote on the motion, 9-0-0.**

20
21 **M. Soares made a motion to continue the Woodmont Commons PUD**
22 **Public Hearing to the September 11, 2013 Planning Board meeting. L.**
23 **Wiles seconded the motion. No discussion. Vote on the motion, 9-0-0.**

24
25 A. Rugg announced that this public hearing would continue with a discussion
26 about the proposed Development Agreement.

27
28 A. Rugg asked for public input on the Development Agreement. Comments
29 and questions were as follows:

30
31
32 1. **John Wilson, Tranquil Drive, asked if the Development Agreement**
33 **was a collaboration of the Town and the developer.** A. Rugg
34 answered that it was created through the attorneys of the Town and the
35 developer.

36
37 2. **J. Wilson said he had numerous questions about the**
38 **Development Agreement but asked that due to the late hour, he be**
39 **allowed to continue his questions at the September 11 hearing after**
40 **addressing key questions at this time.** A. Rugg suggested emailing
41 additional questions and comments to C. May, but J. Wilson stated his
42 preference to ask them at a public hearing.

43
44 3. **J. Wilson** indicated his approval of the developer being required to pay
45 for shortfalls if the development is not determined to be tax positive,
46 although he **questioned the perceived brevity of the document and a**
47 **lack of details, particularly concerning the proposed resolution of**
48 **any disputes between the Town and the developer.**

49

1 **4. J. Wilson suggested an independent Certified Public Accountant**
2 **(CPA) found suitable by both the Town and developer be hired to**
3 **develop the calculations needed to determine tax positivity and**
4 **then confirm the annual results provided by the developer. A.**
5 Pollack read the methodologies proposed by the developer to determine
6 whether the project is tax positive (see pp. 17-18 of Attachment #5). **T.**
7 **Freda questioned the criteria used to create the formula, saying**
8 **they were not reflective of potential impacts to the Town.** He added
9 that the FIA created by an economist should not be the basis of the
10 calculations and agreed that a CPA be used instead. A. Pollack defended
11 the capabilities and credentials of the Woodmont Commons fiscal impact
12 consultant, DPF, adding that the methodology had been peer reviewed
13 through HSH who found it sound and subsequently vetted through a
14 presentation to the Board and the public. Town Attorney Michael Ramsdell
15 explained that the formula was based on the FIA which included interviews
16 between DPF and Town departments to address potential capital and
17 operating costs resulting from Woodmont Commons. The projections
18 created from that input was then provided to those departments for their
19 review and comment. The result was an agreement between the Staff,
20 HSH, and the Woodmont Commons team on the formulas and models.
21 While HSH found the report to be somewhat optimistic, they still agreed it
22 demonstrated adequately that the project could remain tax positive. The
23 Development Agreement then took the concept further to address concerns
24 of the Board that the tax positive status should be reviewed on an annual
25 basis. Negotiations have continued throughout the Development
26 Agreement process, language has been amended and agreed to by each
27 party. M. Ramsdell noted that outstanding concerns from the Board about
28 fiscal impacts to the School system were addressed as recently as today
29 through a discussion between himself, the Town Manager, School
30 Department Superintendent, School Attorney, and the School's Financial
31 Officer to ensure the needs of the school could be adequately met.
32 Regarding dispute resolutions, M. Ramsdell said dispute would assuredly
33 arise at some point over the 20-year life of the project, however the
34 Development Agreement is clear so to how determine revenue amounts and
35 tax positivity. J. Wilson stated that he verified with the School District that
36 they were only asked to address operating costs, not capital costs,
37 therefore the figures shown in the School FIA were akin to those in a short
38 term budget rather than a long term financial plan addressing capacity. A.
39 Pollack stated his objection to J. Wilson's assertion, saying it was incorrect.
40 **J. Wilson reiterated his suggestion to formulate a method to**
41 **determine tax positivity that includes enough detail to ensure its**
42 **proper use in the future, especially if the project is transferred to**
43 **another developer.**

44
45 **5. J. Wilson suggested the Development Agreement should provide**
46 **the Town with a security interest to safeguard taxpayers from a**
47 **future developer's inability to fund any shortfalls in their tax**
48 **positive status.**
49

1 **6. J. Wilson stated a clause should be included in the Agreement to**
2 **address any ambiguities between a vote by Londonderry taxpayers**
3 **to make capital expenditures that will provide services believed to**
4 **be the result of Woodmont Commons and the developer's possible**
5 **claim that Woodmont Commons did not create the demand for the**
6 **services in question.** A. Sypek pointed out that when a public service
7 need is determined, it is based on the need of the Town as a whole, not the
8 need of an individual development. L. El-Azem noted the items the
9 developer has already anticipated as outlined in the Development
10 Agreement (e.g. cemetery land, a police substation, etc.) and suggested
11 that the Board's expectations should be limited to what is offered. J. Wilson
12 asked that the Board consider long term capital impacts.

13
14 **7. J. Wilson asked if the developer would be able to recoup any**
15 **payouts made due to a shortfall if they regain their tax positive**
16 **status the following year.** M. Ramsdell said there is no provision in the
17 Development Agreement that would allow the developer to do so.

18
19 **8. J. Wilson questioned the concept of the Town using eminent**
20 **domain to take property if the developer cannot make a suitable**
21 **arrangement to buy property.** A. Rugg and M. Ramsdell explained that
22 the Development Agreement states that the Town must find such a taking
23 in the best interests of Londonderry and can refuse. M. Ramsdell said that
24 is simply a statement of existing State law.

25
26 **9. J. Wilson questioned the practicality of the provision allowing a**
27 **developer outside of Woodmont Commons to make use of excess**
28 **capacity created by Woodmont Commons and therefore allow**
29 **Woodmont Commons to ask the Town to impose an impact fee on**
30 **the outside developer in order to recoup the costs of the excess**
31 **infrastructure now being used to service that outside development.**
32 A. Rugg said this is something currently done in town. J. Wilson suggested
33 a provision be added that if a legal issue subsequently ensues, Woodmont
34 Commons must pay the Town's resulting legal costs to defend the provision.

35
36 **10. J. Wilson asked if there were any current provisions in place**
37 **that obligate the Town to accept private streets as public and**
38 **therefore maintain them.** A. Rugg said there is no requirement on the
39 part of the Town to do so.

40
41 **11. J. Wilson asked if it was accurate that the Development**
42 **Agreement allows the developer to request public assistance in**
43 **paying for infrastructure improvements.** A. Rugg confirmed the
44 developer could ask, but that residents would have the ability to decline by
45 a vote at a Town Meeting.

46
47 **12. J. Wilson asked if the developer is interested in creating a Tax**
48 **Increment Finance (TIF) district.** A. Rugg said a TIF would be approved
49 by a vote of the taxpayers.
50

1 13. **Mike Speltz, 18 Sugarplum Drive, confirmed that the August dates**
2 **referred to in the Development Agreement do not indicate that the**
3 **document will be adopted by the end of the month.**

4
5 14. **M. Speltz asked if the Town Council will adopt the Development**
6 **Agreement before advisement from the Planning Board as he**
7 **interpreted the last sentence of Section III.A (p. 7) to indicate.** M.
8 Ramsdell said the sentence states that the Board must first approve the
9 PUD Master Plan before the Council approves the Development Agreement.

10
11 15. **M. Speltz suggested adding a provision of duration regarding**
12 **substantial progress into Section III.B (p. 7) to release the Town**
13 **from the agreement if a hiatus of development extends for a**
14 **significant amount of time.** M. Ramsdell said the Development
15 Agreement also acknowledges additional vesting beyond the 20-year
16 provision under III.B by observing State law of substantial progress that
17 allows the developer to vest his rights in the Agreement as long as there is
18 substantial progress within a four year period. A. Rugg noted that III.B also
19 protects the town because the development must remain tax positive in
20 each of the 20 years.

21
22 16. **M. Speltz asked if the Board had determined the PUD to exist as**
23 **a single development under the Town's ordinance since I-93 divides**
24 **it.** A. Rugg said the Board had determined the PUD to be a single
25 development. **M. Speltz asked if the Board had accepted the FIA as**
26 **he had heard Attorney Ramsdell state.** A. Rugg clarified that while
27 there has been positive consensus on both issues amongst Board members,
28 no votes or official adoptions of any kind have taken place.

29
30 17. **M. Speltz asked that a phasing plan of some degree be added to**
31 **the PUD Master Plan or Development Agreement to give the Town a**
32 **vision in order to anticipate future needs and demands on the town.**
33 L. Wiles voiced his intent to vote approval of the Master Plan only if the
34 developer is required to submit both an annual and five year phasing plan
35 and report annually on any deviations from the previous report.

36
37 18. **M. Speltz suggested attaching the Town's Growth Management**
38 **Ordinance and phasing ordinance to the PUD Master Plan.**

39
40 19. **M. Speltz stated that the Declaration of Restrictive Covenants**
41 **gives the developer and any subsequent developer the ability to**
42 **intervene in any land use decisions involving individual**
43 **developments within Woodmont Commons.** He suggested adding a
44 sunset provision so that a developer's authority is not allowed in perpetuity.

45
46 20. **M. Speltz suggested that standards referred to in the**
47 **Development Agreement be described as "those in effect at the time**
48 **of site and subdivision plan acceptance by the Planning Board"** to
49 allow for advancements in technology and methodology that may occur
50 during the 20 year buildout of Woodmont Commons.

1
2 **21. Mike Brown, 5 Carousel Court, asked for clarification regarding**
3 **whether the Development Agreement has any connection to the**
4 **waivers requested in the PUD Master Plan, i.e. whether they are**
5 **mutually exclusive.** M. Ramsdell said that to his knowledge, there are no
6 waiver provisions in the Development Agreement. **M. Brown confirmed**
7 **that any of the waivers requested in the PUD Master Plan can be**
8 **stricken before its final approval by the Planning Board even if they**
9 **are not referred to in the Development Agreement (e.g. phasing).**

10
11 **22. M. Brown asked how the developer can be exempted from the**
12 **GMO once it sunsets if the ordinance allows for the Town Council to**
13 **reenact it.** M. Ramsdell said there is no waiver request of the GMO in the
14 Development Agreement. L. El-Azem stated it has been explained that the
15 sunset provision is not pertinent to Woodmont Commons, that there is no
16 waiver request from the GMO in the Development Agreement, and that
17 there is a request for a waiver from the phasing ordinance in the PUD
18 Master Plan. T. Freda asked if the statement on p. 21 of the Development
19 Agreement that "no future town development moratoria...shall apply to the
20 [Woodmont Commons] Development" applies to the GMO. M. Ramsdell
21 replied that the language is a reference to any State statute or Town
22 ordinance not currently in existence. Once the developer has vested, a new
23 ordinance would not apply. M. Ramsdell said the sunset provision of the
24 GMO will most likely not have any impact on the development.

25
26 **23. M. Brown asked for clarification that no changes to any of the**
27 **Town's zoning ordinance over the next 20 years will impact**
28 **Woodmont Commons** as indicated in Section II.C(4) on p. 6 of the
29 Development Agreement. M. Ramsdell answered that once a developer's
30 rights vest under State statute, no additional changes to an ordinance will
31 apply to that development.

32
33 There was no additional public input.

34
35 A. Rugg stated that the public hearing was continued to September 11.

36
37 L. El-Azem asked if the Chair would entertain a recommendation to the Town
38 Council regarding the Development Agreement. A. Rugg replied that it would
39 be appropriate to make such a recommendation now since the Town Council
40 has the authority to amend any of the details.

41
42 **L. El-Azem made a motion to recommend to the Town Council the**
43 **adoption of the Development Agreement with the condition that the**
44 **Planning Board must approve the PUD Master Plan and with**
45 **confirmation of the language regarding water on p. 16 of the**
46 **Development Agreement. R. Brideau seconded the motion.**

47
48 A. Rugg entertained discussion on the motion. L. Wiles asked if a public
49 hearing would be held on the Development Agreement revision submitted
50 August 28. A. Rugg said this public hearing would constitute as the Planning

1 Board's public hearing on the document, adding that the Town Council would
2 have their own separate public hearing. J. Laferriere and L. Wiles expressed
3 concern that a sufficient amount of time was not given to the public to review
4 the document. A. Rugg said the motion was only to recommend approval to
5 the Town Council who will have the ability to request further amendments. J.
6 Laferriere asked if Board members believed they had adequate time to review
7 the document. M. Newman stated that she did, including confirmation that all
8 of the items the Board had discussed with the Town Attorney as needing
9 correction or clarification had been addressed in this revision. M. Soares asked
10 why a surety bond is not a requirement of the Development Agreement in
11 order to ensure any shortfall in the tax positive status is paid by the developer.
12 M. Ramsdell replied that in addition to an adequate amount being unknown,
13 demonstration of tax positivity will be done annually, so the chance that a bond
14 is needed would be very low. Additionally, the master developer cannot assign
15 any substantial portion of any assets of the development without approval by
16 the Town, therefore the Town would have the ability if a new master developer
17 took over to request a bond after assessing the new developer's financial
18 ability to cover any shortfalls. M. Ramsdell said there is language included in
19 the Agreement that any disputes will be governed under State law and go to
20 either Federal District Court or Rockingham County Superior Court.

21
22 Seeing no further discussion, A. Rugg called for a vote on the motion. **Vote on**
23 **the motion: 7-2-0 with L. Wiles and T. Freda in opposition.**

24
25 A. Rugg said the Development Agreement would go to the Town Council for
26 approval and that the Council would make final determination on the issues in
27 the Agreement.

28
29 **Other Business**

30
31 There was no other business.

32
33 **Adjournment:**

34
35 **M. Newman made a motion to adjourn the meeting. L. Wiles seconded**
36 **the motion. Vote on the motion: 9-0-0.**

37
38 The meeting adjourned at 11:54 PM.

39
40 These minutes prepared by Associate Planner Jaye Trottier

41
42 Respectfully Submitted,

43
44
45
46 Lynn Wiles, Secretary



**WOODMONT COMMONS
PLANNED UNIT DEVELOPMENT
MASTER PLAN
AUGUST 2013**

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1.0 PUD Master Plan Information

WOODMONT COMMONS

PLANNED UNIT DEVELOPMENT MASTER PLAN

JULY 2013

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1.1 PURPOSE OF THE WOODMONT COMMONS PUD

Section 1.0 PUD Master Plan Information is non-regulatory and provides information about the project, Woodmont Commons. The Woodmont Commons Planned Unit Development Master Plan (PUD Master Plan) contains the associated regulations and standards in *Section 2.0 PUD Regulations and Standards*, *Section 3.0 Mitigation and Improvement Requirements* and *Section 4.0 Supplemental Documents*.

1.1.1 Intent of the Woodmont Commons PUD

The purpose of *Section 2.8 Planned Unit Development* of the Town of Londonderry, New Hampshire Zoning Ordinance (*Londonderry Zoning Ordinance*) is to create a process to allow owners to propose projects largely independent from current land use regulations.

The Woodmont Commons Planned Unit Development Master Plan (PUD Master Plan) was prepared in response to the purpose of the ordinance. Its conception, preparation and analysis has been integrated with the desires and aspirations of the community. Over the course of multiple years, and through dozens of meetings with citizens and public officials of the Town, a vision, a master plan and associated rules and regulations were crafted to accomplish the following goals:

- Accommodate development in an environmentally responsible manner;
- Create a development that will, upon completion, facilitate choice among citizens as to where and how they want to live, at the various stages of their lives and according to the resources available to them;
- Establish a framework for development that will deliver a walkable environment that comprises a variety of uses and building types;
- Develop in a manner that is in alignment with accepted standards and performance for public infrastructures;

- Allows for substantial commercial and mixed-use development to create a positive economic impact for the town;
- Create regulations and establish a process that allows flexibility in phasing and in the actual development plan.

1.1.2 PUD Planning Process

Woodmont Commons informally began its planning process in Spring, 2010 and included preliminary meetings with local and State officials and a preliminary design charrette.

A much larger and fully public week-long design charrette was held on the property in September, 2010. This charrette was led by the owners and the firm of Duany Plater-Zyberk & Company of Miami.

The first discussions with Londonderry Community Development Department staff and other Town staff pursuant to *Section 2.8.3.1.1* of the *Londonderry Zoning Ordinance* commenced in Spring 2010 and conceptual discussions with the Planning Board pursuant to *Section 2.8.3.1.2* of the *Londonderry Zoning Ordinance* soon followed.

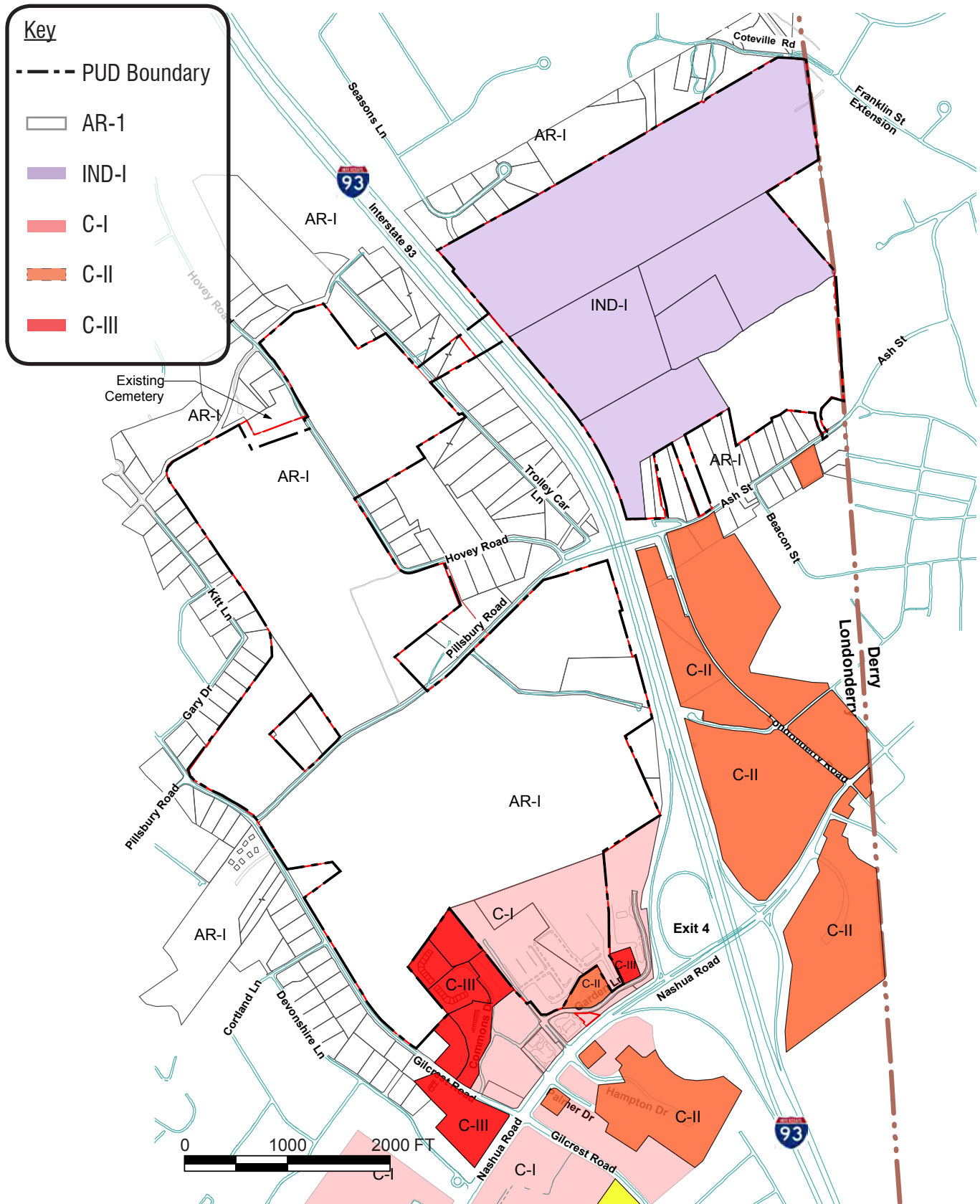
These conceptual discussions with the Planning Board continued into 2011, while at the same time Attorney John Michels held meetings at the site twice weekly for months. These on-site public meetings allowed discussions about the project in a less formal setting.

Over the course of several months, the concept of the Woodmont Commons PUD evolved and adapted to meet areas of concern and opportunities. Since this is the first PUD application to be submitted in Londonderry, new issues arose, which were discussed and addressed.

1.1.3 Location



1.1.4 Current Underlying Zoning



1.2 INFORMATION PLANS

1.2.1 Early Plans

As a component of the rezoning process prescribed in *Section 2.8 Planned Unit Development* of the *Londonderry Zoning Ordinance*, a series of concept plans were prepared during the planning process to demonstrate how projects may develop in accordance with the PUD Master Plan. Additional land was added to the project, but it remains approximately 600 acres on both sides of I-93.

Conceptual discussions with the Planning Board developed the ideas of breaking the site down into Subareas and establishing ranges and limits of land uses around the PUD. The plans have evolved as a response to the comments received during these public discussions. The project submittal package was accepted by the Planning Board as a Complete Application in October, 2012.



(A) Examples of early plans from the charrette

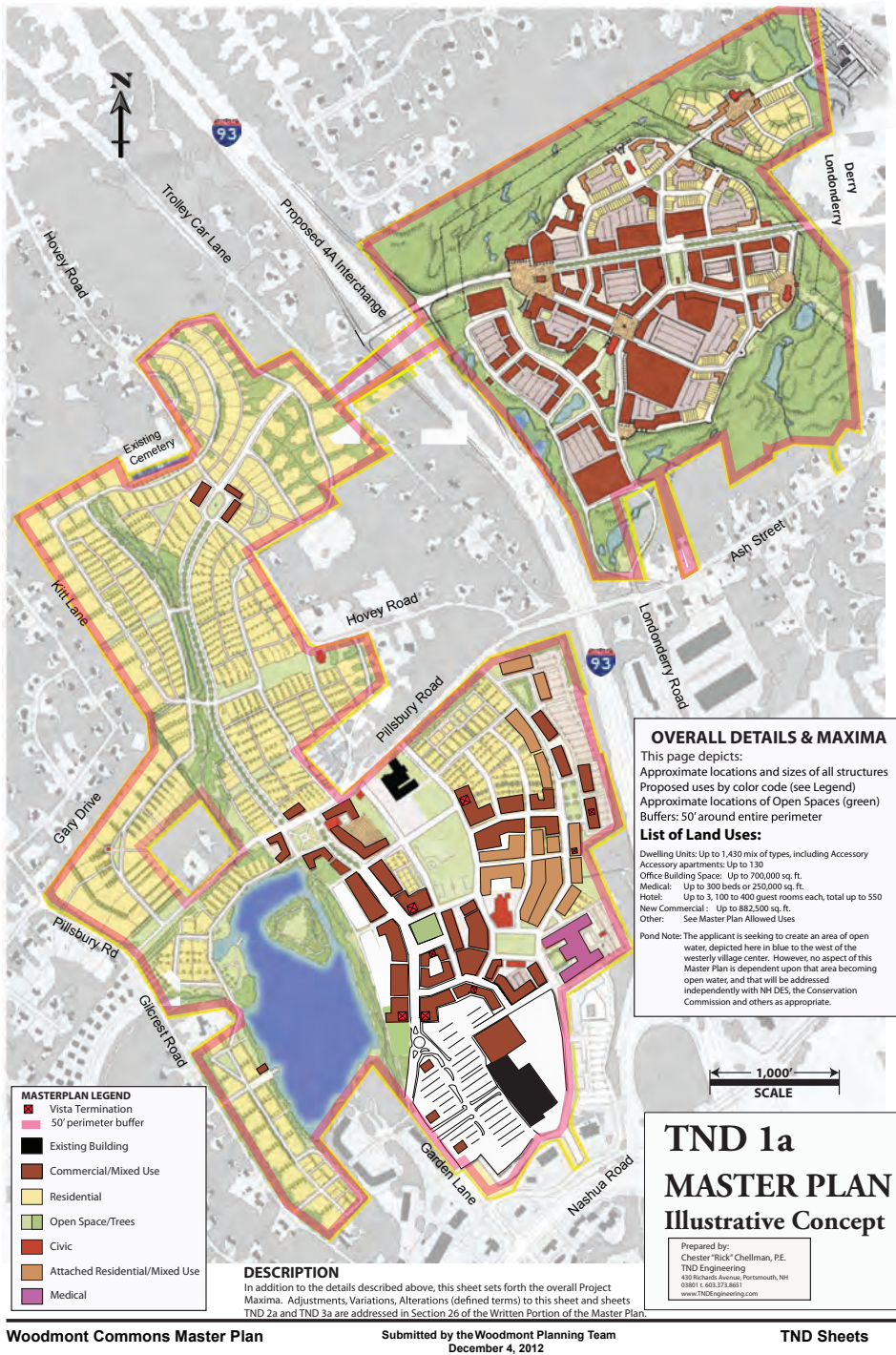
The charrette provided input for a number of plans that were incorporated into the plans submitted with the submission presented to the Planning Board in October, 2012.

1.2.1 Early Plans



(B) Examples of early plans from the charrette

1.2.1 Early Plans



TND-1a

This was a concept plan from the revised project submittal package given to the Planning Board on October 3, 2012. The Planning Board accepted this document as a complete application in October, 2012.

1.2.1 Early Plans



Preliminary Versions, Information Plan

These two concept plans were created during the development of the Information Plan as part of the review process for the complete application.

1.2.2 Information Plan Overview

Overview of the Information Plan

The Information Plan in *Section 1.2.4 The Information Plan* represents four planning concepts:

- Understandings to accommodate abutters;
- Maximum yield profile for the development;
- A graphic example of the form of development required by the Master Plan;
- One of many possible development programs and therefore site plans that may occur in the future.

Information Plan by Subarea

The items shown on the Information Plan are presented as approximations or preliminary estimates subject to change. This includes the proposed general estimates of location, size, use(s) for each structure; the proposed general estimates of location and width of all streets, drives, sidewalks, and paths; the proposed general estimates of location and number of spaces for each parking area; the proposed open space areas; and the natural and cultural resources proposed to be preserved. The Perimeter Buffers to adjoining property are shown on the Information Plan in *Section 1.2.4 The Information Plan* and in *Section 2.2.2 Land Use Plan*.

Open Space

The approach to Open Space and Conserved Green Space within the PUD Master Plan is based upon several key ideas:

- Perimeter Buffers – the Woodmont Commons PUD will include a Perimeter Buffer with landscaping to screen existing single-family residences from adjoining incompatible uses. Where uses on both sides are the same or are compatible, then the landscape treatment, site improvements and architecture may be planned to complement each other and enhance views to, from and across the sites. Along Gilcrest

Road, this Perimeter Buffer would include preservation of some existing orchard plantings or of similar trees in an orchard configuration;

- An enhanced agricultural impoundment as an open space feature – A large, natural and low-lying area exists near the southwest corner of the Woodmont Commons PUD in Subarea WC-3 that is an existing agricultural impoundment. To the extent that it may be approved by state and federal regulatory authorities, it is proposed that this area be conserved and become an enhanced open water impoundment with bordering open space and public access along its perimeter, connecting to local streets and paths. Should state and federal regulatory approval be denied, WC-3 may be conserved in a manner similar to the diagram on the next page;
- An accessible, ecological approach to major stormwater drainage features – Existing agricultural drainage areas on the west side of the Woodmont Commons PUD would be modified and enhanced to become a network of open spaces and stormwater drainage corridors that accomplish both environmental and public access goals. The drainage ways, which lead to the enhanced impoundment, would be designed to provide incremental levels of water quality improvement for stormwater before it enters the feature. The corridors would also provide pedestrian and bicycle connections for the community at the perimeter of the landscaped space;
- Conserved Green Space in WC-12 – Conserved Green Space will be provided in Subarea WC-12 and connected by a Perimeter Buffer along residential edges of the PUD boundary. This buffer will serve as links to existing natural corridors that reach to the borders of the Woodmont Commons PUD. It is proposed that this Conserved Green Space would take advantage of wetland preservation or enhancements that emerge from the state and federal review and

1.2.2 Information Plan Overview

approval processes;

- Provision and distribution of publicly accessible open space – Publicly accessible space must be connected to a street, sidewalk network or walking path. A variety of different types of Conserved Green Space and/or Shared Open Space would be created within or directly accessible to the development and neighborhood fabric.



Alternate Information Plan

If the proposed pond in WC-3 is not approved by State or Federal authorities having jurisdiction, then one possible alternative is shown above.



Alternate Information Plan

This alternate information plan provides Single-Family Attached houses in WC-2 along Pillsbury Road instead of commercial uses

1.2.2 Information Plan Overview

Transportation

The Woodmont Commons PUD will be developed incrementally over a number of years. As a result, transportation accommodation will be required that matches the scale, type and pace of development in order to create a coordinated infrastructure that can safely and efficiently respond to the automotive trips generated by the development within the Woodmont Commons PUD.

In crafting the PUD Master Plan, several development scenarios, conceptualized mitigation areas, and transportation demand management measures were evaluated as options for enhancing the operating characteristics of the roadways and intersections and reducing the number of single-occupancy vehicle trips. The approach to transportation is founded on the following major principles and considerations:

- The PUD will generate some vehicle trips that will remain within the boundaries of the project (internal trip capture). Although the actual trend is expected to be higher, the conservative analysis prepared for the PUD Master Plan is based upon an estimated internal capture rate of 23%. This capture effect will be realized over time as a mixture of land uses are built and interconnected within the PUD;
- Traffic operations were analyzed over a 20-year horizon within the PUD Master Plan. The options within the model for future improvements account for an at-buildout condition during the traditional weekday morning and evening peak periods;
- Traffic study updates to track the effect of background development unrelated to development within the Woodmont Commons PUD and other changing conditions may be required at the time of PUD Subdivision and/or PUD Site Plan submittal. These updates will vary in scope depending on the scale of the development;
- The development potential within WC-12 will

expand if the New Hampshire Department of Transportation (NHDOT) constructs Exit 4A and the ancillary exit-related infrastructure. Without Exit 4A, the potential to develop certain land uses within WC-12 will be scaled appropriately to preserve an acceptable level of service;

- The planned roadway and intersection improvements will be coordinated with the Town of Londonderry and, where applicable, NHDOT. The conceptual designs contemplate several elements from the *Route 102 Corridor Study*, which was prepared for the Town of Londonderry by the Southern New Hampshire Planning Commission (SNHPC) in 2002. In some cases the physical improvements will require participation from the Town of Londonderry to secure rights-of-way and/or other legal interests for temporary and permanent impacts to adjacent land.

1.2.3 Interpretation





The Information Plan reflects one of many possible plans that can be prepared that conform to the rules and regulations of this PUD Master Plan. The example illustrations as shown within *Section 1.0 PUD Master Plan Information* depict a few of other possible approaches that may reflect future PUD Subdivision and PUD Site Plans that also conform to the PUD Master Plan.

1.2.4 The Information Plan

Illustration only. Refer to *Section 1.2.3 Interpretation*.



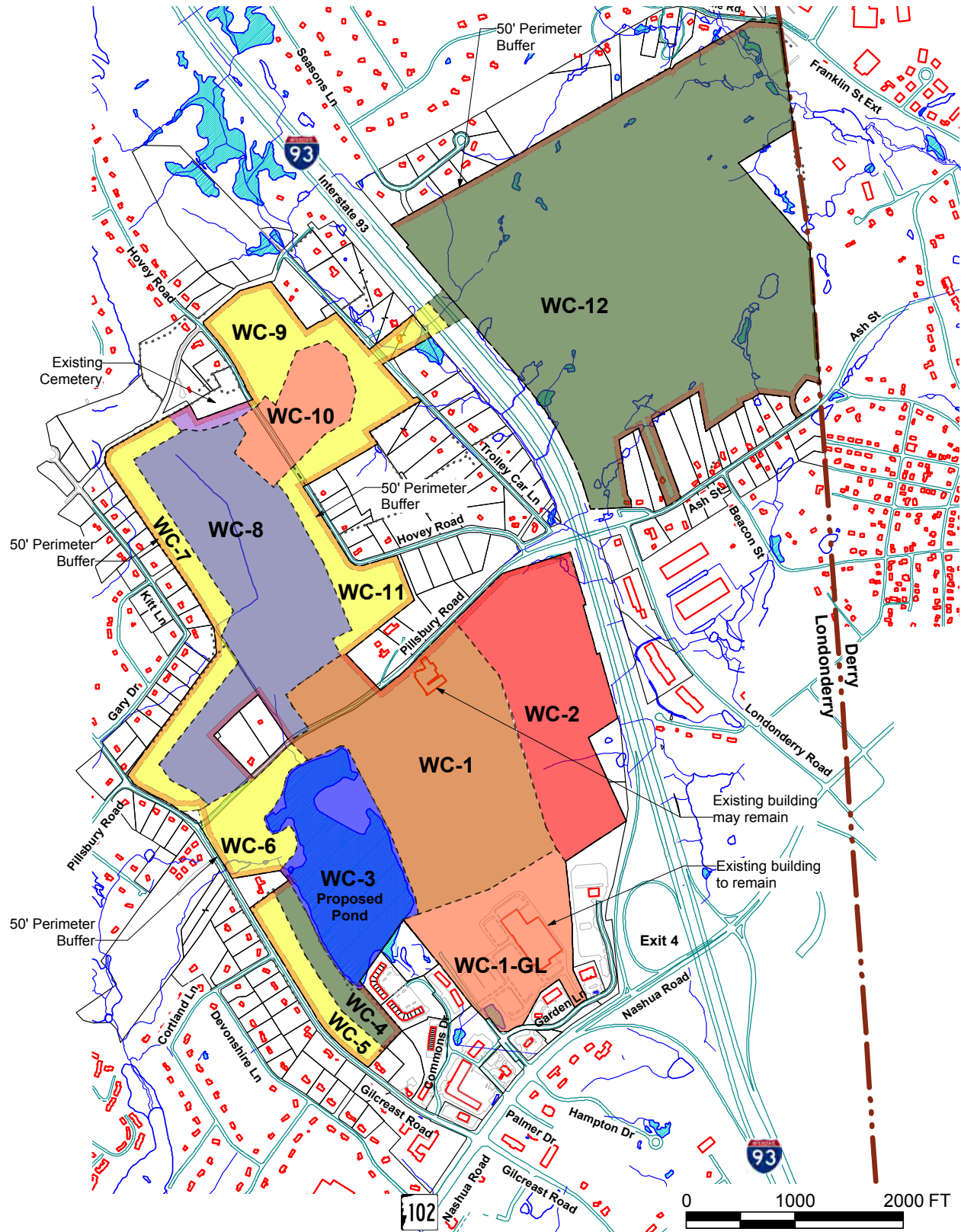
LEGEND

-  PUD Boundary
-  Existing Buildings
-  Existing Streets
-  Property Lines

INFORMATION PLAN

1.2.4 The Information Plan

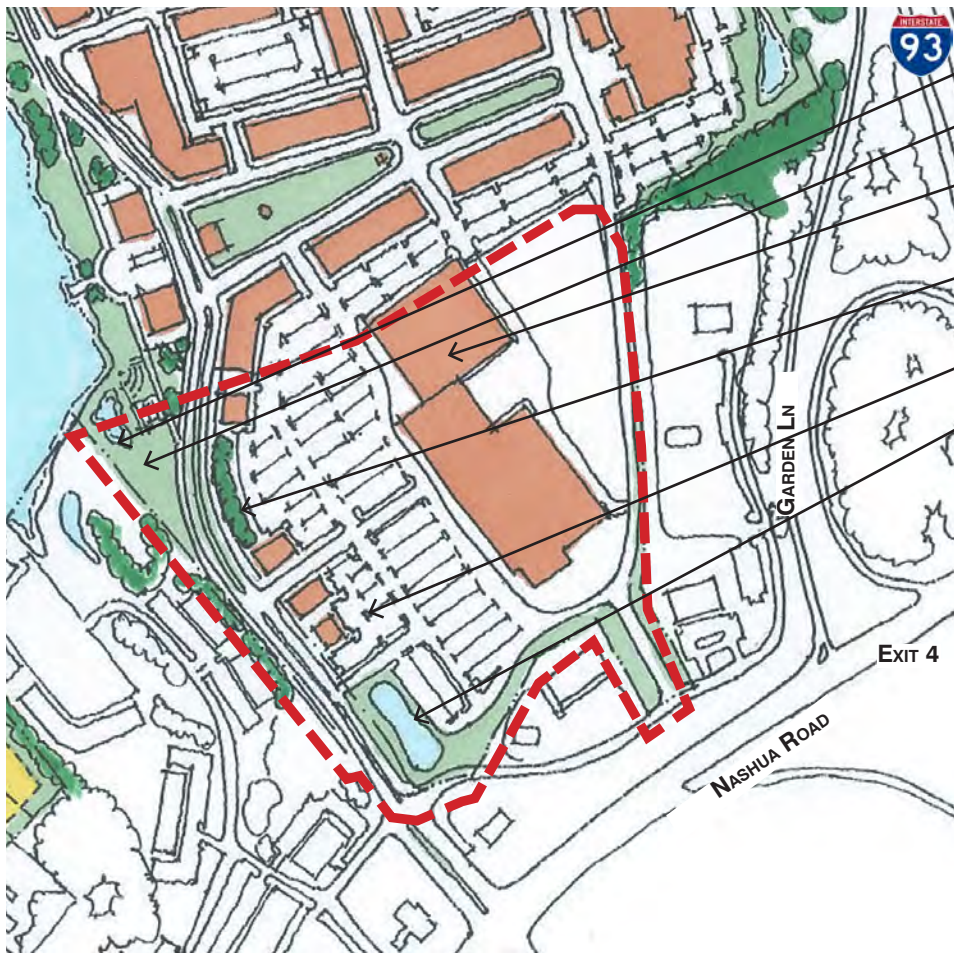
Outline of each Subarea.



SUBAREA PLAN

1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



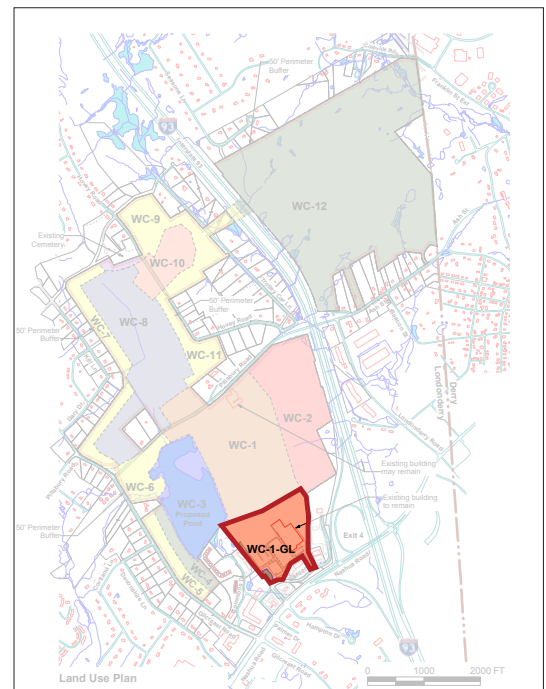
INFORMATION PLAN COMPONENTS

- Possible detention area
- Shared Open Space – Passive – Park
- New commercial uses with parking in front and rear for sites at the street
- New commercial uses or landscaping along entrance road
- Existing parking
- Conserved Green Space – with existing detention

SUBAREA WC-1-GL

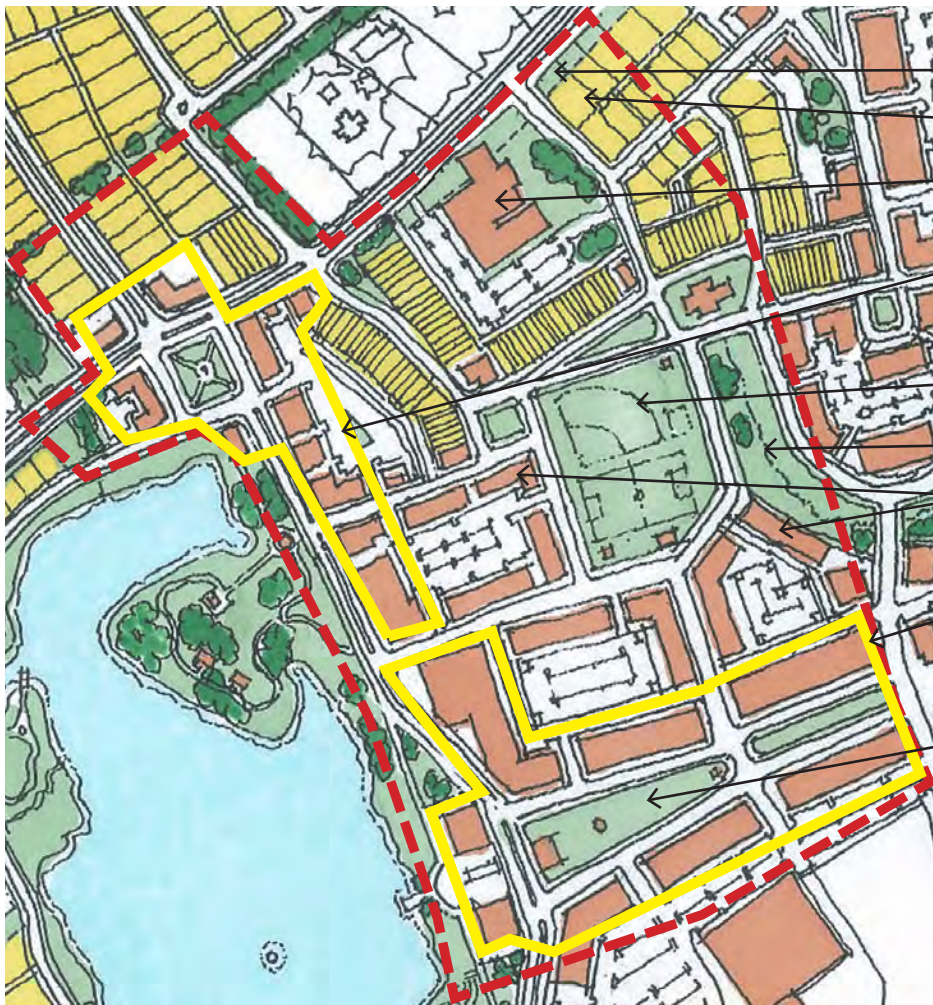
LEGEND

Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



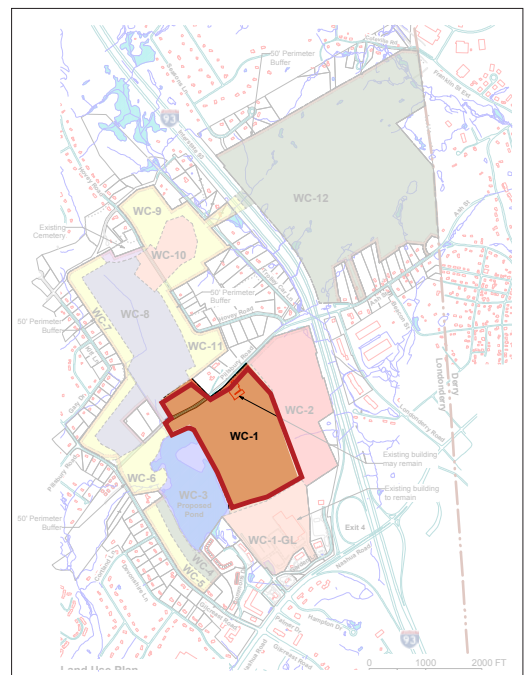
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Residential uses along Pillsbury Road
- Existing building may be preserved and reused
- Commercial uses transition in scale to smaller building footprints as they approach the edge of the Subarea
- Shared Open Space – Active
- Conserved Green Space
- Multifamily uses with potential ground floor commercial uses near the Village Center
- Village Center core area with commercial first floors and possible mix of uses above and integrated into the plan
- Shared Open Space – Passive – Green

SUBAREA WC-1

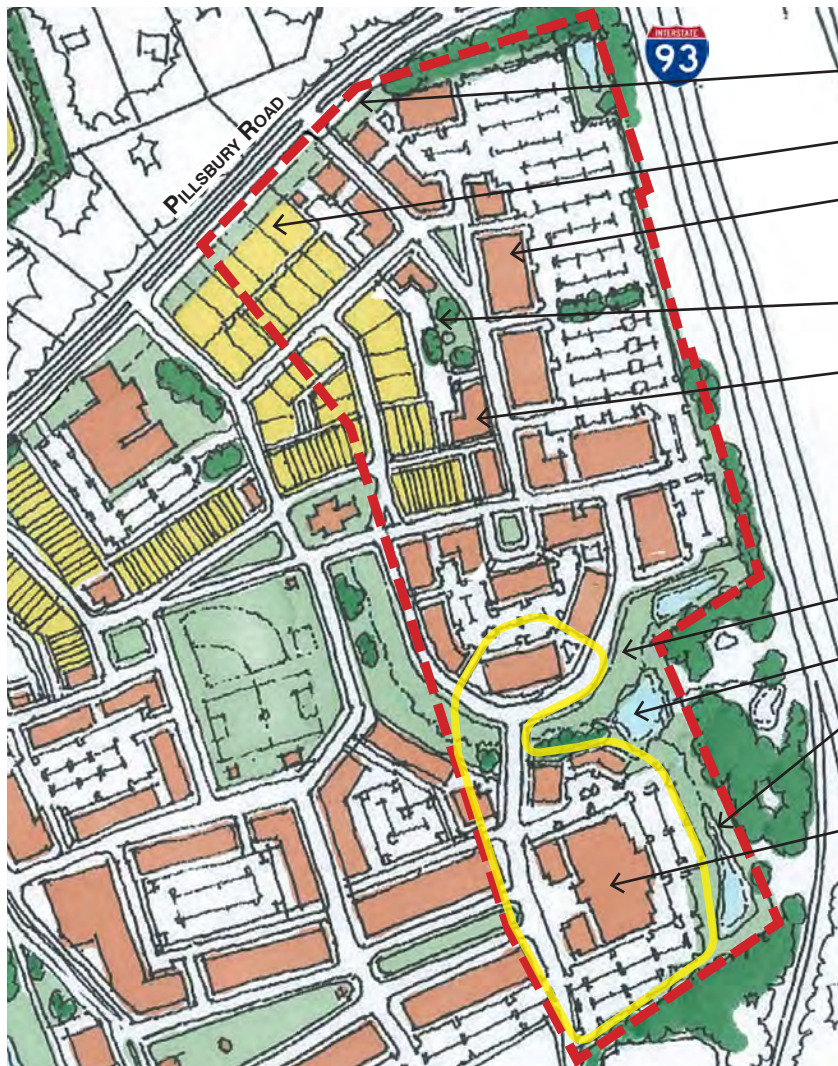
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Subarea Line



1.2.4 The Information Plan

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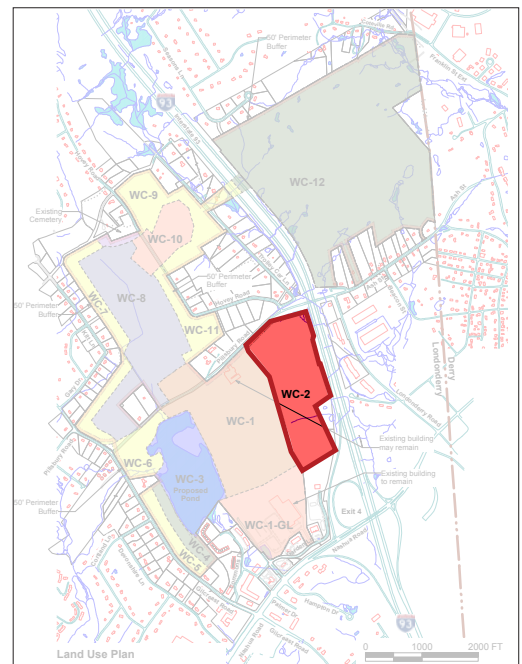
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Residential uses along Pillsbury Road
- Dedicated Office Building zone along I-93 corridor; note that other uses are permitted
- Shared Open Space – Passive
- Commercial, Accommodation, Civic and Residential uses transition in scale to smaller building footprints as they approach the edge of the Subarea where adjacent Subarea has similarly scaled uses
- Conserved Green Space
- Detention areas
- Village Center core area with possible large commercial anchor; note that other uses are permitted

SUBAREA WC-2

LEGEND

Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



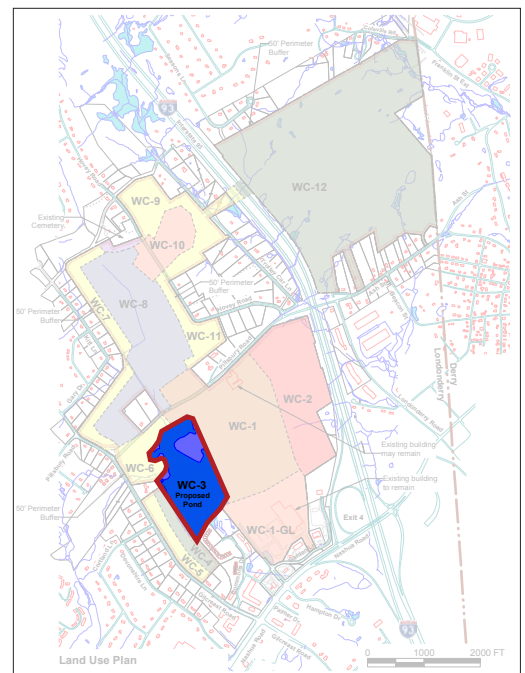
INFORMATION PLAN COMPONENTS

- Trail system
- Shared Open Space – Passive uses
- Trail system integrated with sidewalk at street
- Conserved Green Space – may include Active or Passive Shared Open Space with implementation of proposed pond, detention area
- Sidewalk or trail at perimeter of proposed pond

SUBAREA WC-3

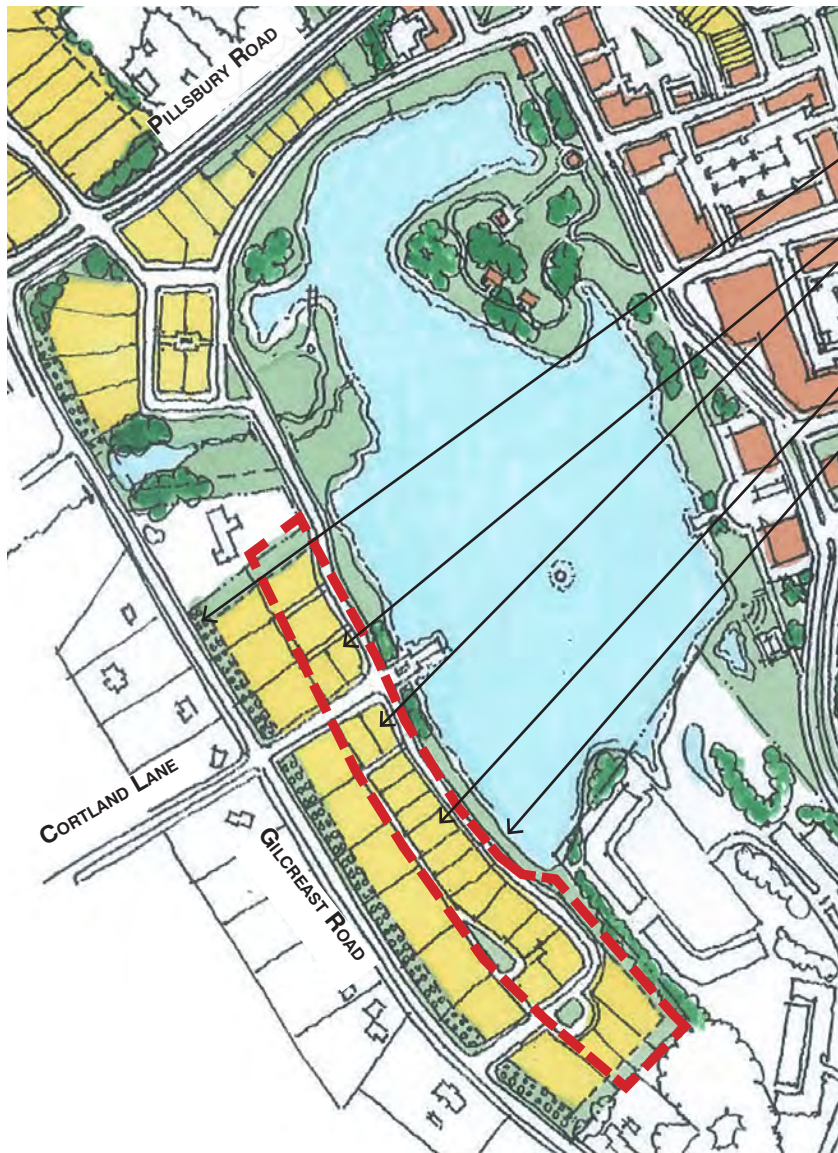
LEGEND

Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



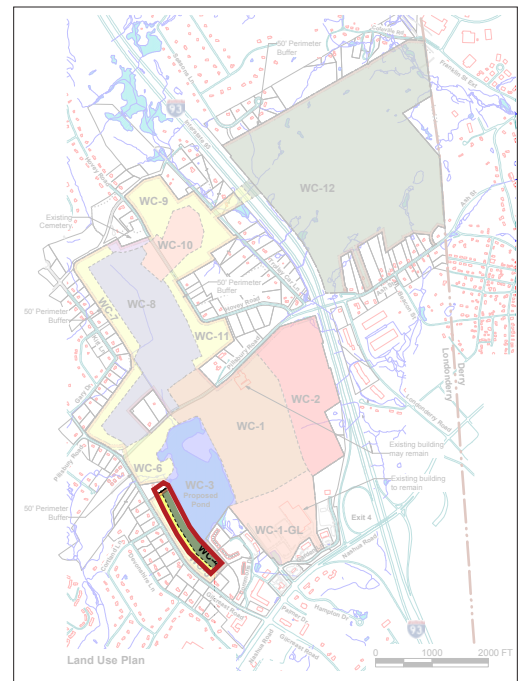
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Residential uses or limited Civic, Accommodation or Commercial uses at connector street to Gilcrest Road
- Residential uses
- Sidewalk or trail at perimeter of proposed pond

SUBAREA WC-4

LEGEND

----- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to *Section 1.2.3 Interpretation*.



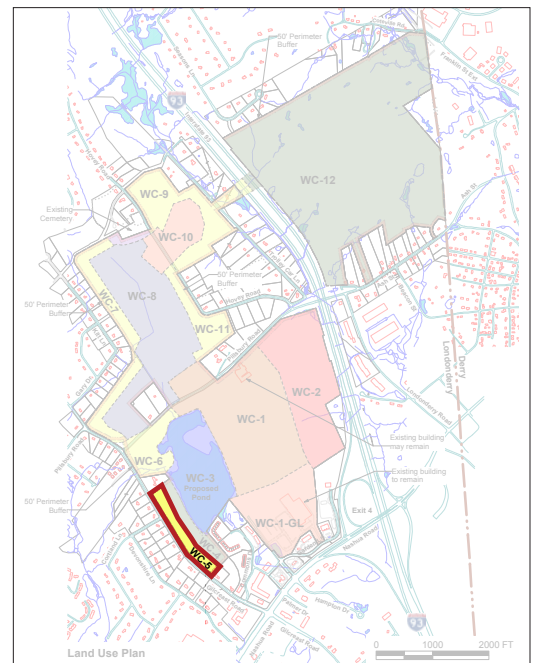
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Perimeter Buffer with orchard-like planting
- Residential uses similar to abutting residential uses
- Possible connection to Gilcrest Road

SUBAREA WC-5

LEGEND

Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



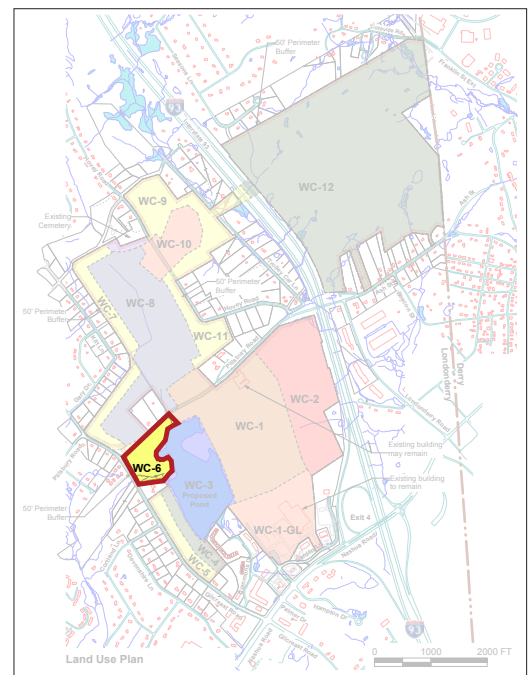
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Perimeter Buffer with orchard like planting
- Residential or other uses
- Conserved Green Space
- Perimeter Buffer

SUBAREA WC-6

LEGEND

--- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



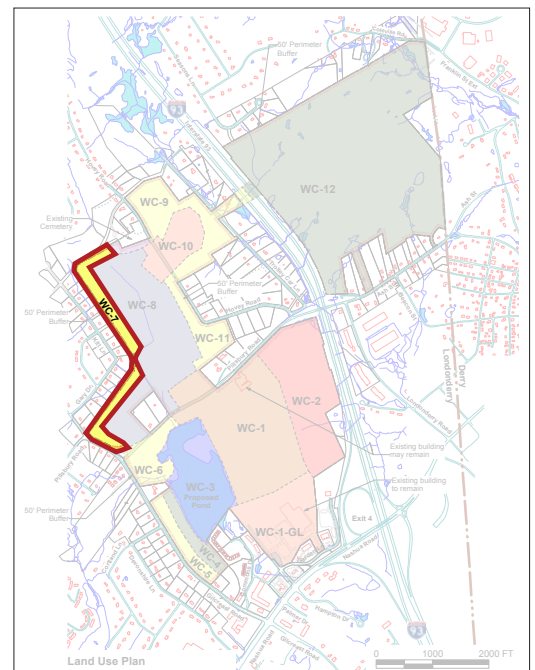
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Residential uses similar to abutting residential uses
- Possible trail connection
- Conserved Green Space

SUBAREA WC-7

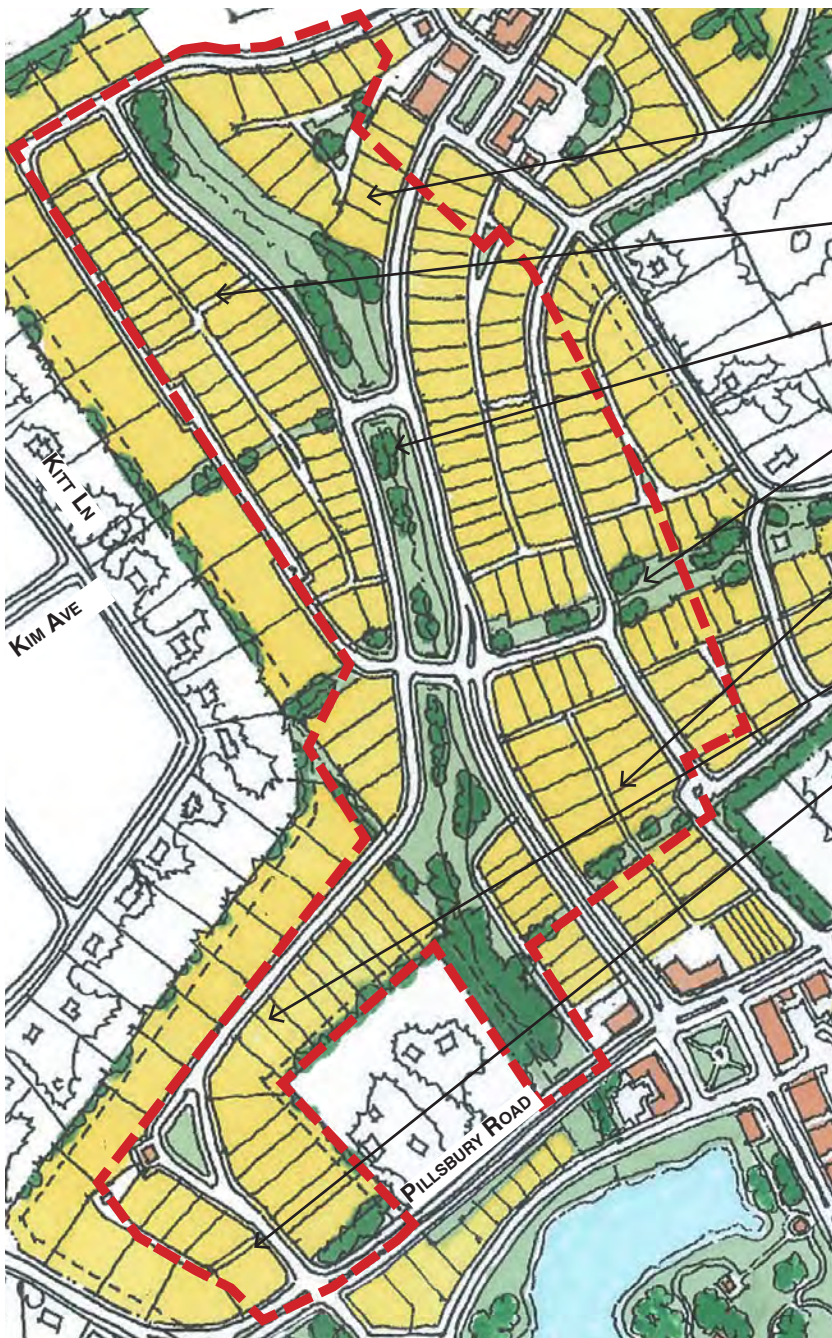
LEGEND

--- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



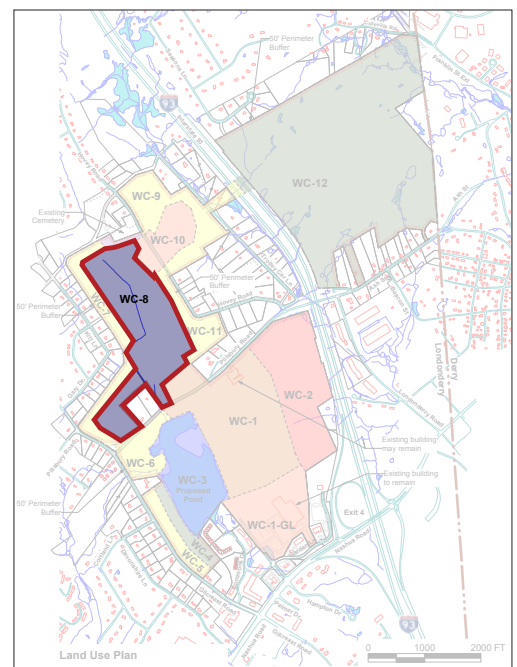
INFORMATION PLAN COMPONENTS

- Residential or other small neighborhood service type uses approaching Subarea WC-10
- Residential uses
- Conserved Green Space with trail system
- Shared Open Space – Passive with a trail system connecting to Conserved Green Space
- Residential or other small neighborhood service type uses approaching Subarea WC-1
- Residential uses
- Residential or other small neighborhood service type uses approaching Subarea WC-6

SUBAREA WC-8

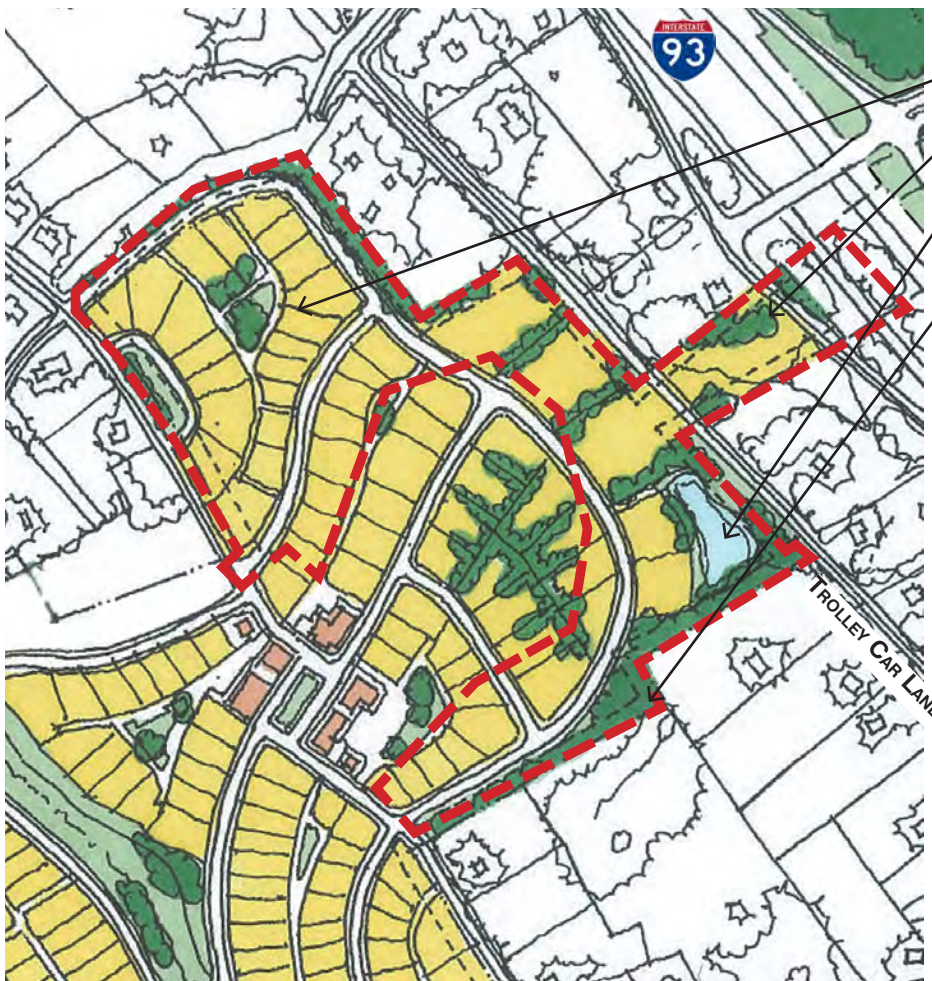
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Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



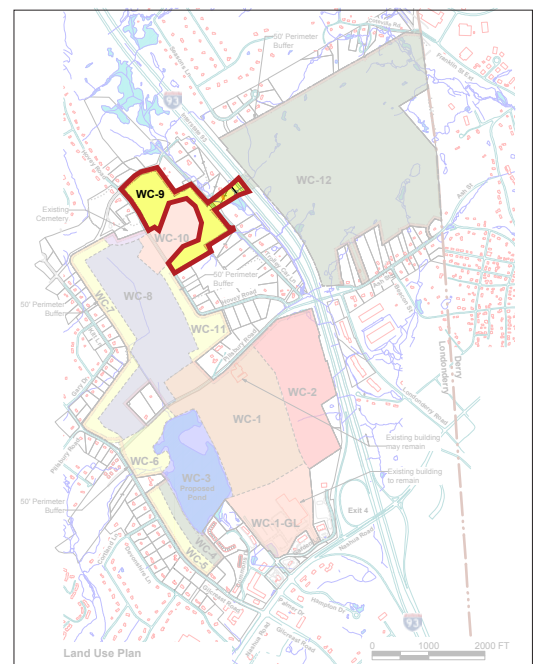
INFORMATION PLAN COMPONENTS

- Residential uses
- Conserved Green Space
- Conserved Green Space with possible detention area
- Perimeter Buffer

SUBAREA WC-9

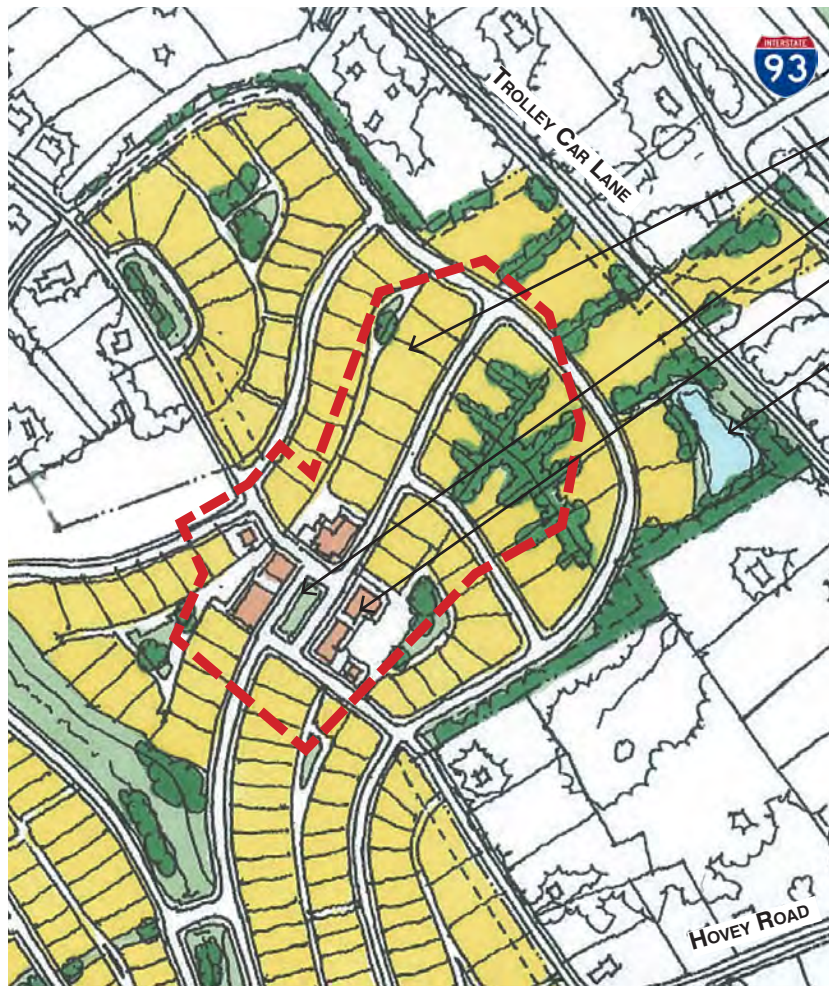
LEGEND

--- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



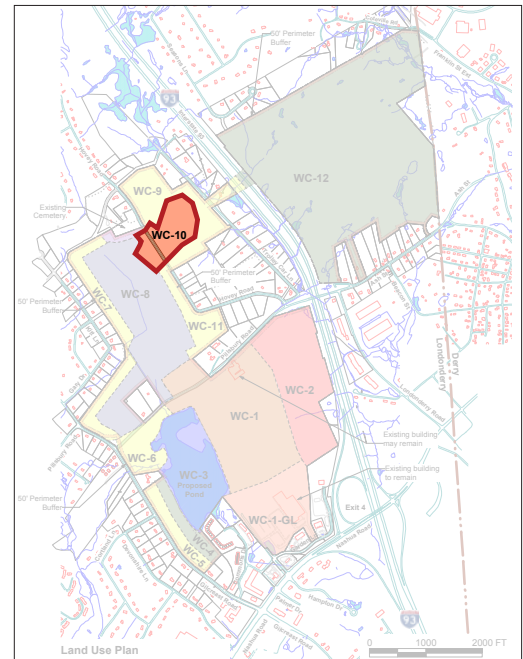
INFORMATION PLAN COMPONENTS

- Residential uses
- Shared Open Space – Passive – Square
- Commercial uses, small, neighborhood services type uses
- Possible detention area

SUBAREA WC-10

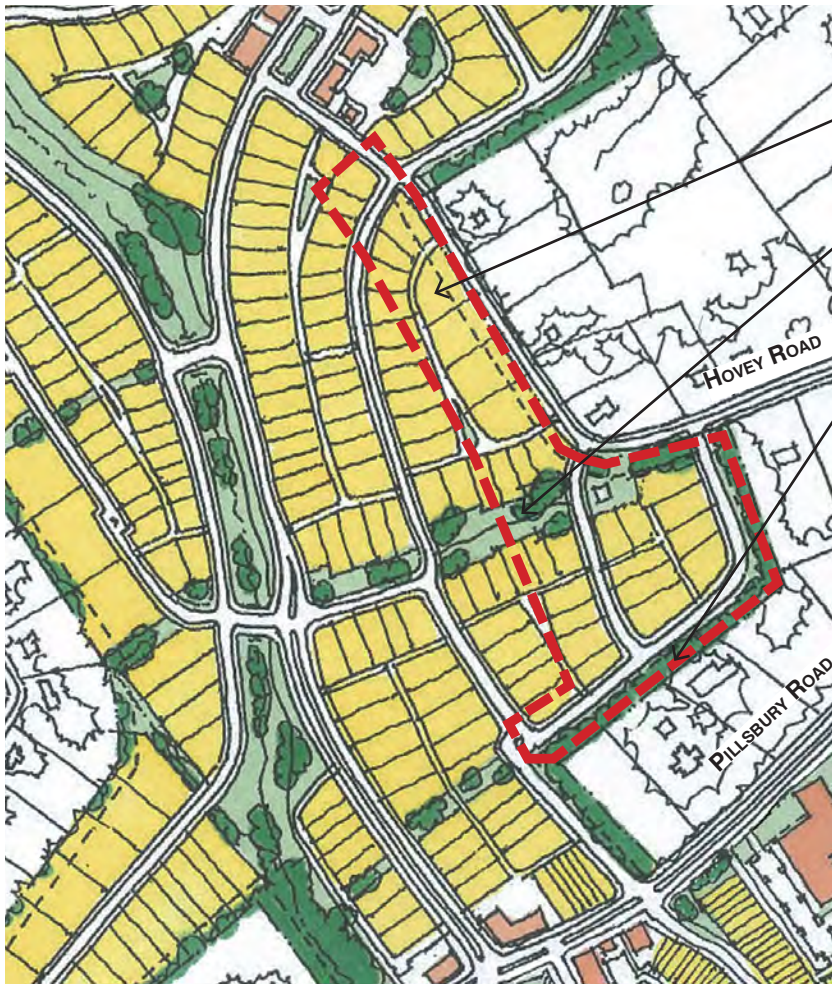
LEGEND

----- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to *Section 1.2.3 Interpretation*.



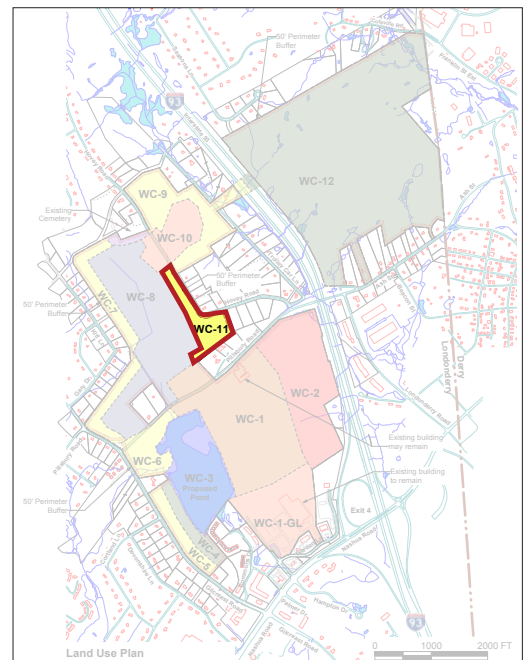
INFORMATION PLAN COMPONENTS

- Residential uses
- Shared Open Space – Passive – Green with trail connection to WC-8
- Perimeter Buffer

SUBAREA WC-11

LEGEND

 Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



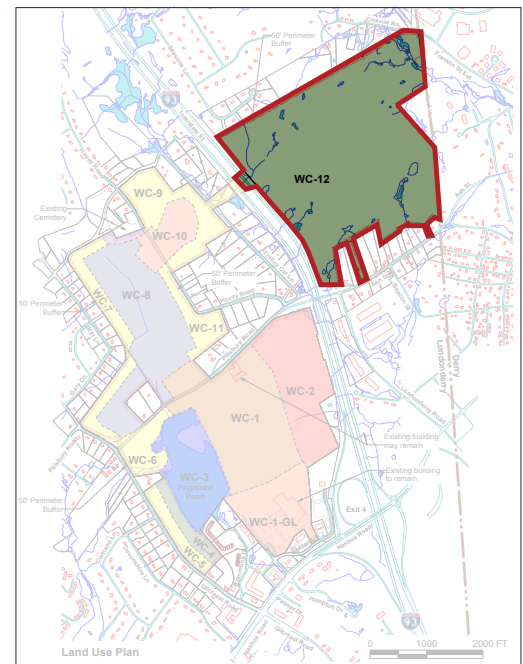
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Exit 4A
- Residential uses
- Possible trail connection
- Mix of uses including Commercial uses with a regional draw, larger format retail and dedicated office uses
- Shared Open Space – Passive – Square
- Shared Open Space – Passive – Park with connected trail system
- Conserved Green Space
- Possible Institutional or Office use
- Possible detention area

SUBAREA WC-12

LEGEND

Subarea Line



1.3 OWNERSHIP ARRANGEMENT

1.3.1 Current Ownership

The approximately 600 acres of land within the Woodmont Commons PUD is owned by the following: Pillsbury Realty Development, LLC (Pillsbury or “Developer”), Hyrax Derry Partners, LLC, Demoulas Supermarkets, Inc., and Robert D. and Stephen R. Lievens. Pillsbury, on behalf of these land owners, serves as the primary applicant and principal liaison to the Town of Londonderry with respect to planning and land use approvals. Pillsbury will likewise serve as the initial Developer on behalf of the land owners, presiding over internal development controls and density accounting, conveyances of land subject to approved restrictive covenants, and the Developers’ collective obligations pursuant to the Woodmont Commons Development Agreement.

1.3.2 Ownership Organization

The land owners are obligated to each other through a series of written and informal joint venture agreements. The Developer is obligated to the Town of Londonderry through the Woodmont Commons Development Agreement. The Developer is obligated to their grantees, assignees, licensees and other future development entities (collectively, the “Future Site Developers”) through the PUD Master Plan.

1.3.3 Covenants, Restrictions and Easements

Prior to the land owners’ conveyance of any property within the Woodmont Commons PUD and subsequent to the approval of the PUD Master Plan, the land owners shall each record in the Rockingham County Registry of Deeds a Declaration of Restrictive Covenants (hereinafter, collectively, “Declarations”), duly executed by the record owner or owners of each

parcel included within the Woodmont Commons PUD, stating that the land is within the zoning jurisdiction of the Town of Londonderry and is zoned as a Planned Unit Development pursuant to the terms of the PUD Master Plan.

Said Declarations shall further state that the land and use of the land is subject to regulations applicable to the Woodmont Commons PUD as stated in the PUD Master Plan, a copy of which is available for public inspection at the Planning Department of the Town of Londonderry (“Planning Department”) at the Londonderry Town Hall, 268B Mammoth Road, Londonderry, New Hampshire 03053.

In addition, Declarations shall specify that: (i) each deed conveying land within Woodmont Commons recorded subsequent to the recordation of the Declarations shall state that the respective conveyance is subject to the Declarations, but that the failure to include such statement, or to include a sufficiently suitable statement in a deed shall not affect the enforceability of the Declarations with respect to the property conveyed or invalidate the respective conveyance; and (ii) no projects, rezoning petition, PUD Master Plan adjustment, variation, alteration or amendment, application for variance, special exception or conditional use approval shall be submitted to the Town, or its land use boards, or proposed in any manner, without the written consent of the Developer, which shall not be unreasonably withheld.

A form of restrictive covenant, to be executed and recorded by each of the land owners, is included in *Section 4.6 Sample Restrictive Covenant*.

1.3.4 Master Property Owners’ Association

The Developer, the land owners and Future Site Developers are subject to the terms and conditions of the PUD Master Plan. No master property owners’ association is planned at this time. However, one or more associations may be created in the future.

1.4 ORGANIZATION OF THE PUD MASTER PLAN DOCUMENTS

1.4.1 Documents and Relationships

The PUD Master Plan is composed of the sections listed in the Table of Contents on page 3. These sections serve a variety of purposes and have specific relationships to the Town's land use and development ordinances and regulations. There is also a specific relationship between components of the PUD Master Plan and the Woodmont Commons Development Agreement (Development Agreement). The assembled documents in the PUD Master Plan update and replace, thereby superseding, components of the previously completed Woodmont Commons PUD Master Plan application to the Planning Board (October 3, 2012).

Londonderry Zoning Ordinance

The PUD Master Plan has been established in compliance with the *Londonderry Zoning Ordinance* and its provisions for a special approach to land use management, as defined by the *Londonderry Zoning Ordinance Section 2.8 Planned Unit Development*. As stated in the PUD Ordinance, alternative regulatory standards can be substituted for typical standards if those standards are specifically waived and new provisions provided in the PUD Master Plan are approved by the Planning Board. The *Londonderry Zoning Ordinance* describes:

“In devising the PUD Master Plan, subject to specific limitations, guidelines and objectives stated elsewhere in this ordinance, there is flexibility in the selection of land uses, density, setbacks, buffers, building heights, lot sizes, lot dimensions, parking and road design requirements, and most site design and development standards contained in the *Londonderry Zoning Ordinance*, *Site Plan Regulations*, and *Subdivision Regulations*.” (Section 2.8.3.1)

The PUD Master Plan has been developed in accordance with these provisions.

The PUD Master Plan contains a number of alternative Zoning provisions and associated waivers. The alternative Zoning provisions are contained in various portions of *Sections 2.0, 3.0 and 4.0*, and the Zoning waivers are listed in *Section 2.1 Introduction*. Existing typical Zoning standards may still apply to future projects, if those standards have not been waived in the approved PUD Master Plan. In such instances, the *Londonderry Zoning Ordinance* provisions in effect on the date of the completed the Woodmont Commons PUD Master Plan application (October 3, 2012) will be applicable. Otherwise, the PUD Master Plan shall be the governing document for all development within the Woodmont Commons PUD.

Section 2.8 Planned Unit Development of The *Londonderry Zoning Ordinance* lists a series of formal submittal requirements and information that must be provided to the Planning Board for their review prior to an approval, but which do not become part of the alternative standards and regulations that will be applied to the Woodmont Commons PUD. Components satisfying formal submittal requirements are included *Sections 2.0 and 3.0*. Other components that provide information and technical evaluations associated with the Planning Board's review and approval process as required by the *Londonderry Zoning Ordinance* are included in this Section and *Section 4.0 Supplemental Documents*.

PUD Master Plan Information

The planning process leading to Planning Board approval of a PUD Master Plan requires the preparation and submittal of information to assist staff and third parties in the review and evaluation of the Woodmont Commons proposal before it is adopted as a PUD Master Plan. The *Londonderry Zoning Ordinance* recognizes that “Many of these items may be presented as approximations or preliminary estimates subject to

1.4.1 Documents and Relationships

change, where appropriate.” (*Section 2.8.9.2*). *Section 1.0 PUD Master Plan Information* includes some of the elements required by *Section 2.8 Planned Unit Development of the Londonderry Zoning Ordinance*. Specifically, this consists of these portions of the required submittals:

- Existing underlying zoning classification and classification of all adjoining lots (*Londonderry Zoning Ordinance Section 2.8.9.2.1*);
- Total acreage of the tract (*Londonderry Zoning Ordinance Section 2.8.9.2.3*).

Some of the required elements have been incorporated into the alternative regulations and standards for the PUD Master Plan, and are contained in *Section 2.0 PUD Regulations and Standards*. These components specifically include:

- Proposed total number of dwelling units and overall density for the tract;
- Proposed Conserved Green Space and Shared Open Space areas;
- Natural and cultural resources to be preserved;
- Proposed Perimeter Buffers, if appropriate, to adjoining property.

All other required elements have been prepared as approximations or preliminary estimates and are subject to change, and are not part of the regulations or standards that will be applied to the projects within the Woodmont Commons PUD. The PUD Master Plan documents providing this type of information are contained in *Section 1.0 PUD Master Plan Information* and *Section 4.0 Supplemental Documents*.

Thresholds, Mitigation and Improvement Requirements

In order to provide a properly planned development, projects may be eligible for Planning Board approval if they are within defined thresholds, provide appropriate mitigation of potential impacts, or are accompa-

nied by certain improvements. The PUD Master Plan incorporates such standards within the special regulatory framework for the Woodmont Commons PUD. All thresholds, mitigation and improvement requirements that will be integral to Planning Board project reviews and approvals are addressed and incorporated into *Section 2.0 PUD Regulations and Standards* and *Section 3.0 Mitigation and Improvement Requirements*.

PUD Regulations and Standards

The superseding regulations and standards that provide unique guidance for projects within the Woodmont Commons PUD have been assembled within a consolidated and coordinated portion of the PUD Master Plan, *Section 2.0 PUD Regulations and Standards*. Once approved, this Section will serve as the superseding regulatory framework for proposals requiring zoning approvals, PUD Subdivision approvals, or PUD Site Plan approvals within the Woodmont Commons PUD. This Section also describes the special administrative processes that will be used to ensure conformance of future projects with the PUD Master Plan.

PUD Subdivision Regulations

The *Londonderry Subdivision Regulations* provide for appropriate subdivision of land into individual parcels that are adequately served by streets or other infrastructure components. However, alternative subdivision regulations can be established within a PUD Master Plan that includes waivers of portions of the corresponding *Londonderry Subdivision Regulations*. The PUD Master Plan provides a number of special regulations and standards that will allow innovative subdivision of the land, streets and infrastructure; these alternative PUD Subdivision Regulations and Standards are contained in *Section 2.3 PUD Subdivision Regulations and Standards*. The specific list of Subdivision waivers is contained in *Section 2.1 Introduction*. If an existing provision of the *Londonderry*

1.4.1 Documents and Relationships

Subdivision Regulations is not waived in the PUD Master Plan, then future proposals will need to comply with the applicable provisions in effect on the date of the Woodmont Commons PUD Master Plan application (October 3, 2012) which was deemed complete by the Londonderry Planning Board.

PUD Site Plan Regulations

Multi-family and non-residential projects in Londonderry are subject to the *Londonderry Site Plan Regulations* and a related review and approval process. Similar to the special provisions associated with Zoning and subdivisions, a number of alternative regulations governing future PUD Site Plans within the Woodmont Commons PUD are contained within the PUD Master Plan (*Section 2.4 PUD Site Plan Regulations and Standards*). The specific list of Site Plan waivers is contained in *Section 2.1 Introduction* and are waived. If an existing provision of the *Londonderry Site Plan Regulations* is not waived in the PUD Master Plan, then future proposals will need to comply with the applicable Londonderry provisions in effect on the date of the completed Woodmont Commons PUD Master Plan application (October 3, 2012).

Supplemental Documents

Several supplemental documents have been assembled as appendices; each has a particular relationship to the PUD Master Plan and related approval processes.

- *Section 4.1 Updated Abutters List* – This is a complete list of abutters based on information obtained prior to the final submittal of this PUD Master Plan for Planning Board approval; provision of this list fulfills a formal requirement of the *Londonderry Zoning Ordinance*;
- *Section 4.2 Master Plan Traffic Impact Assessment (MTIA)* – This is a comprehensive technical evaluation of potential transportation impacts based on estimates and approximations of development scenarios. This analysis is part of the

PUD Master Plan Information required by the *Londonderry Zoning Ordinance*. The findings of the MTIA form the basis for various thresholds, mitigation standards or improvement requirements associated with certain types of projects and approvals that are described in relevant portions of *Section 2.0 PUD Regulations and Standards*;

- *Section 4.3 Infrastructure Memorandum* – This is technical information regarding future utility and stormwater management infrastructure based on estimates and approximations of development scenarios. It has been provided as PUD Master Plan Information fulfilling the submission requirements (*Londonderry Zoning Ordinance, Sections 2.8.9.2.14 to 2.8.9.2.17*). Related thresholds, mitigation or improvement requirements that are incorporated into subsequent Planning Board project review and approval processes are provided within *Section 2.0 PUD Regulations and Standards*;
- *Section 4.4 Master Fiscal Impact Analysis (MFIA)* – This section contains information provided at the request of the Planning Board as part of their review of the PUD Master Plan. It evaluates the prospective fiscal impact upon the Town of Londonderry based on estimates and approximations of development scenarios. Related thresholds, mitigation or improvement requirements that will be incorporated into subsequent Planning Board project review and approval processes are provided within *Section 2.0 PUD Regulations and Standards*;
- *Section 4.5 Development Agreement* – Some of the actions, mitigation or improvements associated with implementing the PUD Master Plan are subject to a specific agreement between the Town of Londonderry and the proponents of Londonderry Common, rather than through the regulatory framework and associated approvals that are addressed within the PUD Master Plan documents. The separate Develop-

1.4.1 Documents and Relationships

ment Agreement is included for information and reference as a supplemental document;

- Section 4.6 Sample Restrictive Covenant – This Section includes a form of restrictive covenant, to be executed and recorded by each of the Developers;
- Section 4.7 Sample Forms – This Section contains two sample forms that will be used during the PUD Subdivision and PUD Site Plan application processes;
- Section 4.8 Londonderry Zoning Ordinance, as Amended Through December 2011 – This Section contains the text of the *Londonderry Zoning Ordinance, as Amended Through December 2011* and is the version of the *Londonderry Zoning Ordinance* referred to throughout this PUD Master Plan;
- Section 4.9 Subdivision Regulations, June 10, 2009 – This Section contains the text of the *Subdivision Regulations, June 10, 2009* and is the version of the *Londonderry Subdivision Regulations* referred to throughout this PUD Master Plan;
- Section 4.10 Site Plan Regulations, June 10, 2009 – This Section contains the text of the *Site Plan Regulations, June 10, 2009* and is the version of the *Londonderry Site Plan Regulations* referred to throughout this PUD Master Plan;
- Section 4.11 Typical Details for Site and Roadway Infrastructure, May 2009 – This Section contains the text of the *Typical Details for Site and Roadway Infrastructure, May 2009* and is the version of the *Typical Details for Site and Roadway Infrastructure* referred to throughout this PUD Master Plan.



2.0 PUD Regulations and Standards

WOODMONT COMMONS

PLANNED UNIT DEVELOPMENT MASTER PLAN

JULY 2013

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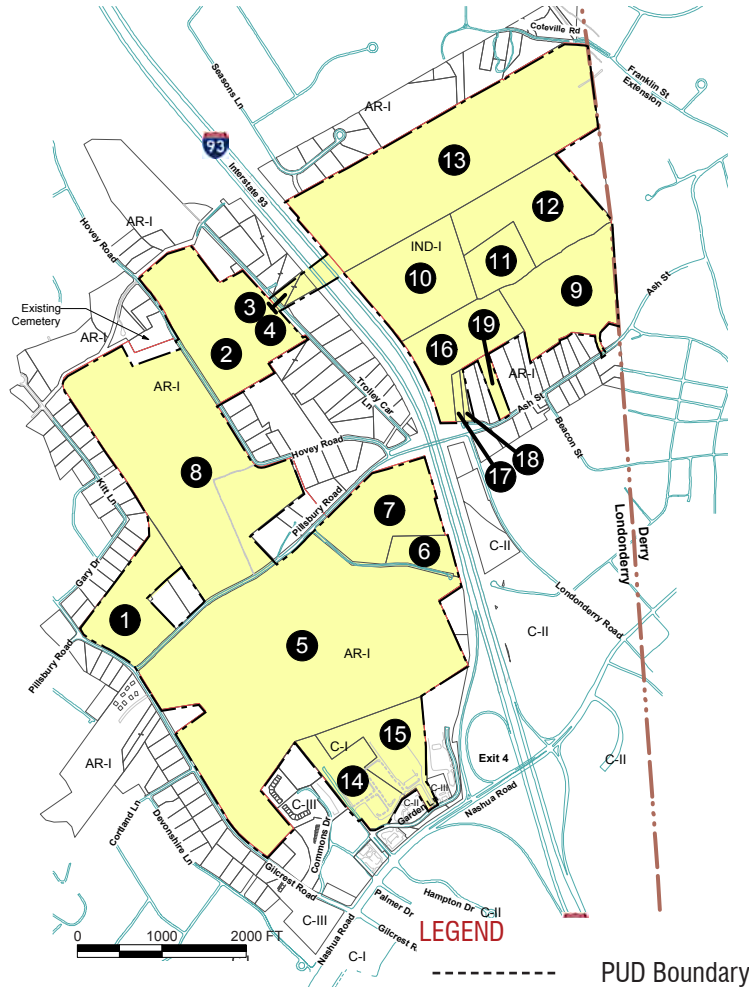
2.1 INTRODUCTION

2.1.1 Purpose of the PUD Regulations and Standards

Section 2.0 PUD Regulations and Standards contains all of the components of PUD Master Plan that shall be used as the criteria to guide all project proposals, reviews and approvals within the Woodmont Commons PUD for which the Planning Board is the regulatory authority for Town of Londonderry. These Regulations and Standards specifically apply to all projects that would otherwise be subject to Planning Board review and approval in implementing the *Londonderry Zoning Ordinance*, the *Londonderry Subdivision Regulations* and the *Londonderry Site Plan Regulations*.

These PUD Regulations and Standards replace or augment all of the Town's regulations and standards that have been waived through the adoption of the PUD Master Plan. If the existing Town regulations and standards subject to Planning Board approval as of October 3, 2012 have not been waived, they will apply as criteria for applicable project reviews and approvals, in concert with the special PUD Regulations and Standards.

2.1.2 Map and Boundary Description



Woodmont Commons Property List

| Lot | Parcel ID | Acres | Underlying Zoning | West | East | Lot |
|-----|------------|---------|---------------------------------|---------|---------|-----|
| 1 | 010 015 0 | 24.000 | Agricultural-Residential (AR-I) | 24.000 | | 1 |
| 2 | 010 023 0 | 38.000 | Agricultural-Residential (AR-I) | 38.000 | | 2 |
| 3 | 010 029 2A | 1.187 | Agricultural-Residential (AR-I) | 1.187 | | 3 |
| 4 | 010 029 2B | 1.187 | Agricultural-Residential (AR-I) | 1.187 | | 4 |
| 5 | 010 041 0 | 162.003 | Agricultural-Residential (AR-I) | 162.003 | | 5 |
| 6 | 010 041 1 | 5.000 | Agricultural-Residential (AR-I) | 5.000 | | 6 |
| 7 | 010 041 2 | 25.190 | Agricultural-Residential (AR-I) | 25.190 | | 7 |
| 8 | 010 042 0 | 96.980 | Agricultural-Residential (AR-I) | 96.980 | | 8 |
| 9 | 010 045 0 | 28.000 | Agricultural-Residential (AR-I) | | 28.000 | 9 |
| 10 | 010 046 0 | 29.000 | Industrial I (IND-I) | | 29.000 | 10 |
| 11 | 010 047 0 | 9.000 | Industrial I (IND-I) | | 9.000 | 11 |
| 12 | 010 048 0 | 29.000 | Industrial I (IND-I) | | 29.000 | 12 |
| 13 | 010 050 0 | 96.000 | Industrial I (IND-I) | | 96.000 | 13 |
| 14 | 010 052 0 | 14.650 | Commercial I (C-I) | 14.650 | | 14 |
| 15 | 010 052 1 | 18.640 | Commercial I (C-I) | 18.640 | | 15 |
| 16 | 010 057 0 | 20.480 | Industrial I (IND-I) | | 20.480 | 16 |
| 17 | 010 058 0 | 1.300 | Agricultural-Residential (AR-I) | | 1.300 | 17 |
| 18 | 010 059 0 | 1.060 | Agricultural-Residential (AR-I) | | 1.060 | 18 |
| 19 | 010 062 0 | 2.720 | Agricultural-Residential (AR-I) | | 2.720 | 19 |
| | | 603.397 | | 386.837 | 216.560 | |

Source: Londonderry GIS

2.1.3 Planning and Design Principles

The PUD Master Plan has been prepared to fulfill specific community and public interests associated with the *Londonderry Zoning Ordinance* and the relevant plans and policies that contributed to the preparation and adoption of *Section 2.8 Planned Unit Development* and of the PUD Master Plan. The following Planning and Design Principles are intended to provide guidance and be employed as supplemental criteria for two circumstances:

- Interpretation of PUD Regulations and Standards – In some cases, interpretation of specific PUD Regulations and Standards may be required based on the unusual circumstances of a specific project proposal. In such cases, the Planning and Design Principles can be employed by the Planning Board as a guidance document.
- Review and Approval of Compliance Alternatives – In some cases, the underlying public purposes may be accomplished as well or better through alternative design and development approaches that have not been envisioned with the range of PUD Regulations and Standards within this document. As a result, applicable portions of the PUD Regulations and Standards may also be implemented through Compliance Alternatives that are agreed to by both the Planning Board and the project proponents. In such cases, the Planning Board shall use the following Planning and Design Principles as the criteria for determining that the Compliance Alternative achieves the underlying public purposes for any specific Regulation or Standard.

The applicable Planning and Design Criteria address the following:

- Aesthetically attractive features – The features of the PUD Subdivision and/or PUD Site Plan will enhance the aesthetics of the Woodmont Commons PUD as viewed from publicly accessible roads, streets, paths or open space within

or adjacent to the PUD.

- Connected, walkable neighborhoods – The characteristics of the improvements or development will contribute to a walkable community that is linked to other portions of the Woodmont Commons PUD or publicly-accessible pedestrian routes adjacent to the PUD.
- Quality site design – The site design will provide public benefits in terms of sustainable use of resources, visual quality from public vantage points, and complementary relationships to adjacent land or development.
- Quality architectural design – Design that will follow Architectural Standards and feature complementary relationships to adjacent land or development as viewed from publicly accessible roads, streets, paths or open space within or adjacent to the PUD.
- Potential impacts and mitigation needs – The proposed PUD Subdivision and/or PUD Site Plan will either have no a material effect on potential municipal impacts or will reduce such impacts. Associated mitigation requirements will employ the PUD Master Plan Regulations and Standards.

2.1.4 Applicability

The PUD Regulations and Standards shall be applied to any proposed change in land use or improvements that would otherwise be subject to compliance or approval by the Town under the *Londonderry Zoning Ordinance*, *Subdivision Regulations* and *Site Plan Regulations* effective on October 3, 2012.

Waivers/Modifications/Compliance with Specified Development Standards

Consistent with the purposes of a Planned Unit Development (PUD) as expressed in *Section 2.8.2* of the *Londonderry Zoning Ordinance*, including allowing “a landowner to propose his/her own development project largely independent from current land use regulations otherwise applicable to that property,” and to otherwise fulfill the objective of creating a “special zoning district ... in terms of uses, dimensions, other development standards,” the PUD Master Plan waives and modifies, pursuant to *Section 2.8.7.4.1 and 2.8.7.4.2*, certain of the existing development standards contained within the *Londonderry Zoning Ordinance*, the *Londonderry Site Plan Regulations* (LSPR), and the *Londonderry Subdivision Regulations* (LSR). To the extent that a development standard is not specifically waived and modified, or the applicable standard is not otherwise determinable, the most appropriate existing and underlying standard shall be applied as determined by the Planning Board. See *Section 2.8.10.2*. Following approval of the PUD, and with respect to the Planning Board’s receipt and review of any PUD Site Plan or PUD Subdivision application concerning the development of lands within the PUD, the Planning Board shall evaluate conformity with the PUD Master Plan, as the same may be amended from time to time.

Application of the following development standards of the *Londonderry Zoning Ordinance* are hereby waived in favor of the alternative design standards contained within the approved PUD Master Plan:

| <i>Londonderry Zoning Ordinance</i> Section | Explanation of Waiver or Modification |
|--|--|
| Section 1.2 – Impact Fees | Impact fees chargeable for site-specific development will be calculated during PUD Site Plan and/or PUD Subdivision reviews in accord with <i>Section 4.5 Development Agreement</i> . |
| Section 1.3 – Residential Development Phasing | Phasing of residential units shall not be limited by the express calculation of <i>Section 1.3.3</i> . |
| Section 1.5.2 – Conditional Use Permits | Approval of the PUD Master Plan, and its process for amendments to the PUD, supersedes the need for conditional use permits or zoning variances and special exceptions. See <i>Londonderry Zoning Ordinance Section 2.8.3.9</i> ; see also PUD Master Plan amendment procedure in <i>Section 2.5 Administration</i> . The exception to this waiver is that a Conditional Use Permit shall be required prior to the disruption of prime wetlands or prime wetland buffers (other than improvements such as boat ramps, beaches and docks that are designed to make wetland features more readily accessible for use and which shall be reviewed by the Planning Board during PUD Site Plan review). |

2.1.4 Applicability

| | |
|--|---|
| Section 2 – Zoning Districts/Use Table | <p>Within the PUD Master Plan, proposed zoning districts and dimensional standards, along with the uses permitted within each development Subarea, shall, in the first instance, be defined by the terms of the PUD Master Plan, including the Land Use Plan, and associated regulations, standards, tables, illustrations and alternative design standards. Compliance with the PUD Master Plan including <i>Section 2.3 PUD Subdivision Regulations and Standards</i> and <i>Section 2.4 PUD Site Plan Regulations and Standards</i> shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review. In the event of a conflict on roadway dimensional standards between the PUD Master Plan and the Town of Londonderry's <i>Typical Details for Site and Roadway Infrastructure</i>, the PUD Master Plan shall control. Lastly, consistent with <i>Londonderry Zoning Ordinance 2.8.6.2</i>, "uses that are permitted in the underlying zoning district ... shall be considered permitted uses in a PUD" and may be proposed for site development notwithstanding the approval of a PUD. The Town is not obligated to accept as public such roadways as are not built to applicable Town standards. The addition of a sidewalk on one or both sides of the roadway, however, does not automatically disqualify a roadway from public acceptance provided the Developer arranges for the maintenance of said sidewalks.</p> |
| 2.6.3 Conservation Overlay Districts | <p>Approval of the comprehensive PUD Master Plan supersedes the need for conditional use permits. The exception to this waiver is that a Conditional Use Permit shall be required prior to the disruption of prime wetlands or prime wetland buffers (other than improvements such as boat ramps, beaches and docks that are designed to make wetland features more readily accessible for use and which shall be reviewed by the Planning Board during PUD Site Plan review).</p> |

2.1.4 Applicability

| | |
|---|--|
| Section 3.1.2 – Local Excavation Standards | Excavation is not a proposed land use within the PUD. Thus, any excavation activities within the PUD, that furthers the purposes of the PUD or prepares the land within the PUD for development along public infrastructure, shall be deemed “exclusively incidental to the construction or alteration of a building or structure or the construction or alteration of a parking lot or way” and exempt from local permitting pursuant to RSA 155-E:2-a(I)(a). The Developer will stabilize or reclaim any excavated areas not immediately developed or redeveloped pursuant to state minimum reclamation standards. |
| Section 3.2 – Performance Standards | Pursuant to <i>Londonderry Zoning Ordinance Section 2.8.7.5</i> , a fifty foot (50’) Perimeter Buffer will be maintained around the exterior PUD tract boundaries as indicated on the Land Use Plan. Within the PUD, performance standards shall be determined by the terms of the PUD Master Plan. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review. |
| Section 3.3.3.1.1 – Conservation Subdivisions | Regardless of lot size, conservation subdivisions shall not be mandatory within the PUD. Instead, conservation subdivisions shall remain “available” as an option for applicants pursuant to <i>Section 3.3.3.1.1</i> and as otherwise consistent with the terms of the PUD Master Plan. If pursued, Compliance with conservation subdivision technical requirements shall be evaluated by the Planning Board during PUD Subdivision review. |
| Section 3.10 – Vehicle Access and Parking | Vehicle and bicycle access, parking space standards and parking loading shall be determined by the terms of <i>Section 2.3.4 Parking and Loading Standards</i> of the PUD Master Plan. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review. |
| Section 3.11 – Signs | Signage standards for commercial and mixed use areas shall be determined by the terms of <i>Section 2.3.7 PUD Subdivision Signage</i> and <i>Section 2.4.4 PUD Site Plan Signage</i> of the PUD Master Plan. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review. For residential areas, signage shall remain subject to <i>Londonderry Zoning Ordinance Section 3.11</i> . |

2.1.4 Applicability

| | |
|--|--|
| Section 3.12.1.1 – Home Occupations | Home occupations shall be permitted in accord with the terms of the PUD Master Plan <i>Section 2.2 Land Use Regulations</i> and without the need for a special exception. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review. |
| Section 3.12.2.1/3.12.3.1 – Child Care/Adult Day Care Facilities as Home Occupations | Child care and adult care facilities as home occupations shall be permitted in accordance with the terms of the PUD Master Plan <i>Section 2.2 Land Use Regulations</i> and without the need for a special exception. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review. |
| Section 3.14.1.2/3.14.1.3 – Fences | Private fences may be erected within rights-of-way, provided the easement is of sufficient width to allow safe placement of the fence. Similarly, sight distance considerations relating to intersection safety shall not prohibit the location of reasonable fencing in the limited context of signalized intersections. |
| Section 4.7 – Definitions | To the extent the PUD Master Plan contains different definitions for terms used in both the <i>Londonderry Zoning Ordinance</i> and the PUD, the definition stated in <i>Section 2.1.5 Definitions</i> of the PUD Master Plan shall prevail. |
| Appendix A – Roadway Classifications | To the extent conflicts arise in relation to definitions and road classifications, <i>Section 2.3.3 Transportation Network</i> shall control. |
| Application of the following development standards of the LSPR are hereby waived in favor of the alternative design standards contained within the approved PUD Master Plan: | |
| LSPR Section | Explanation of Waiver or Modification |
| Section 1.03(d) – Purpose: Sensitivity to Rural and Agricultural Heritage | The Woodmont Commons PUD is specially designed with perimeter uses that are sensitive to and consistent with the character and use of exterior abutting properties. Within the PUD, however, the development themes are neither rural nor agricultural in nature, but express a commonality of theme consistent with the objectives of a planned unit development. |
| Section 3.01(a) – Approval of Improvements | Improvements will be constructed in accord with the PUD Master Plan, the <i>Londonderry Zoning Ordinance</i> , the LSPR and the LSR, consistent with the waivers and modifications approved by the Planning Board. |
| Section 3.01(c) – Standards and Specifications | Improvements will be constructed in accord with the PUD Master Plan, the <i>Londonderry Zoning Ordinance</i> , the LSPR and the LSR, consistent with the waivers and modifications approved by the Planning Board. |

2.1.4 Applicability

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| Section 3.02 – Monuments | All lot boundaries shall be readily determinable within the PUD Master Plan using a computer-aided drafting system (CAD). Conventional monumentation of every boundary shall not be required where not practical. Alternatives to conventional monumentation, such as GIS-based documentation or the use of plaques or other markers, may be used. Given the application of alternative design standards within the PUD, boundaries may be obstructed by the placement of structures, roads and other improvements. |
| Section 3.04 – Utilities | Within the PUD, proposed utilities shall generally be located underground in accord with <i>Section 3.04</i> of the <i>Londonderry Zoning Ordinance</i> but may be located overhead when the utility determines it cannot reasonably go underground or where now existing along proposed private roads and driveways. Utilities shall not conflict with, but may be located underneath drainage systems and within appropriate easements. |
| Section 3.07(a)(1) – Storm Drain System | Underground detention structures shall be permitted, provided access and maintenance is arranged. |
| Section 3.08 – Streets/Driveways/Sidewalks | To the extent conflicts arise, <i>Section 2.3.3 Transportation Network</i> of the PUD Master Plan shall control. |
| Section 3.09 – Landscaping Design Standards | To the extent conflicts arise, <i>Section 2.3.9 PUD Subdivision Landscape</i> of the PUD Master Plan shall control. |
| Section 3.11 – Parking Lot Design Standards Except Section 3.11(d) | As required by the <i>Londonderry Zoning Ordinance</i> , the PUD Master Plan contains specific and alternative design standards relating to parking capacities, layout and function. |
| Section 3.12 – Building and General Appearance Design Standards | The PUD Master Plan includes architectural guidelines under <i>Section 2.4.2 Buildings and Lots</i> for buildings proposed within the Woodmont Commons PUD. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Site Plan review. |
| Section 3.13 – Outdoor Lighting Design Standards | To the extent conflicts arise, <i>Section 2.3.8 PUD Subdivision Lighting</i> of the PUD Master Plan shall control. |
| Section 3.14/4.17 – Traffic Impact Analysis | <i>Section 3.1 Transportation</i> of the PUD Master Plan contains specific and alternative design standards relating to traffic impact analysis. |
| Section 4.14(d) – Improvement Plans/Streets | To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control. |

2.1.4 Applicability

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| Section 4.15 – Building Renderings | The process for submitting building renderings shall be observed during the PUD Site Plan approval process, but substantive evaluation shall compare the conceptual renderings to the alternative architectural design standards incorporated within the PUD Master Plan under <i>Section 2.4.2 Buildings and Lots</i> and not LSPR <i>Section 3.12</i> . |
| Section 5.01 – Commercial Wireless Communication Facilities | <i>Section 5.01</i> shall apply to new freestanding wireless communication facilities proposed within the PUD, but shall not apply to wireless facilities contained and concealed within a clock tower, belfry or similar building feature designed to conceal the facility from public view. |
| Application of the following development standards of the LSR are hereby waived in favor of the alternative design standards contained within the approved PUD Master Plan: | |
| LSR Section | Explanation of Waiver or Modification |
| Section 3.01(c) – Standards and Specifications | Improvements will be constructed in accord with the PUD Master Plan, the <i>Londonderry Zoning Ordinance</i> , the LSPR and the LSR, consistent with the waivers and modifications approved by the Planning Board. |
| 3.02 – Monuments | All lot boundaries shall be readily determinable within the PUD Master Plan using a computer-aided drafting system (CAD). Conventional monumentation of every boundary shall not be required where not practical. Alternatives to conventional monumentation, such as GIS-based documentation or the use of plaques or other markers, may be used. Given the application of alternative design standards within the PUD, boundaries may be obstructed by the placement of structures, roads and other improvements. |
| Section 3.03 – Lots | As required by the <i>Londonderry Zoning Ordinance</i> , the PUD Master Plan contains specific and alternative design standards relating to lot dimensions and building setback lines. The subdivision of perimeter parcels shall comply with <i>Section 3.03(a) and (b)</i> of the <i>Londonderry Zoning Ordinance</i> . |
| Section 3.05 – Utilities | Proposed utilities shall generally be located underground in accordance with <i>Section 3.05</i> of the <i>Londonderry Zoning Ordinance</i> but may be located overhead when the utility determines it cannot reasonably go underground or where now existing along proposed private roads and driveways. Utilities shall not conflict with, but may be located underneath, drainage systems and within appropriate easements. |

2.1.4 Applicability

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| Section 3.09(A) – Streets/General | To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control. To the extent historic stonewalls exist and are intact along the PUD perimeter boundary, such features shall be preserved. |
| Section 3.09(B) – Streets/Access | Within the PUD, newly created lots must have safe, direct frontage and access to an existing roadway or a road proposed in accord with the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> . Proposed streets may be accepted as public ways or held privately with suitable maintenance and joint-use arrangements approved by the Planning Board. To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control. |
| Section 3.09(D) – Streets/Arrangement | Within the PUD, newly created lots must have safe, independent and direct frontage and access to a public space, an existing roadway or a road proposed in accord with the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> . Proposed streets may be arranged in such manner as to connect to private ways, provided suitable maintenance and joint-use arrangements and cross access agreements are approved by the Planning Board. To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control. |
| Section 3.09(E) – Streets/ Classification | To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control. The Town is not obligated to accept as public such roadways as are not built to applicable Town standards. The addition of a sidewalk on one or both sides of the roadway, however, does not automatically disqualify a roadway from public acceptance provided the Developer arranges for the maintenance of said sidewalks. |
| Section 3.09(F, G, I, P, R (incl. Table 1, Pg. 22), S (incl. Tables 2 and 3, Pg., 23)) – Streets/Driveways, Sidewalks, Curbs, Guardrail, Roadway Design Standards, Intersections | To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control. |
| Section 3.10/4.17 – High Intensity Soil Study (HISS) | Since the entire PUD will be serviced by water and sewer, high intensity soil study is not relevant. |

2.1.5 PUD Definitions

Definitions are for the purposes of the PUD Master Plan only. The intent is to define those words and phrases that are either not defined in the *Londonderry Zoning Ordinance* or where the PUD Master Plan requires a variation or clarification of meaning.

The general definitions of the *Londonderry Zoning Ordinance* shall pertain to the PUD Master Plan unless otherwise defined herein. Undefined words and phrases within the PUD Master Plan or the *Londonderry Zoning Ordinance* shall have ordinary dictionary meanings.

APPLICANT/PRIMARY APPLICANT: Pillsbury Realty Development, LLC (“Pillsbury”), on behalf of the Developers, serves as a the Primary Applicant and principal liaison to the Town of Londonderry with respect to planning and land use approvals.

ARTISAN USE: A use that involves a worker or craftsman who practices a trade or handicraft, or one that produces something (as cheese, beer or wine) in limited quantities often using traditional methods.

BLOCK: The aggregate of public frontage, private lots and access lanes.

BLOCK FACE: The dimension of a single block face that includes the block and public frontages as measured along the curb from back of curb to back of curb or back of curb to Recreational Trail or PUD property line.

BLOCK PERIMETER: The distance around the perimeter of a block, measured at the curb.

BOUTIQUE HOTEL: Any building containing up to ten guest rooms which are used, rented, or hired for sleeping purposes by transient guests and with access to units primarily from interior lobbies, courts, or halls.

BUILDING, HEIGHT OF: The vertical distance from the average grade adjoining the walls of the structure to the average of the highest points of a roof surface of a flat roof ; and to the deck line of a mansard roof; and the mean height level between eaves and ridge for a gable, hip or gambrel roof, but does not include structures not intended for human occupancy such as chimneys, water towers, steeples, etc.

BUSINESS CENTER DEVELOPMENT: A tract of land, buildings or structures for business activities planned as a whole and intended to include those uses allowed in the Woodmont Commons PUD whether built at one time as a unit or in two or more development stages.

CONFERENCE/CONVENTION CENTER: A facility designed and used for conventions, conferences, seminars, product displays, recreation activities and entertainment functions, along with accessory functions including temporary outdoor displays, and food and beverage preparation and service for on-premise consumption. The facility may include hotel functions.

COTTAGE COURT: Residential lots oriented onto a common open space with sidewalk connections to a Public Frontage.

CULTURAL USES AND PERFORMING ARTS: Facilities, both indoor and outdoor, for public assembly and group entertainment including: civic theaters and facilities for “live” theater and concerts, motion picture theaters, public and semi public auditoriums and similar public assembly uses.

2.1.5 PUD Definitions

DAY CARE CENTER, ADULT: An Adult Day Care facility in which more than 12 clients receive care, maintenance, and supervision by someone other than a relative or legal guardian for less than 24 hours per day, which has been licensed by a state or county licensing agency.

DEDICATED OFFICE: A freestanding single-use office structure containing business offices as its principal use, with over 10,000 gross square feet in building area that may only be located along the Interstate Highway 93 corridor and within 1,000 feet of the PUD boundary line.

DEVELOPER: Pillsbury Realty Development, LLC (“Pillsbury”).

DRIVE-THRU WINDOWS: A customer service facility designed for the convenience of the motoring public accessory to an office or retail establishment which is intended to enable the customer to transact business with a person located within a structure or a machine without exiting the motor vehicle.

DWELLING UNIT, ACCESSORY: A dwelling unit that is incidental to, subordinate to, and customarily found in connection with a single family dwelling and which is situated on the same lot as the single family dwelling. See *Section 2.4.2 Buildings and Lots*.

DWELLING, SINGLE-FAMILY DETACHED: A detached or free-standing residence other than a mobile home, designed for and occupied by one family only.

DWELLING, SINGLE-FAMILY ATTACHED: An attached residence, designed for and occupied by one family only.

EDUCATION AND TRAINING FACILITIES: Public or private (for profit or nonprofit) educational institutions offering instruction in the following: art, ballet and other dance, computers and electronics, drama, driver education, language, music, and other similar activities. This includes business, secretarial, and vocational schools, establishments providing courses by mail, professional schools (law, medicine, etc.), and seminaries/religious ministry training facilities. Also includes facilities, institutions and conference centers that offer specialized programs in personal growth and development, such as fitness, environmental awareness, arts, communications and management.

FLEX USE: Artisan Use or Light Industrial Use in combination with retail and office uses related to the business.

GALLERY: An attached flat roof projecting from a building over a sidewalk or passage that is partly open to create a portico or colonnade.

GREEN SPACE, CONSERVED: Land that is reserved or dedicated and retains or accommodates predominately natural features that may have public access to the extent consistent with the conservation goals and applicable regulations, and as specified in the requirements within the PUD Master Plan. This may include regulated open space such as designated wetland areas subject to state and federal regulations, areas that provide shared stormwater drainage and water quality features serving multiple Subareas, and intermittent or perennial streams or ponds. No buildings are allowed on the site except Accessory Structures as defined in the *Londonderry Zoning Ordinance* that store maintenance equipment. See *Section 2.2.3 Land Use Standards* and *Section 2.3.6*

2.1.5 PUD Definitions

Conserved Green Space and Shared Open Space Standards.

GROUP CHILD CARE CENTERS: Either a full day or half day child care facility (whether or not the facility is known as day nursery, nursery school, kindergarten, etc.) by which services are regularly provided for any part of a day, but less than twenty-four (24) hours to thirteen (13) or more children.

HOME OCCUPATION: an occupation, profession, activity or use that is clearly a customary, secondary, and incidental use of a residential dwelling unit.

HOTEL: Any building containing eleven or more guest rooms which are used, rented, or hired for sleeping purposes by transient guests and with access to units primarily from interior lobbies, courts, or halls.

INDUSTRIAL USE, LIGHT: A use that manufactures or assembles small or lightweight articles; or items that use moderate amounts of partially processed materials to produce items of relatively high value per unit weight.

INFORMATION PLAN: One of many conceptual plans that could be built according to the regulations, standards and guidelines in this PUD Master Plan.

LAND USE PLAN: A framework of key elements required by *Section 2.8 Planned Unit Development* of the *Londonderry Zoning Ordinance*.

LAND OWNERS: Pillsbury Realty Development, LLC (“Pillsbury”), Hyrax Derry Partners, LLC, Demoulas Supermarkets, Inc., and Robert D. and Stephen R. Lievens (collectively, the “Developers”)

LONDONDERRY SITE PLAN REGULATIONS: The *Site Plan Regulations, June 10, 2009*. This document is provided in *Section 4.10 Site Plan Regulations, June 10, 2009*.

LONDONDERRY SUBDIVISION REGULATIONS: The *Subdivision Regulations, June 10, 2009*. This document is provided in *Section 4.9 Subdivision Regulations, June 10, 2009*.

LONDONDERRY ZONING ORDINANCE: The *Londonderry Zoning Ordinance, as Amended Through December 2011*. This document is provided in *Section 4.8 Londonderry Zoning Ordinance, as Amended Through December 2011*.

MASTER PLAN TRANSPORTATION IMPACT ASSESSMENT: The traffic analysis conducted for Pillsbury Realty Development, LLC for the Woodmont Commons property by TEC as dated February 6, 2013 and included as a supplemental document in *Section 4.2 Master Plan Transportation Impact Assessment*. (Also referred to as the MTIA)

MIXED USE: Two or more uses contained within the same building or on the same lot.

OPEN SPACE, SHARED: Land that is held or restricted for common benefit and as categorized within the regulations and requirements of the PUD Master Plan, regardless of the ownership of the land. May include pervious or impervious surfaces. See *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*. Shared Open Space includes Civic Open Space, Active Open Space and Passive Open Space. Shared Open Space may include Agricultural Open Space. These categories of open space are described in *Section 2.2.3 Land*

2.1.5 PUD Definitions

Use Standards and Section 2.3.6 Conserved Green Space and Shared Open Space Standards.

PARKING STRUCTURE: A building, land, or portion thereof designed or used for the temporary storage of motor-driven vehicles with one or more stories above or below grade, and which shall not be construed as retail space. Also known as a parking deck.

PERIMETER BUFFERS: A buffer that is reserved or dedicated on a common or individual lot along designated perimeters of the Woodmont Commons PUD as indicated on the Land Use Plan. Streets and structures shall be set back a minimum of fifty (50) feet from the exterior PUD tract boundary that abuts residential uses except where transportation or use linkages are desired. Landscaping and other screening devices, such as topography, berms, fences and walls, shall be designed to provide a buffer between the PUD and adjoining property except where compatible uses adjoin one another. Where a buffer is also part of a project, it may also be used to satisfy applicable PUD Subdivision and/or PUD Site Plan Regulations and Standards. May include pervious or impervious surfaces. See *Section 2.3.5 PUD Perimeter Buffers*.

PRIMARY STREET: Primary Streets for Woodmont Commons include both existing and new streets as identified for on-site and off-site improvements in the Master Plan Impact Assessment dated February 6, 2013 and as identified on the Land Use Plan.

PRIVATE FRONTAGE: A Private Frontage is defined as the area between the lot (or property) line and the building façade.

PROFESSIONAL OFFICE: A use within a mixed-use structure or a single-use structure where business or professional services are conducted or rendered.

PUBLIC ACCESS: The ability of the general public to access and use Conserved Green Space and Shared Open Space and their amenities or community facilities designed and made available for this purpose. Public access may be reasonably restricted in terms of times of day or in terms of the activities that are permitted in the interest of security, environmental sensitivity, compatibility with adjacent or nearby uses, and compatibility with access and use of the space by others. Public access may be limited to pedestrian uses and may reasonably restrict the use of motorized vehicles.

PUBLIC FACILITIES: Any facility including but not limited to buildings, property, recreation areas, and roads, which are leased or otherwise operated or funded by a governmental body.

PUBLIC FRONTAGE: A Public Frontage is the area from the curb to the lot (or property) line.

PUBLIC REALM: Any publicly owned streets, sidewalks, pathways, rights-of-way, parks, publicly accessible open spaces (whether Conserved Green Space or Shared Open Space) and any public and civic building and facilities.

PUBLIC UTILITIES: All lines and facilities related to the provision, distribution, collection, transmission, or disposal of water, storm and sanitary sewage, oil, gas, power, information, telecommunication and telephone cable, and includes facilities for the generation of electricity.

PUBLIC UTILITIES STRUCTURE: An electricity or gas substation, water or wastewater pumping station, telephone repeater station, water storage tank, reservoir, recycling collection center, cellular and telephone transmission facilities, or similar structure used as an intermediary switching, boosting, distribution, or transfer station for electricity water, wastewater, cable television, or telephone services between the point of generation and the end user, or a wastewater treatment plant, but not including satellite dish antennas, facilities for the handling of solid waste (except for recycling collection centers), or radio, television, or microwave transmission or relay towers.

RELIGIOUS FACILITIES: a building or buildings used for public worship by a congregation. Religious facilities include churches, synagogues, temples, mosques, and other places of religious worship.

RENTAL CAR SATELLITE OFFICE: Vehicle leasing offices and associated automobile parking.

RESTAURANT: a structure in which the principal use is the preparation and sale of food and beverages to the public on demand from a menu during stated business hours, to be consumed on the premises primarily inside the building. Includes cafes, taverns, and similar establishments but does not include a drive-thru establishment.

RESTAURANT, FAST FOOD: an establishment whose primary business is serving food to the public for consumption on or off the premises by order from and service to vehicular passengers outside the structure.

SECONDARY STREET: Secondary Streets for Woodmont Commons include new streets as identified on the Land Use Plan and as allowed by *Section 2.2.1 Description*.

SETBACK: The area of a lot measured from the lot line to a building façade that is maintained clear of permanent structures, with the exception of permitted encroachments listed in *Section 2.4.2 Buildings and Lots* (Var: build-to-line.)

SUPPLEMENTAL TRAFFIC IMPACT ANALYSIS: Also known as Supplemental TIA. A traffic analysis that may be prepared in conjunction with a PUD Subdivision and/or Site Plan submittal. A Supplemental TIA should be compared to the MTIA provided in *Section 4.2 Master Plan Traffic Impact Assessment*.

TYPICAL DETAILS FOR SITE AND ROADWAY INFRASTRUCTURE – The *Typical Details for Site and Roadway Infrastructure, May 2009* and is the version of the *Typical Details for Site and Roadway Infrastructure* referred to throughout this PUD Master Plan. This document is provided in *Section 4.11 Typical Details for Site and Roadway Infrastructure, May 2009*.

YIELD STREET: A street where a vehicle traveling in one direction slows or stops to allow a vehicle traveling in the opposite direction to pass.

2.2 LAND USE REGULATIONS

2.2.1 Description

The Land Use Plan depicts a framework incorporating the key elements that are inherent within the Concept Plan, including but not limited to:

- Subareas
- Primary Street Network and Secondary Streets
- Conserved Green Space (including regulated environmental areas)
- Open Space
- Stormwater Detention Areas

The Land Use Plan is purposefully flexible with the expressed understanding that PUD Subdivision Plans and PUD Site Plans prepared in accordance with the PUD Master Plan will vary and change according to the following subsections.

Subareas

The lines depicting the Subareas are approximate in location, and may move accordingly:

- The interior boundaries of the Subarea lines in WC-5, WC-6, WC-7, WC-9 and WC-11 may move inward by up to 100 feet;
- The Woodmont Commons PUD's interior Subarea boundaries may move inward or outward by +/- 100 feet;
- Uses common to Subareas that are adjacent may have blocks, lots, buildings, streets, open space and other features that cross over Subarea lines so long as the Use follows the regulations and Standards of the most restrictive Subarea;
- Uses exclusive to a Subarea must be located entirely within the Subarea.

Primary Street Network and Secondary Streets

Two types of streets are depicted on the Land Use Plan in *Section 2.2.2 Land Use Plan*, are approximate in their locations and may move accordingly:

- The Primary Street Network is shown in BOLD black lines on the Land Use Plan in *Section 2.2.2 Land Use Plan*. These streets are required streets in terms of vehicular function. While their actual location may vary, the intent as shown in *Section 4.2 Master Plan Traffic Impact Assessment (MTIA)* must be preserved within the PUD Subdivision and PUD Site Plan(s). The actual design of the Primary Streets shall be in accordance with the MTIA or Supplemental TIA, and shall be as selected from the street standards in *Section 2.3.3 Transportation Network* in accordance with the uses for the Block or Subarea.
- Secondary Streets are shown in MEDIUM black lines on the Land Use Plan in *Section 2.2.2 Land Use Plan*. These streets are shown for information purposes only in order to depict a proper application of the rules and regulations of the PUD Master Plan. Additional streets may be included and located at the time of PUD Subdivision and/or PUD Site Plan submittal. The actual design of the Secondary Streets shall be as selected from the street standards in *Section 2.3.3 Transportation Network* in accordance with the uses for the Block or Subarea.

Conserved Green Space

The areas shown on the Land Use Plan in *Section 2.2.2 Land Use Plan* are depicted based upon the known existing conditions as of the Planning Board acceptance of the complete application on October 3, 2012. Actual conditions, locations and protected measures thereto shall be presented at the time of PUD

2.2.1 Description

Subdivision and/or PUD Site Plan submittal. Conserved Green Space includes regulated environmental areas and may include stormwater detention areas. Conserved Green Space shall be provided as described within *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*.

Shared Open Space

In addition to Conserved Green Space, additional areas shall be dedicated to a variety of civic, active and passive activities and provided as described within *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*. Shared Open Space may include stormwater detention areas.

PUD Subdivision and PUD Site Plan(s)

PUD Subdivisions and PUD Site Plans are to be in accordance with the following:

- The general intent of the Land Use Plan In *Section 2.2.2 Land Use Plan* and contained within *Section 2.0 PUD Regulations and Standards*;
- *Section 2.2.4 Subarea Composition Principles and Standards* as applicable to both subdivisions and site plans.
- *Section 2.3 PUD Subdivision Regulations and Standards* as applicable to subdivisions;
- *Section 2.4 PUD Site Plan Regulations and Standards* as applicable to site plans;
- PUD Subdivisions and Site Plans may include all or portions of one or several Subareas;
- PUD Subdivisions may include Site Plans as part of the submittal;
- The submittal process shall be in accordance with *Section 2.5 Administration*.

Compliance Alternatives

The PUD Master Plan provides special PUD Regulations and Standards to implement Town goals for the area through a series of planning and design principles that are defined in *Section 2.1.3 Planning and Design Principles*. Conformance with the PUD Master Plan and in particular with *Section 2.0 PUD Regulations and Standards* establishes entitlements to undertake projects and improvements.

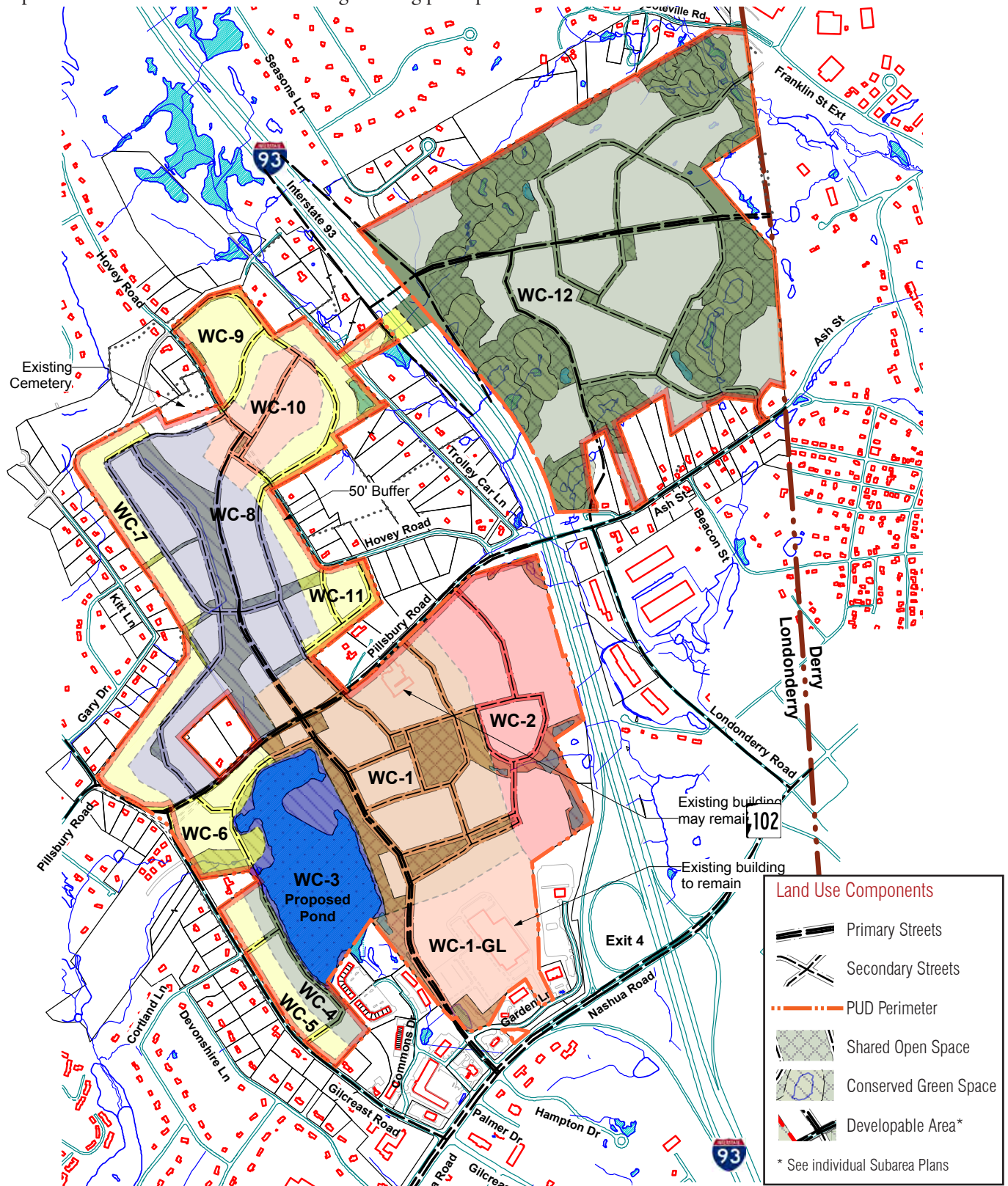
Under PUD Subdivision and/or PUD Site Plan review, the Planning Board and the applicant may mutually agree to an alternative method or standard that meets *Section 2.1.3 Planning and Design Principles* to the same degree or better. This may be accomplished without amending the PUD Master Plan if the applicant documents and the Planning Board approves a Compliance Alternative. A Compliance Alternative must accomplish the relevant Planning and Design Principles: documentation must indicate the specific proposed alternative method or standard that will be used, why the *Section 2.0 PUD Regulations and Standards* are not applicable to the application, and how the project is fully compliant with the intent of the PUD Master Plan.

All PUD Regulations and Standards are subject to these Compliance Alternatives, except for the following, which would need an amendment of the PUD Master Plan if the applicant wishes to modify them for a PUD Subdivision and/or PUD Site Plan application:

- Allowable Uses;
- Building Heights;
- Allowable Densities for Uses and Open Space for the PUD;
- Maximum Use Densities.

2.2.2 Land Use Plan

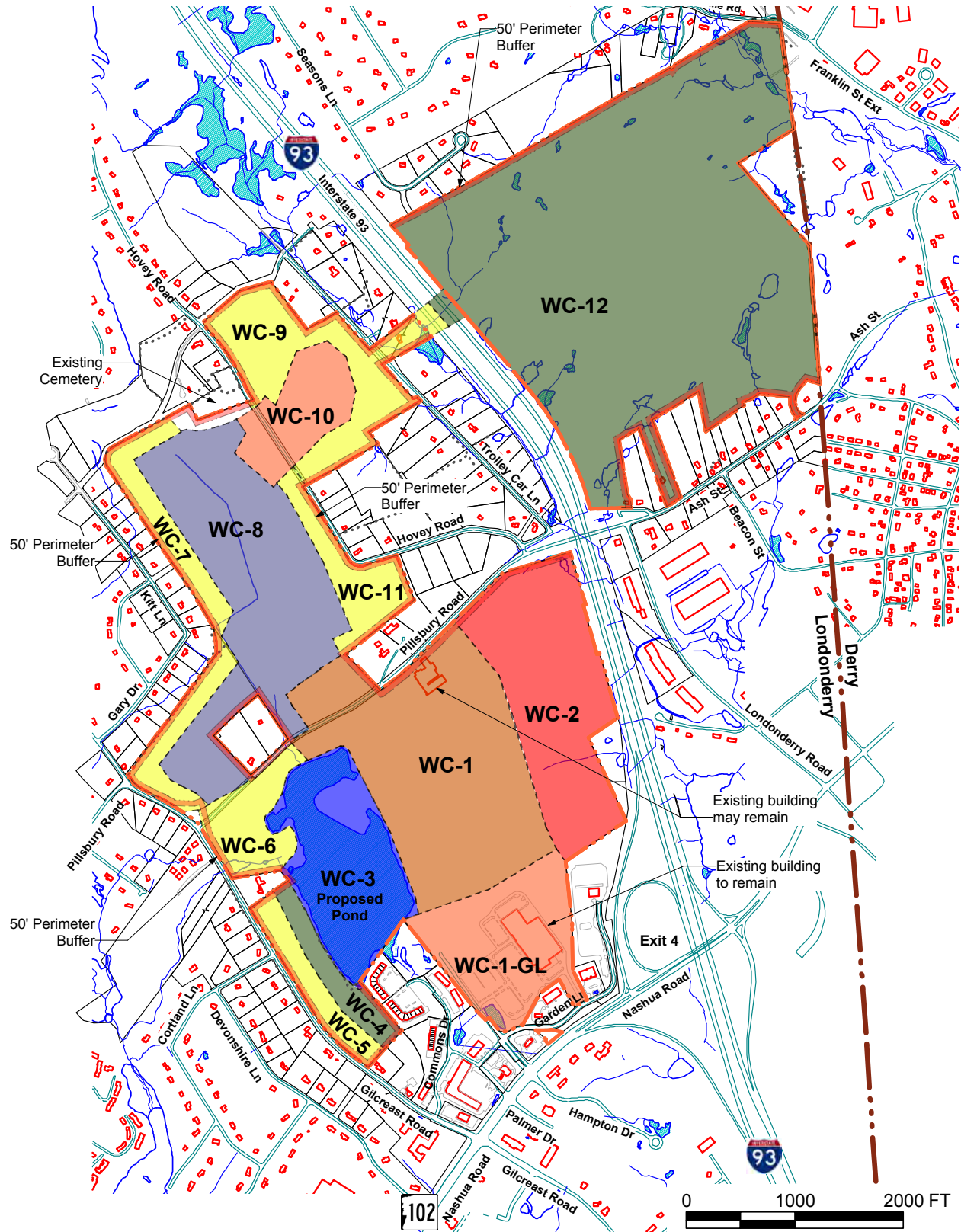
Subareas with the Land Use Components shown. See Section 2.2.4 Subarea Composition Standards and Principles for the development intent of each Subarea and the governing principles and standards.



LAND USE PLAN

2.2.2 Land Use Plan

Outline of each Subarea.



SUBAREA PLAN

2.2.3 Land Use Standards

The Land Use Standards define how land can be developed within the Woodmont Commons PUD. While the Land Use Plan shows the approximate location of streets, Conserved Green Space and Shared Open Space, and developable land, the Land Use Standards provide the rules by which development will be planned and by which PUD Subdivision and PUD Site Plan submittals will be evaluated.

There are several components to the Land Use Standards:

- The **Allowable Uses Table** defines which uses are allowed in each Subarea.
- The **Performance Standards** provide use-specific performance standards.
- The **Allowable Densities Table** defines the maximum density for certain uses by Subarea and for the Woodmont Commons PUD as a whole. This table also provides minimum requirements for Conserved Green Space and Shared Open Space. The footnotes and the flexibility rules are a part of the table and add further clarity about the density requirements.
- **Conserved Green and Shared Open Space Land Use Standards** provide the general rules for provision of these amenities throughout the Woodmont Commons PUD. This Section defines various categories of space and the requirements for the location of publicly accessible space in relation to development within the Woodmont Commons PUD.

These three components are the underlying standards for *Section 2.2.4 Subarea Composition Principles and Standards* which provide more specific details on land use and development requirements by Subarea.

2.2.3 Land Use Standards

Allowable Uses Table

The Allowable Uses Table presents the allowable uses by Subarea. The subsequent sections including the Subarea Standards, and Street, Block, Open Space and Building/Lot Types are subject to and governed by the Allowable Uses Table. For example, an Institutional Building may not be proposed in a Subarea that does not allow institutional uses.

| | WC-1-GL | WC-1 | WC-2 | WC-3 | WC-4 | WC-5 | WC-6 | WC-7 | WC-8 | WC-9 | WC-10 | WC-11 | WC-12 |
|------------------------|---|------|------|------|------|------|------|------|------|------|-------|-------|-------|
| AGRICULTURAL | | | | | | | | | | | | | |
| 1 | Agriculture | A | A | A | A | A | A | A | A | A | A | A | A |
| RESIDENTIAL | | | | | | | | | | | | | |
| 2 | Dwelling, Multi-Family | | A | A | | | | | A | A | A | A | A |
| 3 | Dwelling, Single-Family Detached | | A | A | | A | A | A | A | A | A | A | A |
| 4 | Dwelling, Single Family Attached | | A | A | | A | | A | A | A | A | A | A |
| 5 | Dwelling, Two-Family | | A | A | | A | | A | A | A | A | A | A |
| 6 | Elderly Housing | | A | A | | A | | A | A | A | A | A | A |
| 7 | Mixed Used Residential | | A | A | | | | | A | | A | | A |
| 8 | Accessory Dwelling Units (new subcategory) | | A | A | | A | A | A | A | A | A | A | A |
| CIVIC USES | | | | | | | | | | | | | |
| 9 | Community Center | A | A | A | A | A | | A | | A | | A | A |
| 10 | Public Facilities | A | A | A | A | A | | A | | A | | A | A |
| 11 | Public Utilities Structure | | A | A | | | | | | | | | A |
| 12 | Public Utilities | A | A | A | A | A | A | A | A | A | A | A | A |
| 13 | Recreational Facilities, Public | A | A | A | A | A | | A | | A | | A | A |
| 14 | Religious Facilities | | A | A | | | | | A | | A | A | A |
| 15 | Cultural Uses and Performing Arts | A | A | A | | | | | A | | A | A | A |
| INSTITUTIONAL | | | | | | | | | | | | | |
| 16 | Assisted Living | | A | A | | | | A | | A | | | A |
| 17 | Nursing Home and Accessory Uses | | A | A | | | | | | | | | A |
| 18 | Hospital (new subcategory) | | | | | | | | | | | | A |
| ACCOMMODATION | | | | | | | | | | | | | |
| 19 | Hotels | A | A | A | | | | | | | | | A |
| 20 | Boutique Hotel (new subcategory) | | A | A | | | | A | | A | | A | A |
| 21 | Bed and Breakfast Homestay | | A | A | | A | | A | | A | A | A | A |
| COMMERCIAL USES | | | | | | | | | | | | | |
| 22 | Business Center Development | A | A | A | | | | | | | A | | A |
| 23 | Conference/Convention Center | | A | A | | | | | | | | | A |
| 24 | Day Care Center, Adult | | A | A | | | | A | | A | | | A |
| 25 | Drive-Thru Window | A | A | A | | | | | | | | | A |
| 26 | Financial Institution | A | A | A | | | | | | | A | | A |
| 27 | Flex Uses (new subcategory) | A | A | A | | | | | | | | | A |
| 28 | Education and Training Facilities | A | A | A | | | | | | | | | A |
| 29 | Group Child Care Center | A | A | A | | | | | A | | A | | A |
| 30 | Home Occupation | | A | A | | A | A | A | A | A | A | A | A |
| 31 | Membership Club | A | A | A | | | | | | | | | A |
| 32 | Motor Vehicle Station, Limited Service | A | A | A | | | | | | | | | A |
| 33 | Recreation, Commercial | A | A | A | | | | A | | | A | | A |
| 34 | Retail Sales Establishment | A | A | A | | A | | | A | | A | | A |
| 35 | Parking Structures (new subcategory) | A | A | A | | | | | | | | | A |
| 36 | Professional Office | A | A | A | | | | | A | | A | | A |
| 37 | Dedicated Office Building (new subcategory) | | A | A | | | | | | | | | A |
| 38 | Rental Car Satellite | A | A | A | | | | | | | | | A |
| 39 | Repair Services | A | A | A | | | | | A | | A | | A |
| 40 | Research or Development Laboratories | | A | A | | | | | | | | | A |
| 41 | Restaurant | A | A | A | | A | | | | | A | | A |
| 42 | Restaurant, Fast Food | A | A | | | | | | | | | | A |
| 43 | Restaurant with Take-out, no Drive-through | A | A | A | | | | | | | A | | A |
| 44 | Schools, Private and Charter | | A | A | | | | | A | | A | | A |
| 45 | Service Establishment | A | A | A | | | | | A | | A | | A |

A = Allowable Use within Subarea, subject to the Land Use Allocation Summary Table
 = Use not Allowable within Subarea

2.2.3 Land Use Standards

Performance Standards

Accessory Dwelling Unit

To increase housing alternatives while maintaining neighborhood aesthetics and quality, one accessory apartment within a detached single family dwelling shall be permitted provided the following conditions are met:

- A. Maximum of one (1) accessory apartment per lot up to the maximum limit permitted in the Land Use Allocation Summary for the individual Subarea and the Total PUD Maximum;
- B. The property owner must occupy one of the two units;
- C. The exterior appearance and entrances of the dwelling shall be consistent with a single-family residence;
- D. Only one (1) bedroom is permitted in the accessory apartment and to qualify as an accessory apartment under this Section, the apartment may not exceed 750 square feet of floor space.
- E. Garage parking is encouraged;
- F. No accessory apartment shall be permitted on a lot created as part of a back lot development, per the requirements of *Section 3.4.1.3.7 of the Londonderry Zoning Ordinance*;
- G. A dwelling unit that is counted within the Woodmont Commons PUD maximum for Accessory Units.

Cottage Court

- A. Single-family cottage court developments are permitted in all subareas that permit multi-family uses;
- B. Minimum lot sizes shall be waived;
- C. Cottage units shall be oriented to front, and have a main entry onto, the common open space. Lots abutting a street may front the street. The rear of a cottage unit may face an

Access Lane. Cottage units fronting the street shall have a secondary entrance onto the common open space;

- D. Front, rear and side setbacks from the property lines shall be a minimum of three feet;
- E. Cottage units abutting a public street shall meet the setback requirements of *Section 2.4.2 Building and Lots for Single-Family Detached*;
- F. The common open space shall total at least 250 square feet per cottage unit;
- G. Each individual single-family cottage court may contain four to sixteen cottage units in lots around a common open space;
- H. Parking shall be located on the cottage court property and meet the requirements for single-family parking;
- I. Emergency service access shall be maintained to each dwelling unit via the access lane or other method suitable to the authorities having jurisdiction.

Drive-Thru Windows

- A. Drive-Thru Windows may be attached to Financial institutions, Fast Food Restaurants and Service Establishments such as dry-cleaners in WC-1-GL, WC-1, WC-2, and WC-12;
- B. The Drive-Thru must be located behind the building in WC-1 and WC-2.

Flex Uses

- A. Flex Uses are limited to 5,000 square foot per use in Small, Medium and Large Buildings;
- B. Any Flex Use above 5,000 square foot per use must use a Flex Building.

2.2.3 Land Use Standards

Home Occupations, Child Care and Adult Day Care

Home Occupations and Child Care/Adult Day Care Facilities as Home Occupations are allowed by special exception under *Section 3.12.1.1*, *Section 3.12.2.1* and *Section 3.12.3.1* of the *Londonderry Zoning Ordinance*. In the Woodmont Commons PUD, these uses are allowable as of right, without the need for a special exception. The Planning Board shall evaluate compliance with the above-referenced sections during PUD Subdivision and/or PUD Site Plan review.

Religious Facilities

- A. Religious facilities in residential Subareas must be on lots of two acres minimum and must have traffic access onto a primary road;
- B. All buildings and the parking lot associated with a religious facility will be set back a minimum of 20 feet from all adjacent residential lots, in accordance with *Section 5.02* of the *Londonderry Site Plan Regulations*. The landscaping of this setback buffer zone will be approved by the Planning Board;
- C. Parking for religious facilities shall be determined by the requirements of *Section 3.10*, *Table 1* of the *Londonderry Zoning Ordinance*.

Rental Car Satellite Office

- A. A rental car satellite office includes the vehicle leasing offices and associated automobile parking but does not include vehicle maintenance, repair, preparation or cleaning (other than minor exterior washing);
- B. The accessory automobile parking areas shall be limited to passenger vehicles, trucks and vans amounting to no more than the storage of 25 rental vehicles and associated office parking.

2.2.3 Land Use Standards

Land Use Density Table

The Allowable Densities Table includes maximum amounts of development and minimum amounts of Open Space by Subarea. The development maximums for a “No Exit 4A” scenario are included to the far right and serve as the limit on development until Exit 4A is constructed.

SUBAREA DEVELOPMENT STANDARDS

| LAND USE CATEGORIES | | UNIT OF MEASURE | Total PUD MAXIMUM DEVELOPMENT | WC-1-GL | WC-1 | WC-2 | WC-3 | WC-4 | WC-5 |
|------------------------------|-----------------------------------|-----------------|-------------------------------|---------|----------|---------|------|-------|------|
| Total Land Area | | Acres | | 38 | 77 | 51 | 39 | 9 | 9 |
| Agricultural | | All Types | N/A | | | | | | |
| Residential | New Primary Residence | Units | 1,300 | | 535 | 67 | | 18 | 10 |
| | New Accessory Units | Units | 130 | | 15 | 20 | | 10 | 1 |
| | Existing Units | Units | 9 | | 3 | 1 | | | |
| | Maximum Total | Units | 1,439 | | 553 | 88 | | 28 | 11 |
| Institutional | Hospital ¹ | Beds/GSF | 300/250,000 | | | | | | |
| | Nursing Homes and Assisted Living | GSF | 250,000 | | 20,000 | 20,000 | | | |
| | Maximum Total | GSF | 500,000 | | | | | | |
| Civic | All Types ² | | | | | | | | |
| Accommodations | Hotels ³ | No./Rooms | 3/550 | | 200 | 150 | | | |
| | Boutique Hotels ⁴ | Rooms | 100 | | | | | | |
| Commercial Use | New Commercial Uses | GSF | 882,500 | 50,000 | 362,500 | 100,000 | | 5,000 | |
| | Office Building Space | GSF | 700,000 | | 50,000 | 300,000 | | | |
| | Existing Commercial Uses | GSF | 272,000 | 225,000 | 47,000 | | | | |
| | Maximum Subtotal | GSF | 1,854,500 | 275,000 | 459,500 | 400,000 | | 5,000 | |
| Flexibility Factors * | | | | 15% | 15%/30%* | 30% | 15% | 15% | 15% |

SUBAREA OPEN SPACE AND BUFFER STANDARDS

| OPEN SPACE AND BUFFER CATEGORIES | | UNIT OF MEASURE | Total PUD MINIMUM AREA | SUBAREA OPEN SPACE AND BUFFER STANDARDS | | | | | | |
|---|-----------------------|-----------------|------------------------|---|------|-----|------|-----|-----|--|
| Green and Open Space⁵ | Conserved Green Space | Acres | 89 | | 1.0 | 2.0 | 39.0 | | | |
| | Shared Open Space | Acres | 64 | 1.5 | 10.5 | 2.0 | | 2.5 | | |
| | Total | Acres | 152 | 1.5 | 11.5 | 4.0 | 39.0 | 2.5 | | |
| Perimeter Buffers | | Acres | 38 | | 1.6 | 1.4 | | 1.0 | 2.6 | |

Footnotes

1. Hospital: substitution of the maximum area allocated for Hospital use will require a PUD Master Plan amendment.
2. Civic: Civic uses have no area limitation, subject to traffic and utilities mitigation as described in the Development Agreement.
3. Hotels: A maximum of three hotels is permitted in the PUD. The total of all Hotel rooms within the PUD may not exceed 550 rooms.
4. Boutique Hotel: The total of all Boutique Hotel rooms within the PUD may not exceed 100 rooms.
5. At least one half acre of Shared Open Space or publicly accessible Conserved Green Space must be provided within one quarter mile of the front door of any approved building on a PUD Subdivision or PUD Site Plan application. At least one half acre of Active Open Space must be provided within one half mile of the front door of any approved building on a PUD Subdivision or PUD Site Plan application. Publicly accessible Shared Open Space and Conserved Green Space must be connected to a street or sidewalk network.

The footnotes to this table explain how to apply flexibility rules across Subareas and how to interpret them. Note that the Total PUD Minimum Area must be achieved at build-out but that the Subarea minimum areas have some flexibility in their location only in that they may be contiguous with or partially located in adjacent Subareas if the revised location works better with the development plan and is in accordance with the Shared Open Space and Conserved Green Space Standards.

| LAND USE CATEGORIES | | SUBAREA DEVELOPMENT STANDARDS | | | | | | | MAXIMA WITHOUT Exit 4A | |
|------------------------------|-----------------------------------|-------------------------------|------|--------|------|--------|-------|---------|-----------------------------|-----------------|
| | | WC-6 | WC-7 | WC-8 | WC-9 | WC-10 | WC-11 | WC-12 | West (WC-1-GL -WC-11) | East (WC-12) |
| Total Land Area | | 13 | 23 | 70 | 32 | 17 | 14 | 216 | | |
| Agricultural | All Types | | | | | | | | | |
| Residential | New Primary Residence | 12 | 24 | 190 | 40 | 50 | 24 | 330 | 970 | 330 |
| | New Accessory Units | 4 | | 40 | 10 | 10 | | 20 | 110 | 20 |
| | Existing Units | | | | 2 | | | 3 | | |
| | Maximum Total | 16 | 24 | 230 | 52 | 60 | 24 | 353 | 1,080 | 350 |
| Institutional | Hospital ¹ | | | | | | | 250,000 | - | - |
| | Nursing Homes and Assisted Living | | | 20,000 | | 20,000 | | 170,000 | | |
| | Maximum Total | | | | | | | 420,000 | - | - |
| Civic | All Types ² | | | | | | | | | |
| Accommodations | Hotels ³ | | | | | | | 200 | 350 | - |
| | Boutique Hotels ⁴ | | | | | | | | | |
| Commercial Use | New Commercial Uses | | | 5,000 | | 10,000 | | 350,000 | 532,500 | |
| | Office Building Space | | | | | | | 350,000 | 300,000 | 400,000 |
| | Existing Commercial Uses | | | | | | | | | |
| | Maximum Subtotal | | | 5,000 | | 10,000 | | 700,000 | 832,500 | 400,000 |
| Flexibility Factors * | | 15% | 15% | 15% | 15% | 15% | 15% | 30% | | |

| OPEN SPACE AND BUFFER CATEGORIES | | SUBAREA OPEN SPACE AND BUFFER STANDARDS | | | | | | |
|---|-----------------------|--|-----|------|-----|-----|-----|------|
| Green and Open Space⁵ | Conserved Green Space | | | 10.0 | | | | 36.5 |
| | Shared Open Space | 5.0 | | 1.0 | 0.5 | 0.5 | | 40.0 |
| | Total | 5.0 | | 11.0 | 0.5 | 0.5 | | 76.5 |
| Perimeter Buffers | | 1.1 | 5.7 | 1.9 | 6.6 | | 2.9 | 13.6 |

* Flexibility Factors

1. Development maximums within each Subarea may exceed each indicated amount in accord with the specified flexibility factor, i.e., 15% or 30%. Such increases would require a proportionate decrease in allocations for the same use elsewhere within the PUD, and remain subject to site-specific mitigation reviews during the PUD Site Plan and PUD Subdivision phase.
2. In WC-1 only, Residential is limited to a 15% Flexibility Factor, all other uses have a 30% Flexibility Factor.
3. Allowable Area may be exchanged between Nursing Homes and Assisted Living, Accommodations and Commercial Uses on a per square foot basis, subject to review and confirmation that increases are consistent with the MTIA and all applicable improvement and mitigation standards, or other basis that confirms that no net impact will occur off-site due to the change.
4. Shared Open Space and Conserved Green Space may be provided at the PUD Subdivision and/or PUD Site Plan level in an adjoining Subarea as long as the proposed project meets the proximity requirements on the previous page.
5. Total PUD Maximum Development may not be exceeded.

2.2.3 Land Use Standards

Conserved Green and Shared Open Space Land Use Standards

Open space is an integral part of the overall Land Use Plan for the Woodmont Commons PUD, which will include areas of publicly accessible open space. Walking trails will run throughout many areas of the site. Dwellings will also be in close proximity to a park or green, and many of the streets will be designed to be linear parks for pedestrians and bicyclists. The publicly accessible open space may be dedicated to a municipal or non-profit entity. Shared Open Space and Conserved Green Space are defined in *Section 2.1.5 PUD Definitions*.

Shared Open Space standards at the PUD Subdivision level include publicly accessible open space, natural vegetation and features, and recreational areas. Shared Open Space includes three categories described in this Section: Civic Open Space, Active Open Space and Passive Open Space. These categories are always publicly accessible and may be independent or combined together with other categories to meet the minimum requirements for each Subarea as shown in the Allowable Densities Table on the previous two pages.

Agricultural Open Space, Perimeter Buffers and Conserved Green Space may be publicly accessible or may be on private land. Agricultural Open Space is a category described in this Section and there are neither minimum requirements nor maximum limitations on this category. Perimeter Buffers are defined in *Section 2.1.5 PUD Definitions* and are a separate line in the Allowable Densities Table. Unless the Perimeter Buffer is contiguous with other publicly accessible space, it is not counted toward the Shared Open Space minimums in the Allowable Densities Table.

Conserved Green Space shall be provided within the Woodmont Commons PUD and there are minimum requirements by Subarea as shown in the Allowable Densities Table above. Development of Conserved Green Space is limited to Recreation Trails and Shared Use Trails, as defined in Open Space Types.

Infrastructure also includes open space, such as medians. A description of infrastructure-related open space is included in *Section 2.3.9 PUD Subdivision Landscape*. Standards at the PUD Subdivision level include defining location and access requirements.

Conserved Green Space and Open Space requirements are contained within *Section 2.4 PUD Site Plan Regulations and Standards*.

Each PUD Subdivision submittal shall provide as follows:

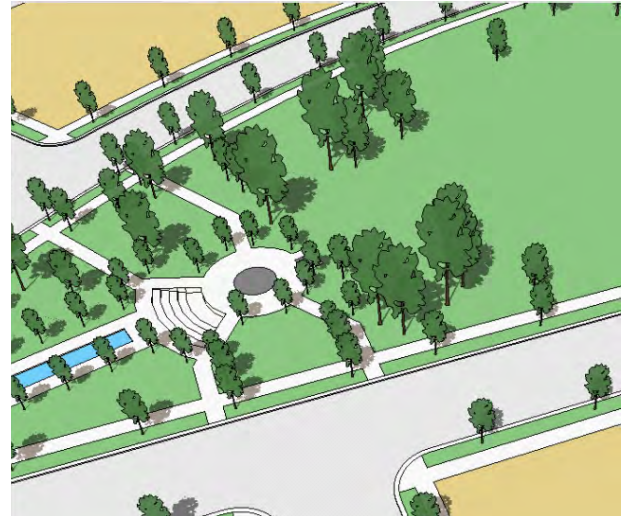
- All PUD Subdivision and/or PUD Site Plan submittals must indicate the types and locations of Conserved Green Space and Shared Open Space(s) within the submittal in general accordance with the Land Use Plan, if applicable. The locations may be adjusted according to the PUD Subdivision requirements.
- At least one half acre of Shared Open Space or publicly accessible Conserved Green Space must be provided within one quarter mile of the front door of any approved building. At least one half acre of Active Open Space must be provided within one half mile of the front door of any approved building.
- Publicly accessible space must be connected to a street, sidewalk network or path.
- Shared Open Space may be constructed at the PUD Subdivision or the PUD Site Plan level.

2.2.3 Land Use Standards

Conserved Green and Shared Open Space Examples: Publicly Accessible Categories

Civic Open Space

Open space intended to serve as a gathering or activity space for civic purposes and is available for public access. Civic purposes include walking, sitting, small gatherings or special events. Civic open space may include adjacent sidewalks, pathways, structures and permanent or temporary accessory uses intended primarily to invite access and use.



Active Open Space

Open space that is intended to support organized, formally defined recreational activities such as playgrounds and recreational fields and is available for public access. Active recreation open space may include pathways, parking areas, structures or accessory uses intended primarily to provide access or support the active use of the open space.



Passive Open Space

Open space that is predominately landscaped with plant materials, available for public access, and adaptable to a range of informal recreational activities including walking and bicycling. Passive recreation open space may include pathways, parking areas, structures or accessory uses such as shared or community gardens intended primarily to provide access or support the passive use of the open space.

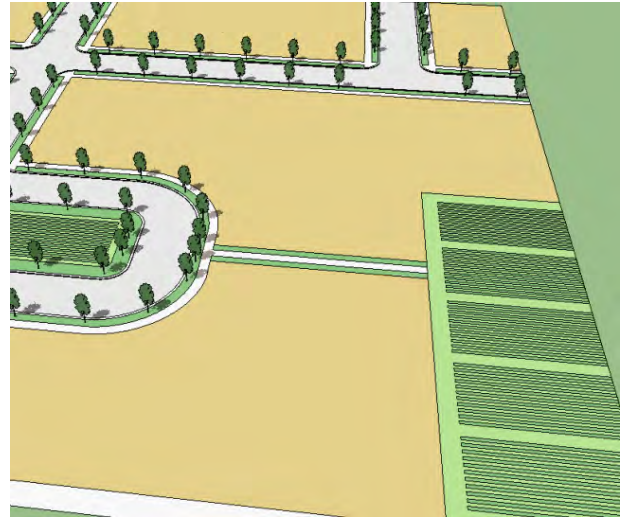


2.2.3 Land Use Standards

Conserved Green and Shared Open Space Examples: Public or Private Categories

Agricultural Open Space

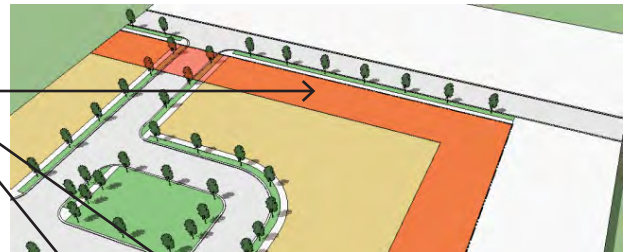
Open space that is used for commercial agricultural production. This type of open space is not intended for public access, but could include such access. Agricultural Open Space may include parking, circulation roads or paths, structures and activities directly associated with the agricultural purposes. A Community Supported Agriculture operation would fall under this category.



Perimeter Buffers

Building setback buffers along designated perimeters of the Woodmont Commons PUD that are predominately planted, but are not required to be publicly accessible. Where a buffer is connected to Shared Open Space (and thus publicly accessible), it may also be used to satisfy the Shared Open Space minimum requirements.

Perimeter Buffers are shown in orange in the illustrations to the right and below.



Conserved Green Space

Open space that retains or provides natural features that may have public access to the extent consistent with the conservation goals and applicable regulations, and as specified in the requirements within the PUD Master Plan. This may include regulated open space such as designated wetland areas subject to state and federal regulations; areas that provide shared stormwater drainage and water quality features serving multiple properties; and intermittent or perennial streams or ponds.



2.2.4 Subarea Composition Principles and Standards

As noted in *Section 2.1.1 Description*, the Subarea Composition Principles and Standards are an integral part of each PUD Subdivisions and/or PUD Site Plan submittal and establish guiding principles and standards by which the Planning Board may review an application.

The following pages provide the intent, the allowable regulatory Types, the Subarea Land Use Plan and the composition standards and principles for each Subarea. Each Subarea has three pages.

The first page describes the intent of the Subarea and the allowable Street, Block, Open Space and Building/Lot Types (described in *Section 2.3 PUD Subdivision Regulations and Standards*). The Types allowable in each Subarea are consistent with its intent, so that the Street Types correspond to the Block Types while the Building/Lot Types correspond to the Block Types and reflect the uses allowable for that Subarea as shown in the Allowable Uses Table. Open Space Types are consistent with the development pattern that results from the combination of Street, Block and Building/Lot Types and create the conditions required by *Section 2.2.3 Land Use Standards*.

The second page for each Subarea is a detailed look at the Land Use Plan with the approximate locations of Primary streets, Secondary streets, conserved Green Space and Shared Open Space. The Subarea Development Densities are also listed.

The third page for each Subarea provides the principles and standards for the Subarea. The Subarea Principles for each Subarea are consistent with its intent and provide guidance to developers (for design and planning), Town Staff (for review) and the Planning Board (for review and approval) in the design of PUD Subdivisions and PUD Site Plans for that Subarea.

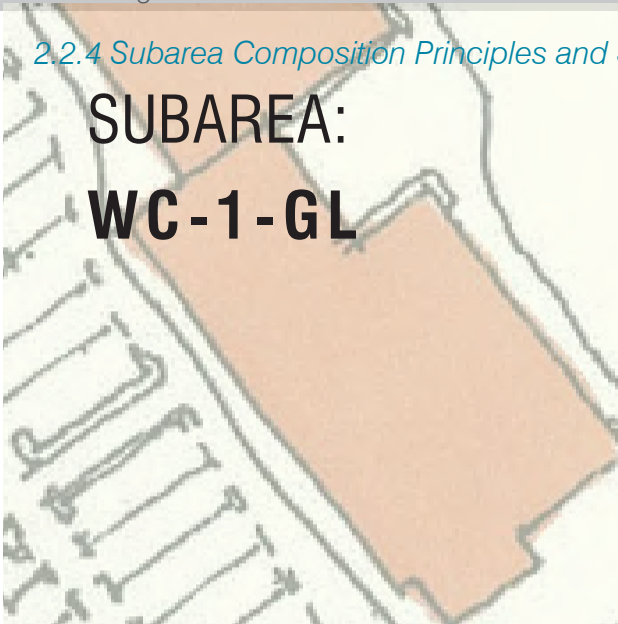
The Subarea Standards are requirements that must be met as part of PUD Subdivision and/or PUD Site Plan approval. These Subarea Standards are broad requirements for the design of streets, pedestrian and bicycle networks, and parking and open spaces. *Section*

2.3 PUD Subdivision Regulations and Standards and *Section 2.4 PUD Site Plan Regulations and Standards* provide additional, specific standards for proposed developments as well as specific parking, lighting, landscape and stormwater standards.

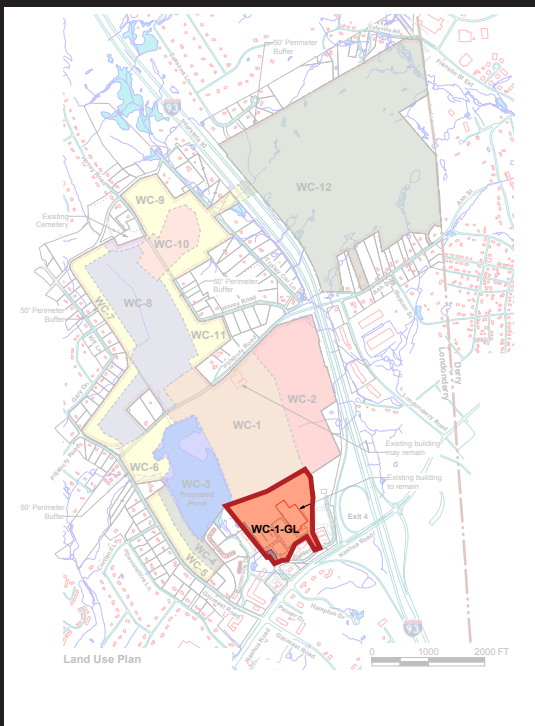
Note that the Compliance Alternatives in *Section 2.2.1 Description* apply to the Subarea Composition Principles and Standards.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-1-GL**



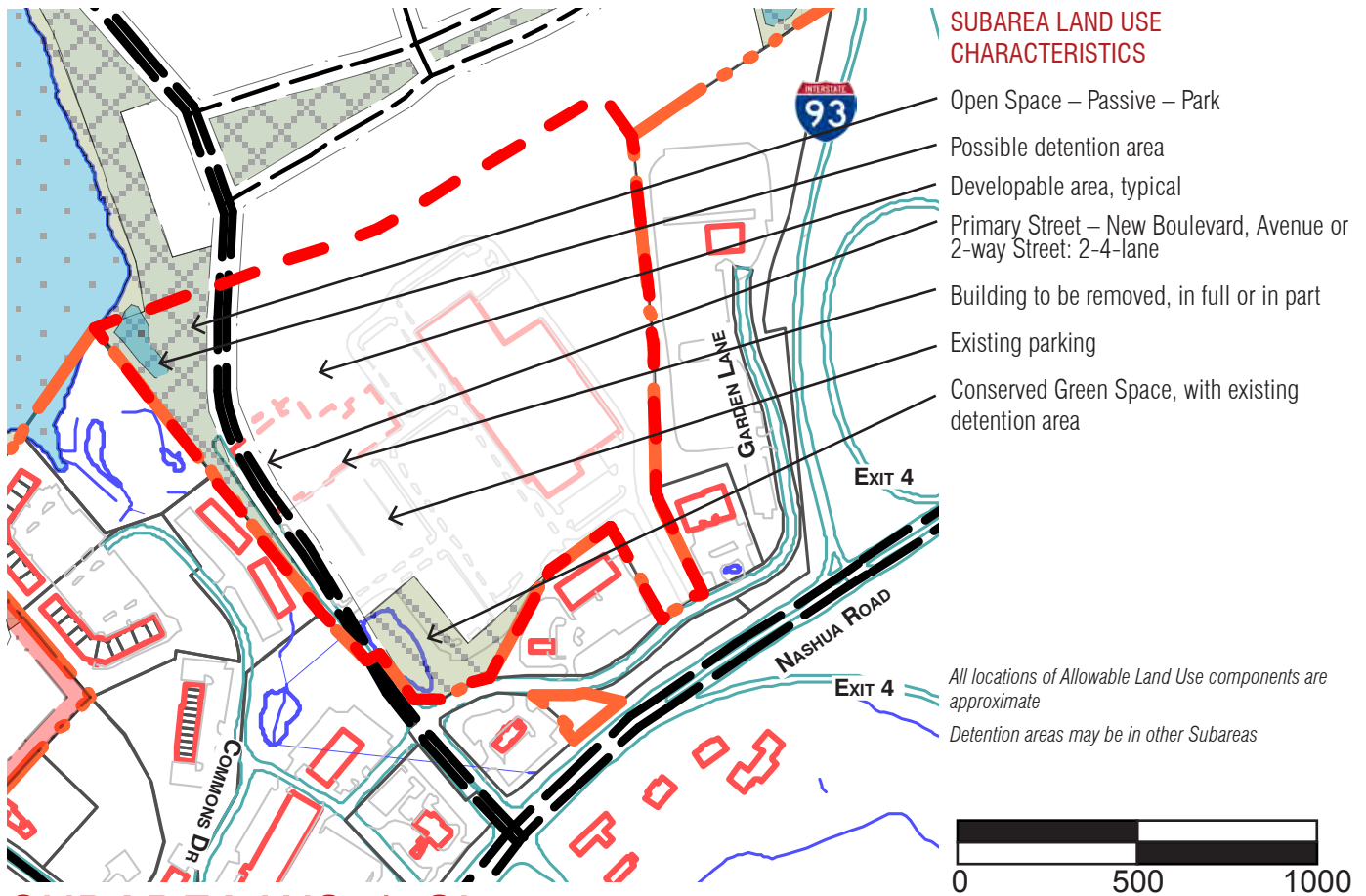
INTENT. The purpose of this Subarea is to create a gateway into the Woodmont Commons PUD and allow access to existing and new retail and commercial uses that serve the Woodmont Commons PUD and the region. This Subarea provides an extension of the commercial uses along Nashua Road.



| Regulating Type | Allowed |
|---|---------|
| STREET TYPES (SECTION 2.3.3) | |
| 1 Boulevard | A |
| 2 Avenue | A |
| 3 Two-way Street | A |
| 4 One-way Street | A |
| 5 Access Lane | A |
| BLOCK TYPES (SECTION 2.3.2) | |
| 1 Village Center | A |
| 2 Neighborhood | |
| 3 Flex | A |
| 4 Perimeter | A |
| 5 Perimeter Neighborhood | |
| OPEN SPACE TYPES (SECTION 2.3.6) | |
| 1 Conserved Green Space | A |
| 2 Green | A |
| 3 Square | A |
| 4 Plaza | A |
| 5 Playing Field | |
| 6 Playground | A |
| 7 Park | A |
| 8 Shared Use Path | A |
| 9 Recreation Trails | A |
| BUILDING AND LOT TYPES (SECTION 2.4.2) | |
| 1 Single Family Detached | |
| 2 Single-Family Attached | |
| 3 Two-Family | |
| 4 Multi-family | |
| 5 Small Building | A |
| 6 Medium Building | A |
| 7 Large Building | A |
| 8 Civic Building | A |
| 9 Dedicated Office Building | |
| 10 Large Format Retail Building | A |
| 11 Institutional Building | |
| 12 Parking Structure | A |
| 13 Flex Building | A |

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-1-GL

Approximate locations of the allowable Land Use components.

SUBAREA DEVELOPMENT

| Subarea Development Maximums Total | |
|------------------------------------|---------------|
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | NOT ALLOWABLE |
| INSTITUTIONAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | 275,000 GSF |

| Subarea Open Space Minima Total | |
|---------------------------------|-----|
| SHARED OPEN SPACE | 0 |
| CONSERVED GREEN SPACE | 1.5 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards

SUBAREA: WC-1-GL

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a regional shopping draw within the overall Woodmont Commons PUD. The overall composition of streets, blocks and open spaces will include the following principles:

- Compatible Use – This is an area that is restricted to retail, Flex Space, commercial uses and mixed-use to be compatible with existing adjacencies.
- Expanding the current development patterns for retail, commercial uses and mixed-use – The development patterns should support vehicular access to the large format retail, Flex Space, mixed-use and commercial uses within this Subarea.
- Well-connected pedestrian-oriented access to the Woodmont Commons PUD – Sidewalks along the primary street will facilitate access to publicly accessible open space and adjoining Subareas.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network will be landscaped or have pedestrian oriented commercial uses. The network provides a north/south connection from Pillsbury Road to Garden Lane which may be a boulevard with a central landscaped median and flanking street trees, sidewalks or shared paths on both sides. Parking is not required along this segment of the primary network, but may be provided in either pockets or complete segments.

SECONDARY STREET NETWORK The secondary street network should be designed to allow vehicular access to the commercial uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Primary Network – The public frontages should be landscaped to reinforce the

characteristics of the network in this area.

- Frontages along the Secondary Network – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to off-street parking. For segments bordering off-street parking, landscaping should be consistent with landscaped screening for the parking lots. For segments with commercial, retail, mixed-use or civic uses, planting strips should be limited to where new streets are provided. Sidewalk paving may extend to the street edge.

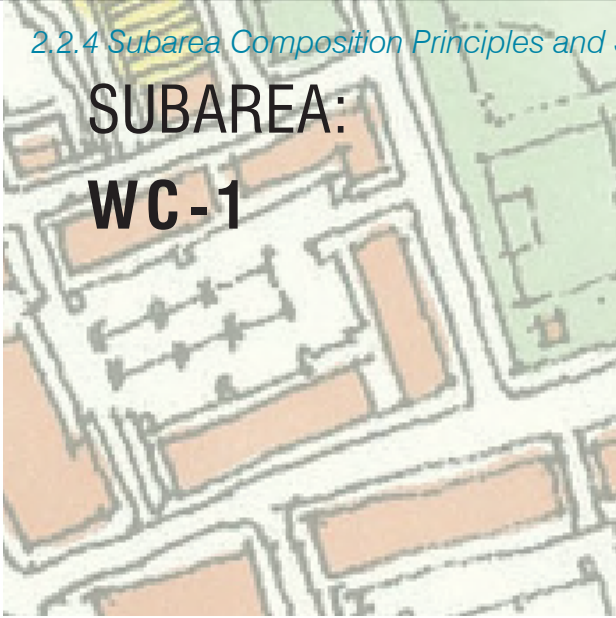
PEDESTRIAN NETWORK Continuous pedestrian networks will be required with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING This Subarea will require off-street parking for development in this area. Off-street parking should have landscape screening along the public frontage or be placed to the rear of buildings.

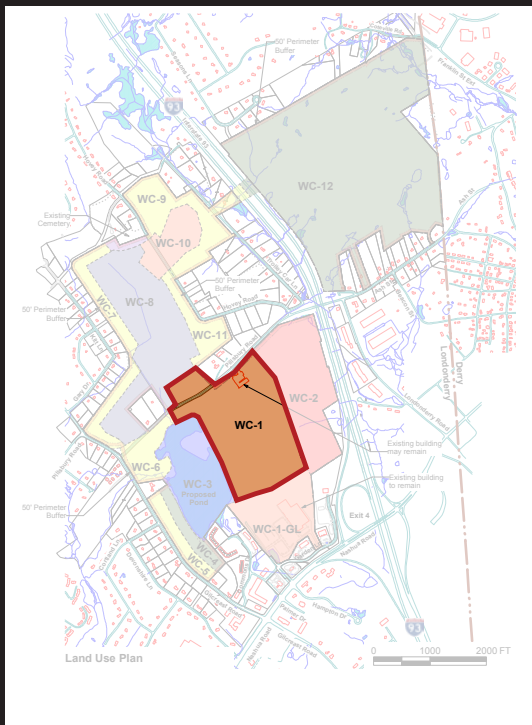
OPEN SPACES Shared open space should be comprised of at least one (1) separate location and one (1) separate publicly-accessible open space type as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards



**SUBAREA:
WC-1**

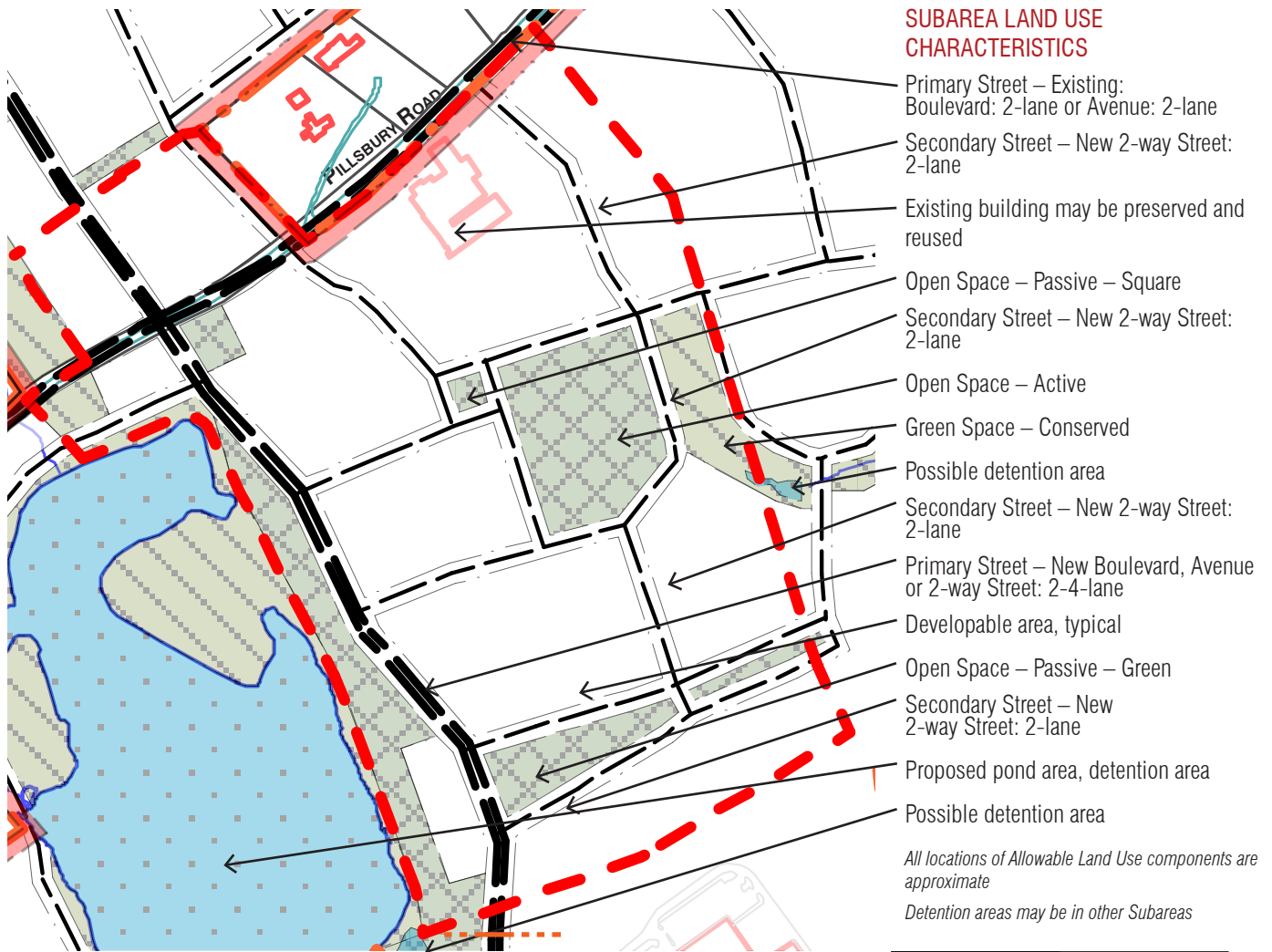
INTENT. This purpose of this Subarea is to create a compact village center with a mix of uses that support diverse building lots and open spaces. The development pattern supports well-connected pedestrian-oriented segments along the streets and sidewalks, lined by residential, retail, mixed-use or commercial uses.



| Regulating Type | Allowed |
|---|---------|
| STREET TYPES (SECTION 2.3.3) | |
| 1 Boulevard | A |
| 2 Avenue | A |
| 3 Two-way Street | A |
| 4 One-way Street | A |
| 5 Access Lane | A |
| BLOCK TYPES (SECTION 2.3.2) | |
| 1 Village Center | A |
| 2 Neighborhood | A |
| 3 Flex | A |
| 4 Perimeter | A |
| 5 Perimeter Neighborhood | A |
| OPEN SPACE TYPES (SECTION 2.3.6) | |
| 1 Conserved Green Space | A |
| 2 Green | A |
| 3 Square | A |
| 4 Plaza | A |
| 5 Playing Field | A |
| 6 Playground | A |
| 7 Park | A |
| 8 Shared Use Path | A |
| 9 Recreation Trails | A |
| BUILDING AND LOT TYPES (SECTION 2.4.2) | |
| 1 Single Family Detached | A |
| 2 Single-Family Attached | A |
| 3 Two-Family | A |
| 4 Multi-family | A |
| 5 Small Building | A |
| 6 Medium Building | A |
| 7 Large Building | A |
| 8 Civic Building | A |
| 9 Dedicated Office Building | A |
| 10 Large Format Retail Building | A |
| 11 Institutional Building | A |
| 12 Parking Structure | A |
| 13 Flex Building | A |

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-1

SUBAREA DEVELOPMENT

| | |
|------------------------------------|-----------------|
| Subarea Development Maximums Total | |
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 553 UNITS |
| INSTITUTIONAL | 20,000 GSF |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | 200 HOTEL ROOMS |
| COMMERCIAL USE | 459,500,000 GSF |

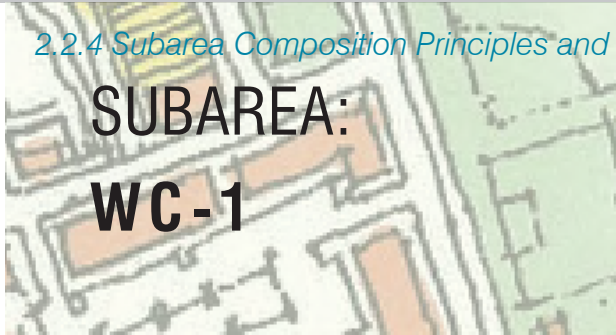
| | |
|---------------------------------|------|
| Subarea Open Space Minima Total | |
| SHARED OPEN SPACE | 10.5 |
| CONSERVED GREEN SPACE | 1.0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



SUBAREA: WC-1

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different uses assembled to create a compact village center within the overall Woodmont Commons PUD. The composition of streets, blocks and open spaces will include the following principles:

- **Variety** – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses. The division of land and infrastructure should support this variety.
- **Diversity of open spaces** – There are a range of open space types that are located within the area, to provide different types of opportunities and amenities.
- **Compact and connected development patterns for retail, commercial uses and mixed-use** – The development patterns should support compact and well-connected pedestrian-oriented segments along the streets and sidewalks at the village center, connected to the primary street network, and lined by retail, Flex Space, commercial uses or mixed-use. The scale and intensity transition to a more residential scale closer to the PUD perimeter and more residential Subareas.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network provides a north/south connection from Pillsbury Road to Garden Lane which may be a boulevard with a central landscaped median and flanking street trees, sidewalks or shared paths on both sides. Parking is required along this segment of the primary network, but may be provided in either pockets or complete segments.

SECONDARY STREET NETWORK The secondary street network should be designed to create developable blocks or to outline planned open space, and incorporate on-street parking where it can serve as a shared parking resource for business, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Primary Network** – Except for areas where there is on-street parking in segments or in pockets, the public

frontages will have street trees to reinforce the characteristics of the network in this area.

- **Frontages along the Secondary Network** – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to on-street parking. For segments intended to have predominately business, civic, accommodation or institutional uses with short term, on-street parking, planting strips should be limited and sidewalk paving generally extended to the street edge. For predominately residential segments, the frontages should be landscaped, with sidewalks off of the street edge.
- **Frontages along Pillsbury Road** – The public frontages along Pillsbury Road should include a planting strip between the street and the sidewalk next to retail, mixed use or commercial uses.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic accommodation or institutional uses. The pedestrian network should connect uses with open space.

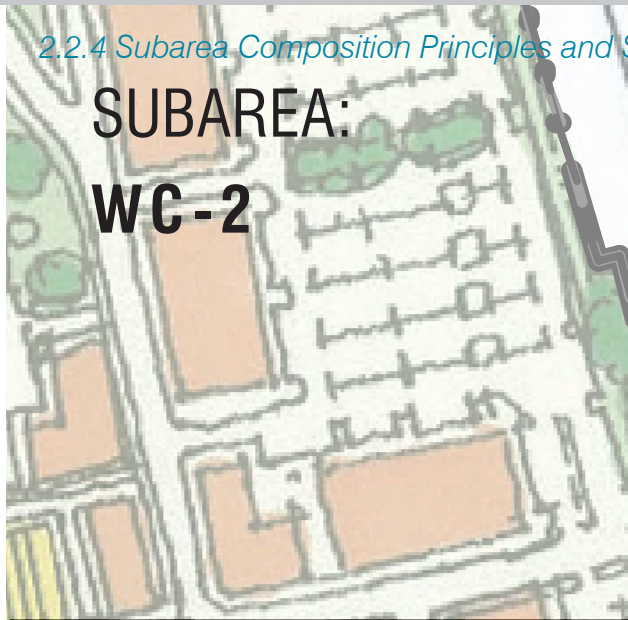
BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to (within Open Space) the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING On-street parking may be considered to contribute to fulfilling the parking requirements of commercial or mixed-use development within 400 feet of the parking spaces for non-residential uses. For residential uses, on-street parking may be allocated for required visitor spaces. Off-street parking should have landscape screening along the public frontage if it fronts on a public street. District parking may be used for off-street parking.

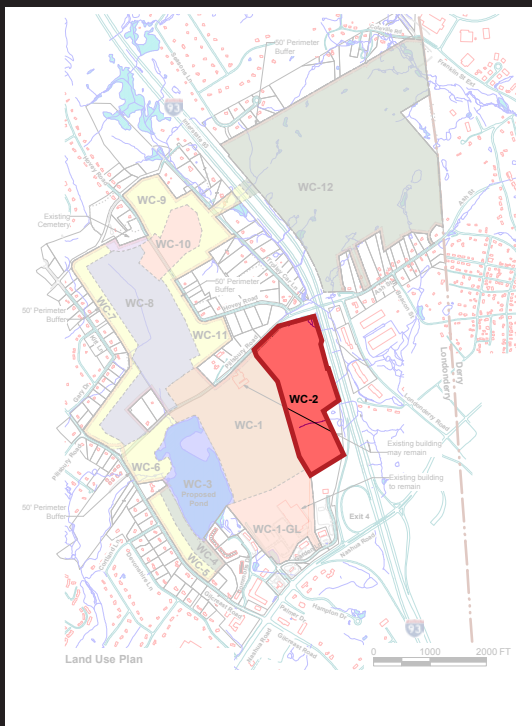
OPEN SPACES Shared open space should be comprised of at least three (3) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-2**



INTENT. The purpose of this Subarea is to create a mixed use district that provides a zone of Dedicated Office buildings and other commercial/mixed-use buildings along the border of I-93 that separates I-93 from residential neighborhoods within the Subarea. The development pattern supports pedestrian-oriented segments along Pillsbury Road and within the residential neighborhoods of the Subarea while providing vehicular access to the larger buildings along the I-93 perimeter.



| Regulating Type | Allowed |
|---|----------|
| STREET TYPES (SECTION 2.3.3) | |
| 1 Boulevard | |
| 2 Avenue | A |
| 3 Two-way Street | A |
| 4 One-way Street | A |
| 5 Access Lane | A |
| BLOCK TYPES (SECTION 2.3.2) | |
| 1 Village Center | A |
| 2 Neighborhood | A |
| 3 Flex | A |
| 4 Perimeter | A |
| 5 Perimeter Neighborhood | A |
| OPEN SPACE TYPES (SECTION 2.3.6) | |
| 1 Conserved Green Space | A |
| 2 Green | A |
| 3 Square | A |
| 4 Plaza | A |
| 5 Playing Field | A |
| 6 Playground | A |
| 7 Park | A |
| 8 Shared Use Path | A |
| 9 Recreation Trails | A |
| BUILDING AND LOT TYPES (SECTION 2.4.2) | |
| 1 Single Family Detached | A |
| 2 Single-Family Attached | A |
| 3 Two-Family | A |
| 4 Multi-family | A |
| 5 Small Building | A |
| 6 Medium Building | A |
| 7 Large Building | A |
| 8 Civic Building | A |
| 9 Dedicated Office Building | A |
| 10 Large Format Retail Building | A |
| 11 Institutional Building | A |
| 12 Parking Structure | A |
| 13 Flex Building | A |

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-2

SUBAREA DEVELOPMENT

Subarea Development Maximums Total

| | |
|----------------|-----------------|
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 88 UNITS |
| INSTITUTIONAL | 20,000 GSF |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | 150 HOTEL ROOMS |
| COMMERCIAL USE | 400,000 GSF |

Subarea Open Space Minima Total

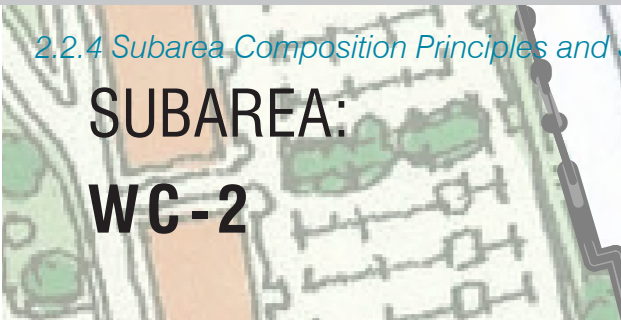
| | |
|-----------------------|-----|
| SHARED OPEN SPACE | 2.0 |
| CONSERVED GREEN SPACE | 2.0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



SUBAREA: WC-2

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different uses assembled to provide a transition from a compact area within the overall Woodmont Commons PUD to a zone of office and other buildings adjacent to I-93. The overall composition of streets, blocks and open spaces will include the following principles:

- Variety – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses, and the division of land and infrastructure should support this variety.
- Diversity of open spaces – There are a range of open space types that are located within the area to provide different types of opportunities and amenities.
- Development along I-93 – The development patterns may support large buildings for offices, Flex Space or other uses that transition in use and scale from the boundary with I-93 to more residential uses. Parking can be placed between the buildings and this boundary.
- Compact and connected development patterns for retail, commercial uses and mixed-use – The development pattern within this Subarea should integrate with that of WC-1.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, Pillsbury Road is the primary street network. Parking is not required along this segment of the primary network.

SECONDARY STREET NETWORK The secondary network should be designed to create developable blocks or to outline planned open space, and incorporate on-street parking where it can serve as a shared parking resource for commercial use, retail, mixed-use, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Primary Network – the public frontages will have street trees to reinforce the characteristics of the network in this area.
- Frontages along the Secondary Network – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to on-street parking. For segments intended to have predominately commercial use, retail, mixed-use, civic, accommodation, residential or institutional uses with short term, on-street parking, planting strips should be limited and sidewalk paving generally extended to the street edge. For predominately residential segments or areas where on-street parking is not expected to serve as a short-term supply, the frontages should be landscaped.

FRONTAGES along Pillsbury Road – The public frontages along Pillsbury Road should include a planting strip between the street and the sidewalk next to retail, mixed use or commercial uses.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic accommodation or institutional uses. The pedestrian network should connect uses with open space.

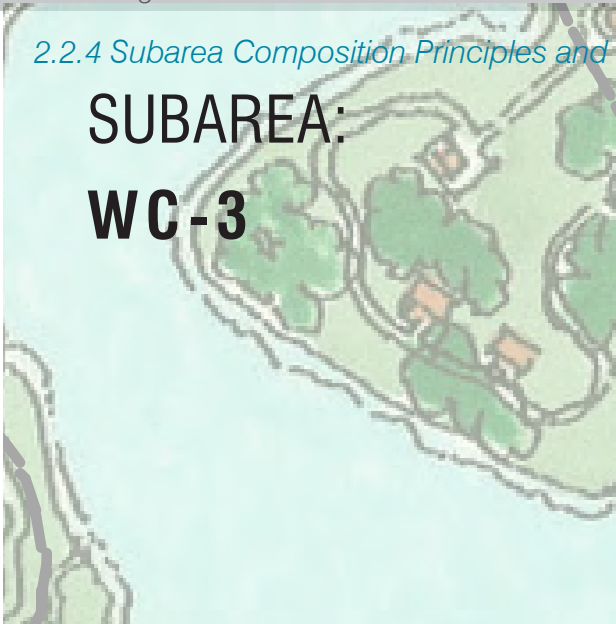
BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING On-street parking may be considered to contribute to fulfilling the parking requirements of commercial or mixed-use development within 400 feet of the parking spaces for non-residential uses. For residential uses, on-street parking may be allocated for required visitor spaces. Off-street parking should have landscape screening along the public frontage if it fronts on a public street. District parking may be used for off-street parking.

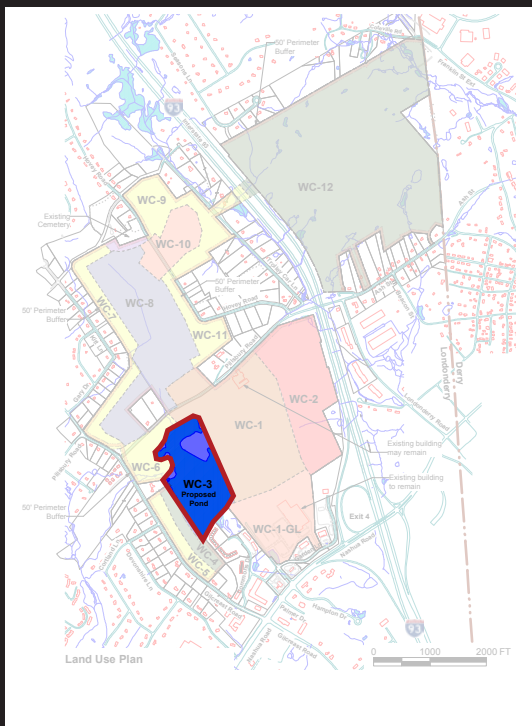
OPEN SPACES Shared open space should be comprised of at least two (2) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-3**



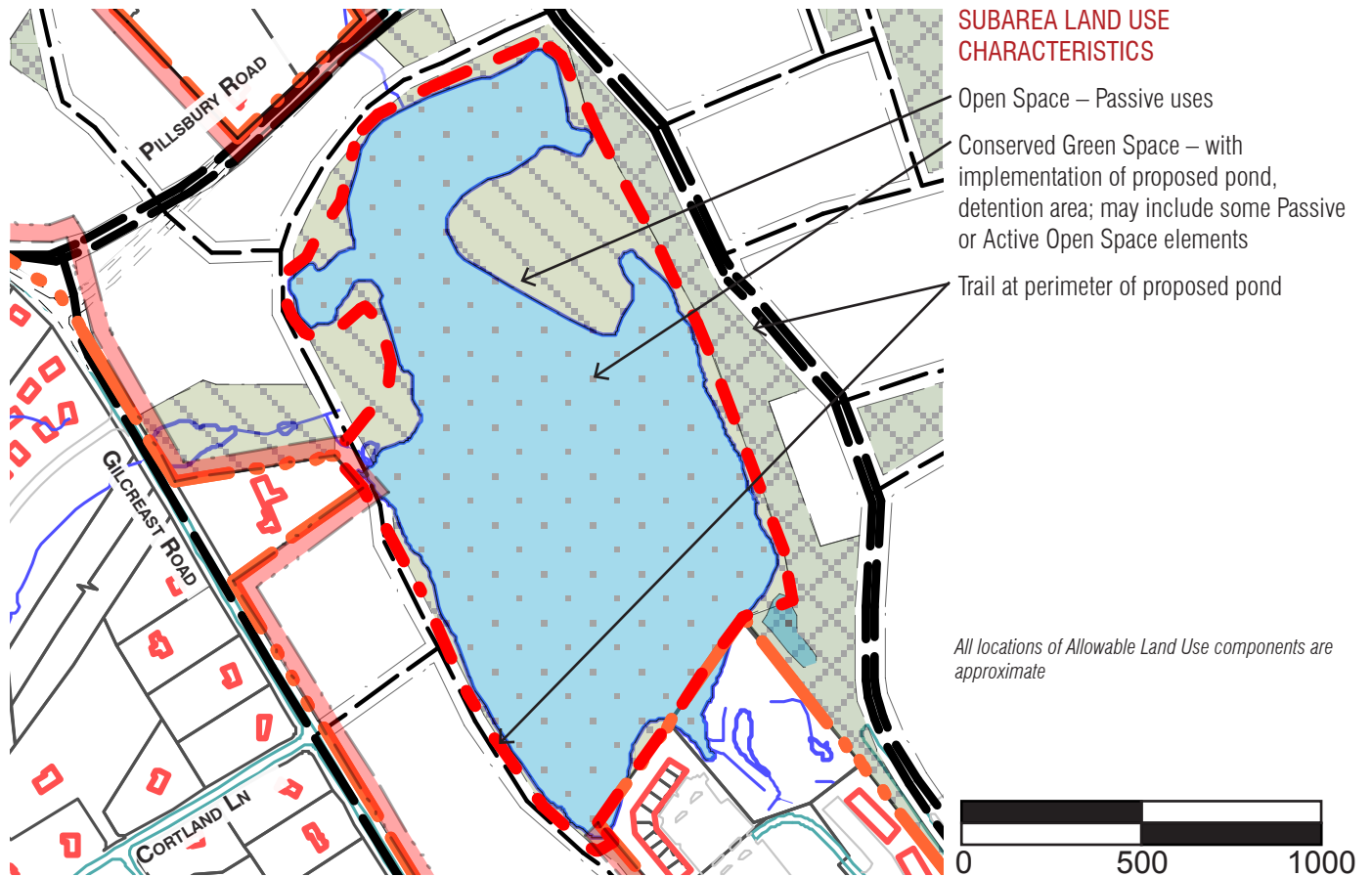
INTENT. The purpose of this Subarea is to preserve significant amounts of open space for public access. Development in this Subarea will be limited to the secondary streets, Recreation Trails and Shared Use Paths that provide public access and accessory buildings that support the use and maintenance of the Subarea.



| Regulating Type | Allowed |
|---|----------|
| STREET TYPES (SECTION 2.3.3) | |
| 1 Boulevard | |
| 2 Avenue | |
| 3 Two-way Street | |
| 4 One-way Street | |
| 5 Access Lane | A |
| BLOCK TYPES (SECTION 2.3.2) | |
| 1 Village Center | |
| 2 Neighborhood | |
| 3 Flex | A |
| 4 Perimeter | |
| 5 Perimeter Neighborhood | |
| OPEN SPACE TYPES (SECTION 2.3.6) | |
| 1 Conserved Green Space | A |
| 2 Green | A |
| 3 Square | A |
| 4 Plaza | |
| 5 Playing Field | |
| 6 Playground | A |
| 7 Park | A |
| 8 Shared Use Path | A |
| 9 Recreation Trails | A |
| BUILDING AND LOT TYPES (SECTION 2.4.2) | |
| 1 Single Family Detached | |
| 2 Single-Family Attached | |
| 3 Two-Family | |
| 4 Multi-family | |
| 5 Small Building | |
| 6 Medium Building | |
| 7 Large Building | |
| 8 Civic Building | A |
| 9 Dedicated Office Building | |
| 10 Large Format Retail Building | |
| 11 Institutional Building | |
| 12 Parking Structure | |
| 13 Flex Building | |

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-3

SUBAREA DEVELOPMENT

| Subarea Development Maximums Total | |
|------------------------------------|---------------|
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | NOT ALLOWABLE |
| INSTITUTIONAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | NOT ALLOWABLE |

| Subarea Open Space Minima Total | |
|---------------------------------|------|
| SHARED OPEN SPACE | 0 |
| CONSERVED GREEN SPACE | 39.0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards

SUBAREA:

WC-3

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea is dedicated to a combination of Conserved Green Space and Passive Open Space including a proposed pond. The overall composition of streets, trails, paths and buildings will include the following principle:

- Compatible Use – This is an area that is restricted to a combination of Conserved Green Space and Passive Open Space. Uses will be compatible with this focus and will include recreation trails and independent use paths that are integrated with the sidewalks at adjacent streets.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary street network should be designed to connect with the street network in adjoining Subareas while creating pedestrian and bicycle access to a system of integrated recreation trails and independent use paths. On-street parking may be provided to allow public access to the open space areas. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Secondary Network – For segments along Open Space, the frontages should be landscaped.
- Frontages bounding this Subarea – Frontages bounding this Subarea should either contain a public sidewalk or a trail located within the private lot.

PEDESTRIAN NETWORK Pedestrian networks, including sidewalks or trails, will be provided to connect the trail network within the Open Space to larger circulation networks, including the trail network in WC-8.

BICYCLE NETWORK Accommodations for bicycles will be provided along trails or on the secondary street network.

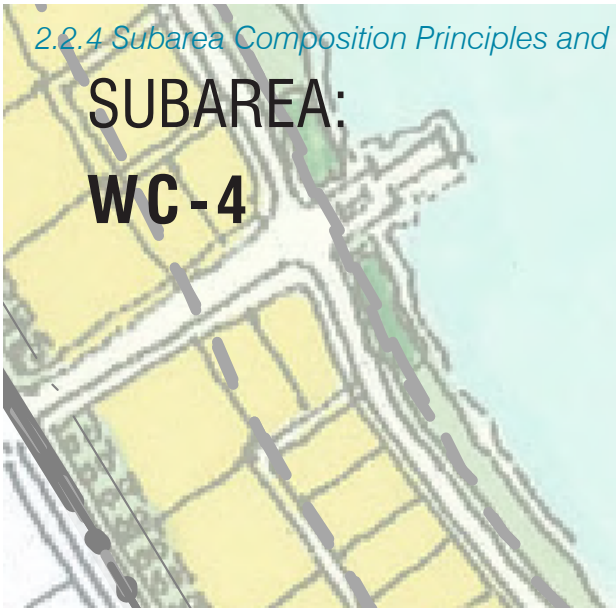
PARKING On-street parking should be provided to

allow public access to Shared Open Space. Other parking lots should be limited and integrated into the landscape.

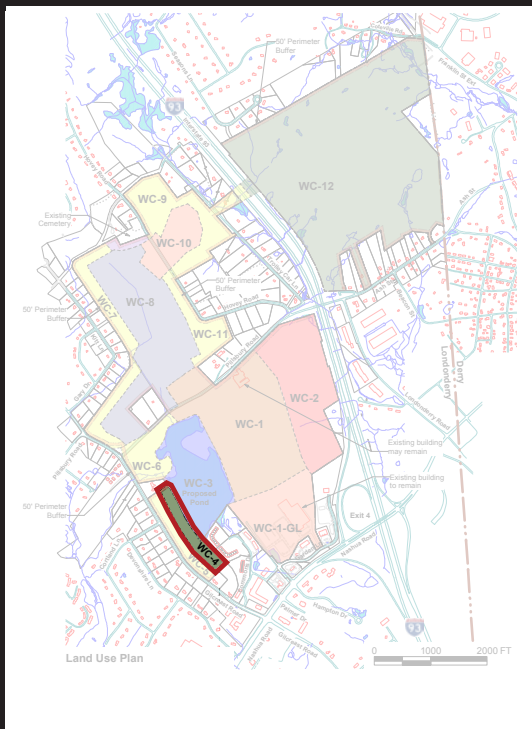
OPEN SPACES This Subarea is predominantly publicly accessible Open Space and Conserved Green Space as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-4**



INTENT. The purpose of this Subarea is to create a residential area that allows for a gradual increase in density from the single-family homes in WC-5 along Gilcrest Road to the mix of uses in WC-1. Development patterns will include residential buildings on smaller lots with access to shared open space.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

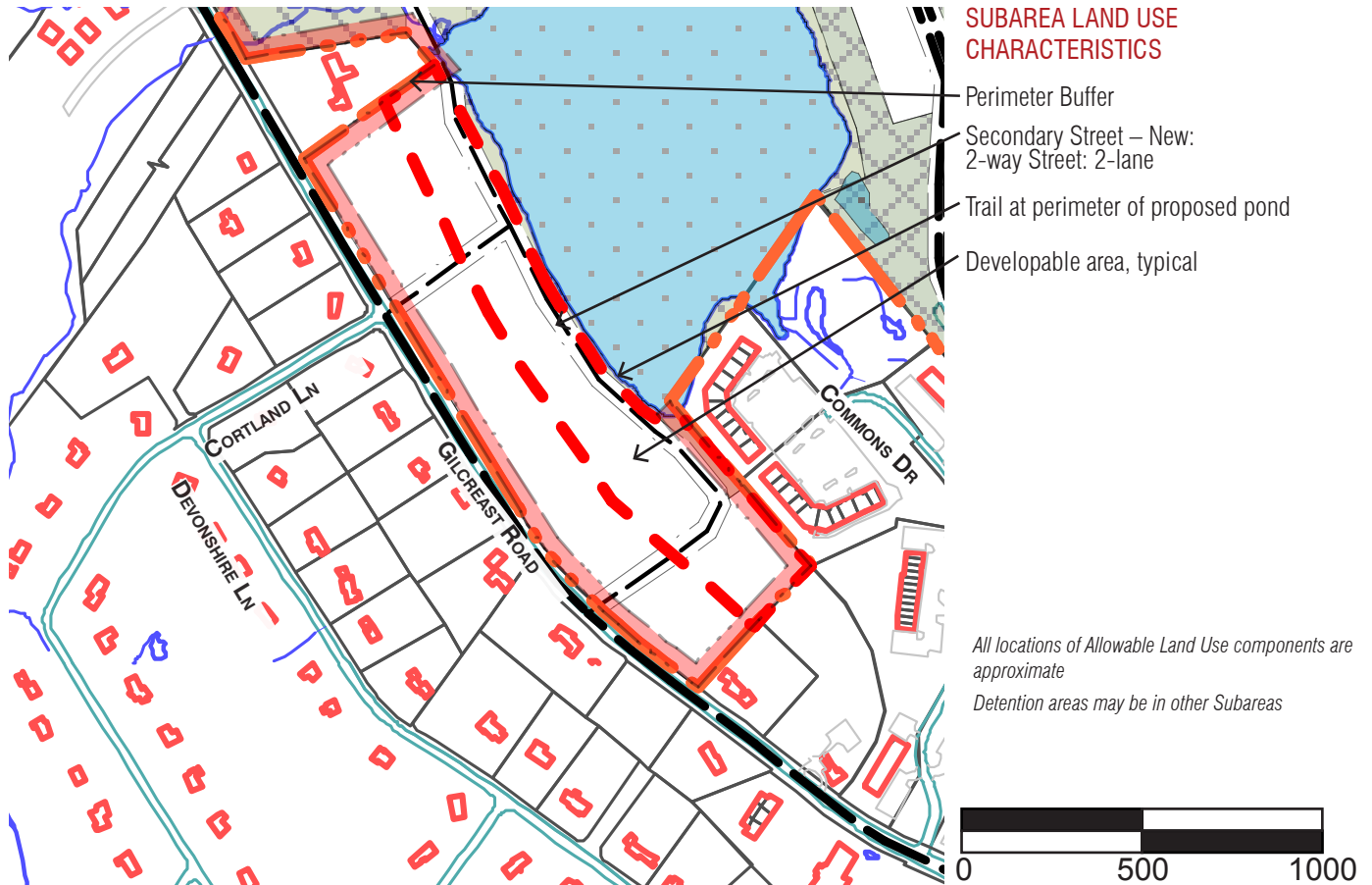
- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached **A**
- 3 Two-Family
- 4 Multi-family
- 5 Small Building **A**
- 6 Medium Building
- 7 Large Building
- 8 Civic Building
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-4

SUBAREA DEVELOPMENT

| | |
|------------------------------------|---------------|
| Subarea Development Maximums Total | |
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 28 UNITS |
| INSTITUTIONAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | 5,000 GSF |

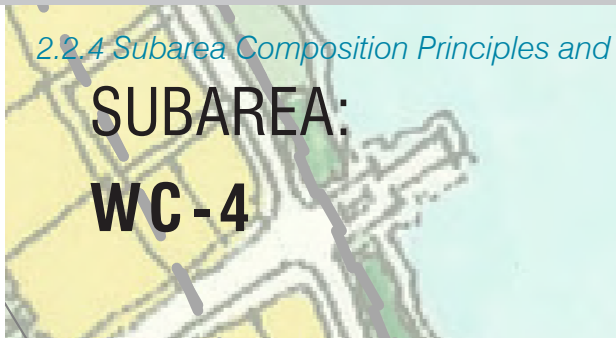
| | |
|---------------------------------|-----|
| Subarea Open Space Minima Total | |
| SHARED OPEN SPACE | 2.5 |
| CONSERVED GREEN SPACE | 0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



SUBAREA: WC-4

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides a transition from existing residential areas outside the Woodmont Commons PUD to the higher density development in WC-1. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is primarily residential uses of a similar scale and character to neighboring properties including single family, duplex and accessory residential units. Some small neighborhood service uses are allowable.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Orientation to Proposed Pond** – This Subarea borders Subarea WC-3, which is connected to the open space surrounding a proposed pond. The configuration of lots and streets responds to adjacencies and access requirements of WC-3 and WC-5.
- **Compatibility with Adjacent Subareas** – This Subarea borders Subarea WC-3 and Subarea WC-5, with open space and residential uses respectively. Buildings on lots in this Subarea should address both the proposed pond in WC-3 and the street network that is contiguous to WC-5.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks and open space and to limit the direct access onto Gilcrest Road. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The public frontages along the secondary network should be provided with planting strips and sidewalks. On-street parking may

be provided.

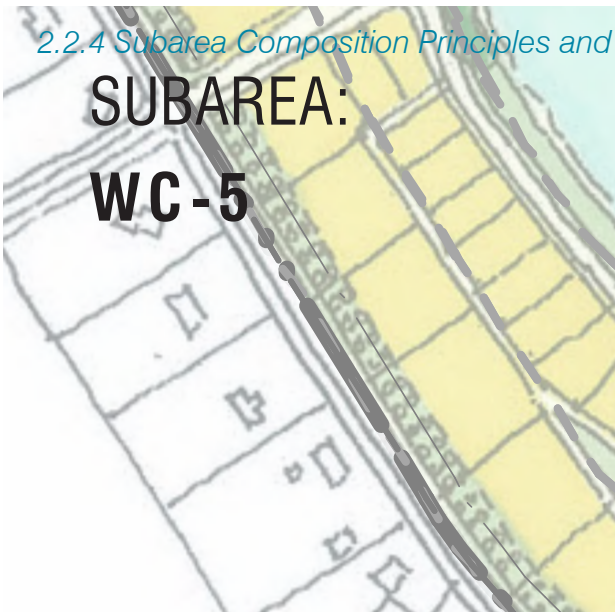
PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Accommodations for bicycles will be provided along trails or on the secondary street network.

PARKING On-street parking should be provided to allow for guest parking and public access to Shared Open Space. Off-street parking may be required for other uses.

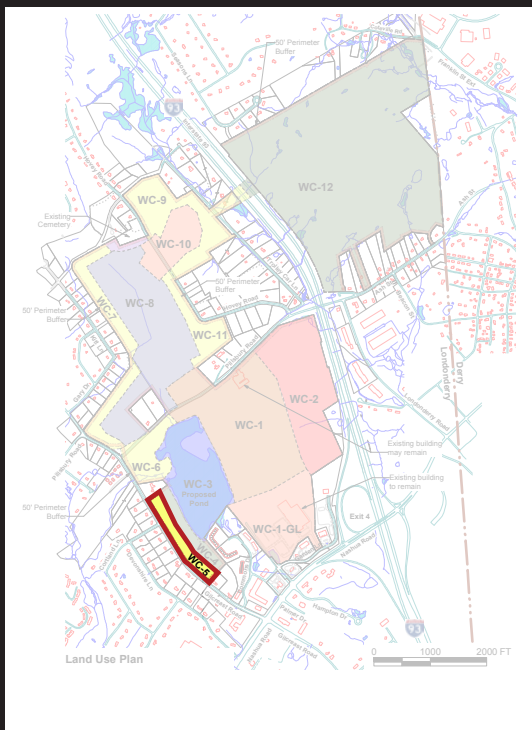
OPEN SPACES Shared open space should be comprised of at least one (1) separate location and ones (1) separate publicly-accessible open space type along the perimeter of the pond as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards



**SUBAREA:
WC-5**

INTENT. This Subarea is intended to create a residential neighborhood that is compatible with the context of existing residential properties.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

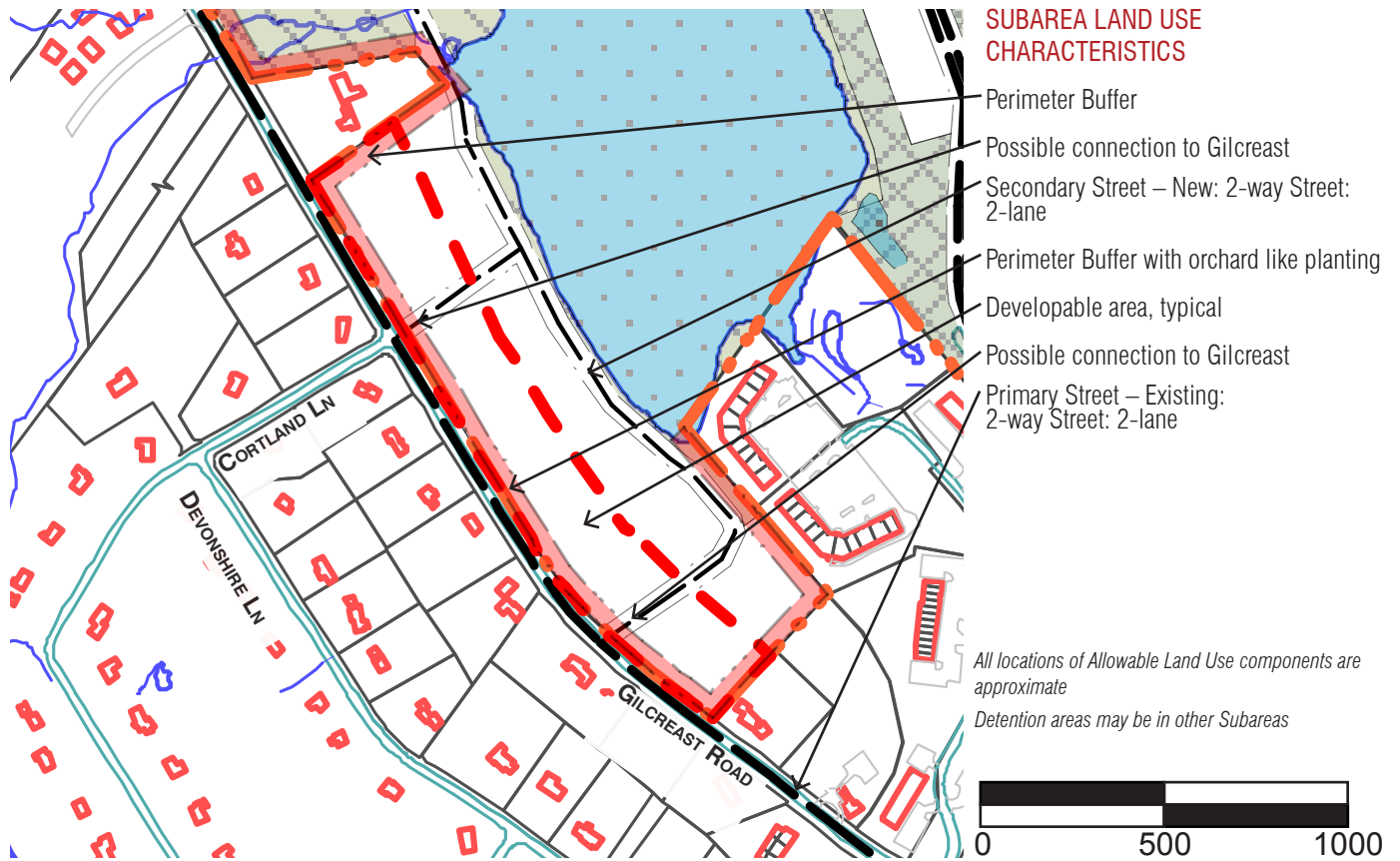
- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached
- 3 Two-Family
- 4 Multi-family
- 5 Small Building
- 6 Medium Building
- 7 Large Building
- 8 Civic Building
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-5

SUBAREA DEVELOPMENT

Subarea Development Maximums Total

| | |
|----------------|---------------|
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 11 UNITS |
| INSTITUTIONAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | NOT ALLOWABLE |

Subarea Open Space Minima Total

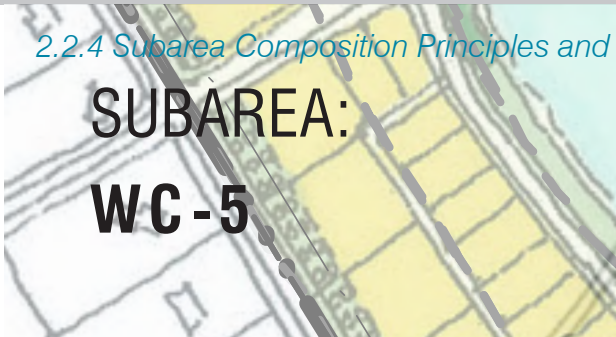
| | |
|-----------------------|---|
| SHARED OPEN SPACE | 0 |
| CONSERVED GREEN SPACE | 0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



SUBAREA: WC-5

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea is intended to provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is restricted to residential uses of a similar scale and character to neighboring properties including single family and accessory residential units.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Compatible Site Disposition** – The minimum frontage (150 feet for single family residences) and minimum setback (50 feet front setback as indicated on the Land Use Plan) provide an overall scale of lots and deployment of buildings that is compatible and respectful of the adjacent context.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Along Gilcrest Road, the perimeter buffer should include orchard-like plantings.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network is an existing street at the boundary of the Woodmont Commons PUD, Gilcrest Road. No more than two egresses onto Gilcrest Road will be provided within this Subarea.

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks and to limit the direct access onto Gilcrest Road. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The

relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Primary Network** – Buildings must be set back beyond the 50' buffer at Gilcrest Road. The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.
- **Frontages along the Secondary Network** – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.

PEDESTRIAN NETWORK Sidewalks do not exist along Gilcrest Road, independent shared use paths or sidewalks at egress locations onto Gilcrest Road are required to provide pedestrian access into the Woodmont Commons PUD and to connect to the larger pedestrian network within the PUD. The pedestrian network should connect uses with open space.

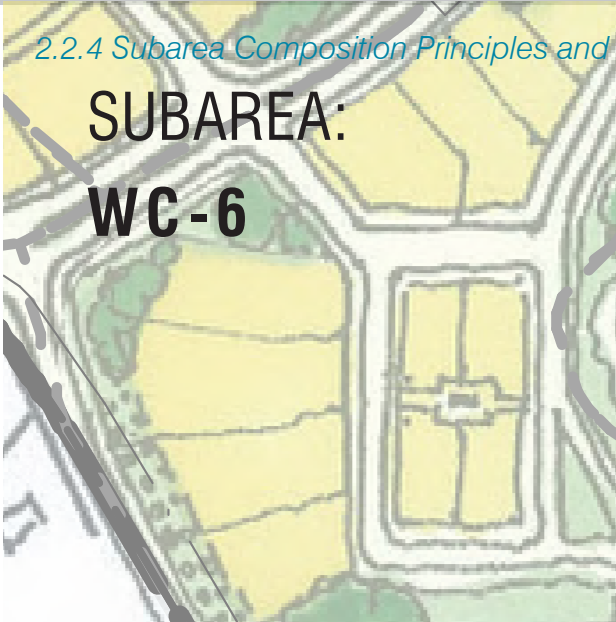
BICYCLE NETWORK Bike paths or routes do not exist along Gilcrest Road. Bicyclists will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both. Access at egress locations onto Gilcrest Road will be connected to the larger bicycle network within the Subarea.

PARKING On-street parking shall be provided on the internal street network but not on Gilcrest Road.

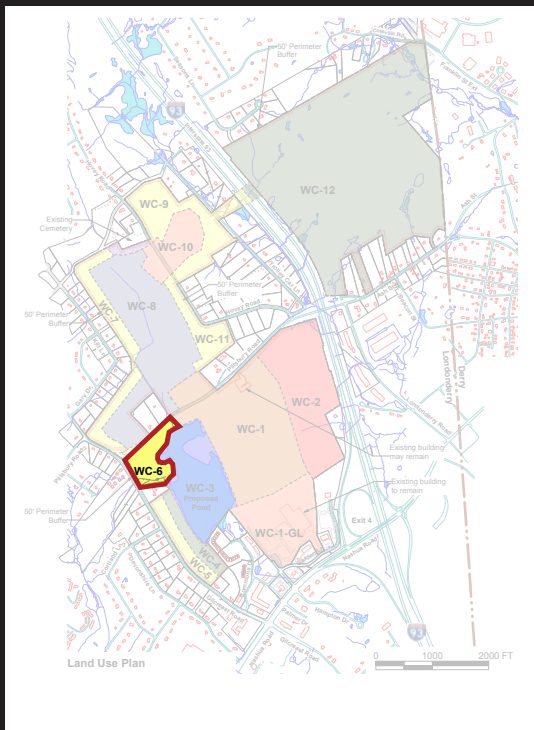
OPEN SPACES No other minimum open space requirements are defined within this Subarea as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-6**



INTENT. The purpose of this Subarea is to create a residential neighborhood that is compatible with the context of existing residential properties. The development pattern could include an internal integrated mix of smaller civic and institutional uses with a pedestrian-oriented network of streets.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

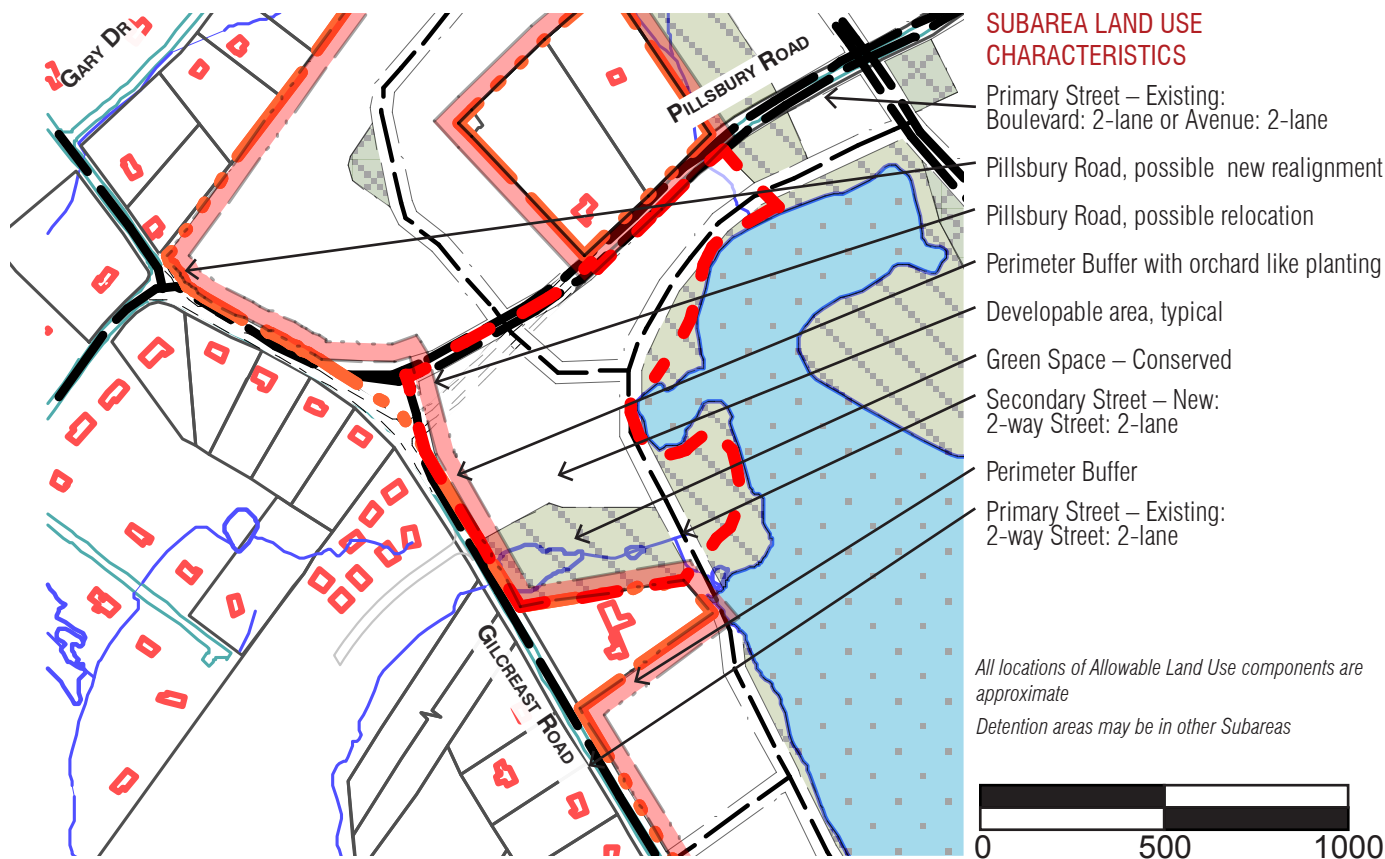
- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached **A**
- 3 Two-Family
- 4 Multi-family
- 5 Small Building **A**
- 6 Medium Building
- 7 Large Building
- 8 Civic Building **A**
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building **A**
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-6

SUBAREA DEVELOPMENT

Subarea Development Maximums Total

| | |
|----------------|---------------|
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 16 UNITS |
| INSTITUTIONAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | NOT ALLOWABLE |

Subarea Open Space Minima Total

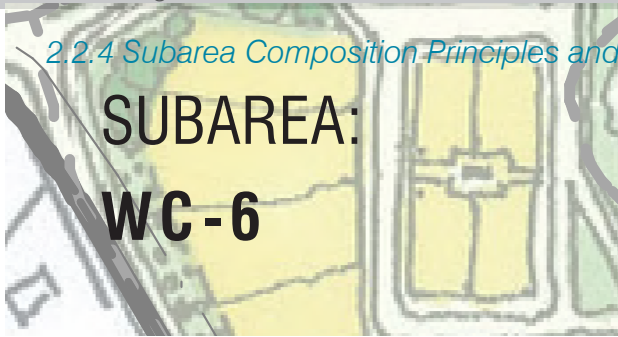
| | |
|-----------------------|-----|
| SHARED OPEN SPACE | 5.0 |
| CONSERVED GREEN SPACE | 0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is primarily residential uses of a similar scale and character to neighboring properties including single family, duplex and accessory residential units. Some small non-residential uses are allowable internal to the Subarea.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Compatibility with Adjacent Subareas** – This Subarea borders Subarea WC-3. Pedestrian sidewalk access at the street should be integrated with the recreation trails and independent use paths in that Subarea.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Along Gilcrest Road, the perimeter buffer should include orchard-like plantings.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, Pillsbury Road is the primary street network. A possible improvement outlined in the Master TIA is the redesign of Gilcrest Road as it meets Pillsbury Road. Parking is not required along this segment of the primary network.

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks and open space and to limit the direct access onto Gilcrest Road. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.
- **Frontages along Pillsbury Road** – The public frontages along Pillsbury Road should include a planting strip between the street and the sidewalk next to retail, mixed use or commercial uses.

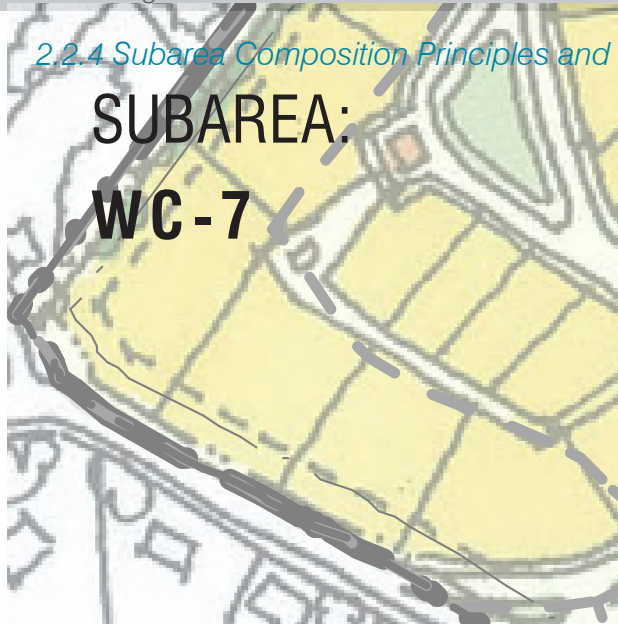
PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea and will link to sidewalks and trails around the pond. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Accommodations for bicycles will be provided on the Secondary Street Network.

PARKING On-street parking should be provided to support residential uses. Off-street parking may be required for other uses.

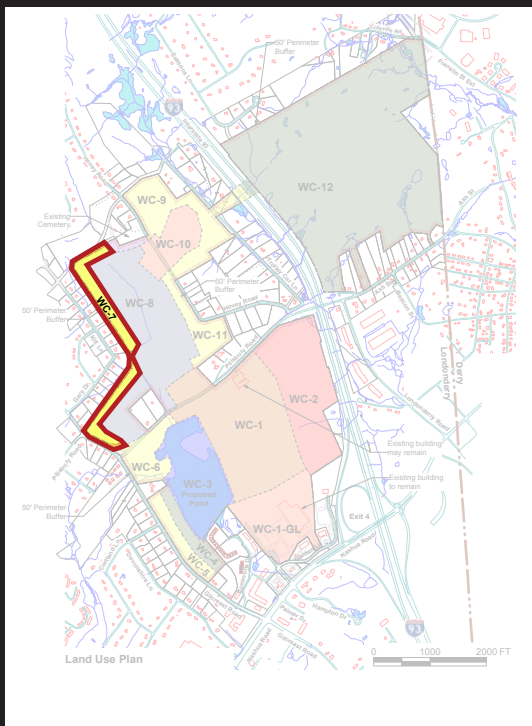
OPEN SPACES Shared open space should be comprised of at least two (2) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards



**SUBAREA:
WC-7**

INTENT. The purpose of this Subarea is to create a residential neighborhood that is compatible with the context of existing residential properties.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

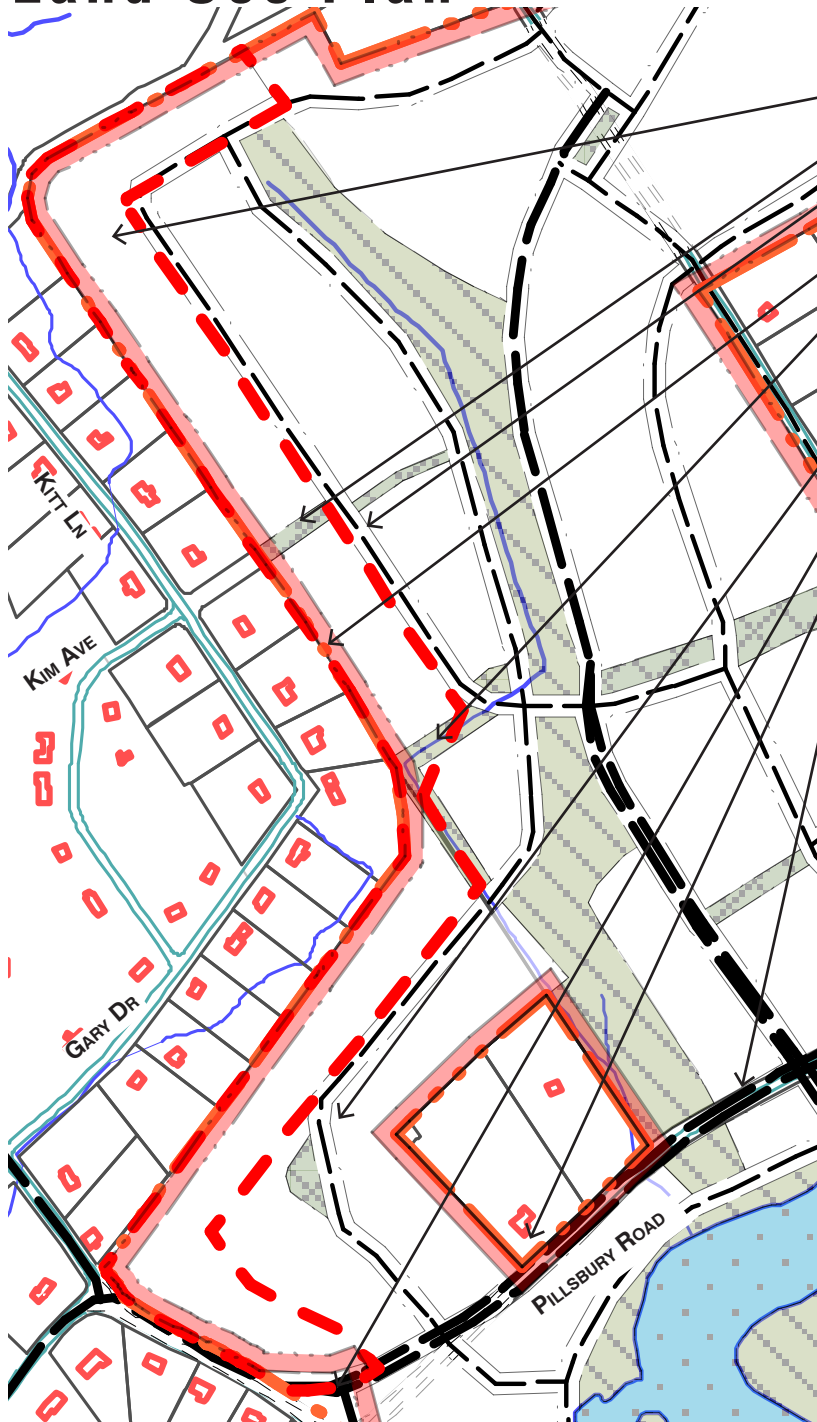
- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached
- 3 Two-Family
- 4 Multi-family
- 5 Small Building
- 6 Medium Building
- 7 Large Building
- 8 Civic Building
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA LAND USE CHARACTERISTICS

- Developable area, typical
- Possible trail connection
- Secondary Street – New:
2-way Street: 2-lane
- Perimeter Buffer
- Conserved Green Space
- Secondary Street – New:
2-way Street: 2 to 4 – Lane
- Pillsbury Road, possible relocation
- Pillsbury Road, possible realignment
- Primary Street – Existing:
Boulevard: 2-lane or Avenue: 2-lane

All locations of Allowable Land Use components are approximate
 Detention areas may be in other Subareas



SUBAREA WC-7

LAND USE COMPONENTS

- - - - - PUD Perimeter
- Subarea Boundary
- - - - - Subdivision Lot Line
- Primary Street as identified in the Master TIA

- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

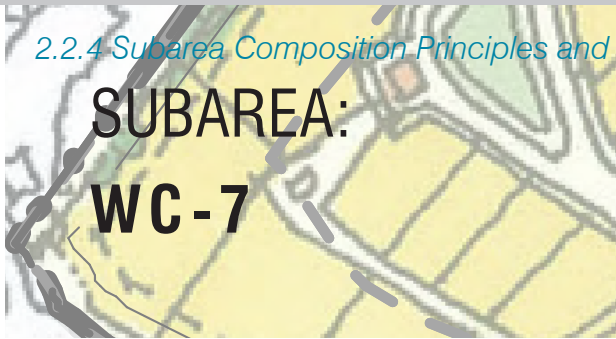
SUBAREA DEVELOPMENT

| | |
|------------------------------------|---------------|
| Subarea Development Maximums Total | |
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 24 UNITS |
| INSTITUTIONAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | NOT ALLOWABLE |

| | |
|---------------------------------|---|
| Subarea Open Space Minima Total | |
| SHARED OPEN SPACE | 0 |
| CONSERVED GREEN SPACE | 0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is restricted to residential uses of a similar scale and character to neighboring properties including single family and accessory residential units.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Compatible Site Disposition** – The minimum frontage (150 feet for single family residences) and minimum setback (50 feet rear or side setback, as indicated on the Land Use Plan) provide an overall scale of lots and deployment of buildings that is compatible and respectful of the adjacent context.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, Pillsbury Road is the primary street network. Parking is not required along this segment of the primary network.

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.

PEDESTRIAN NETWORK Pedestrian networks will

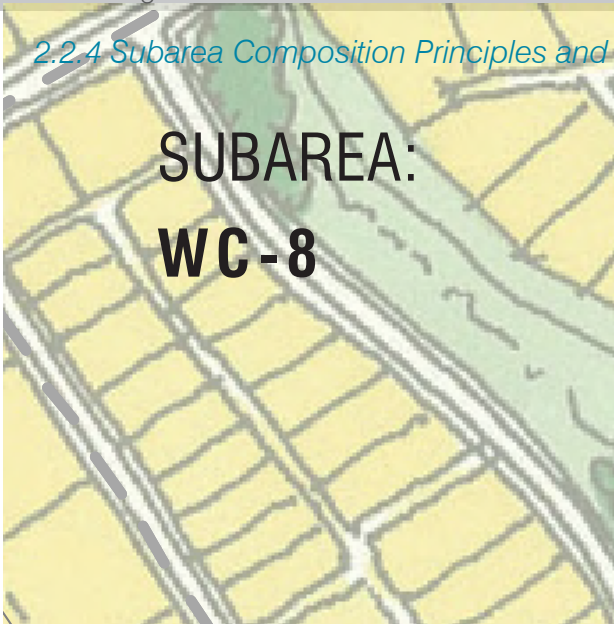
be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Bike paths will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both.

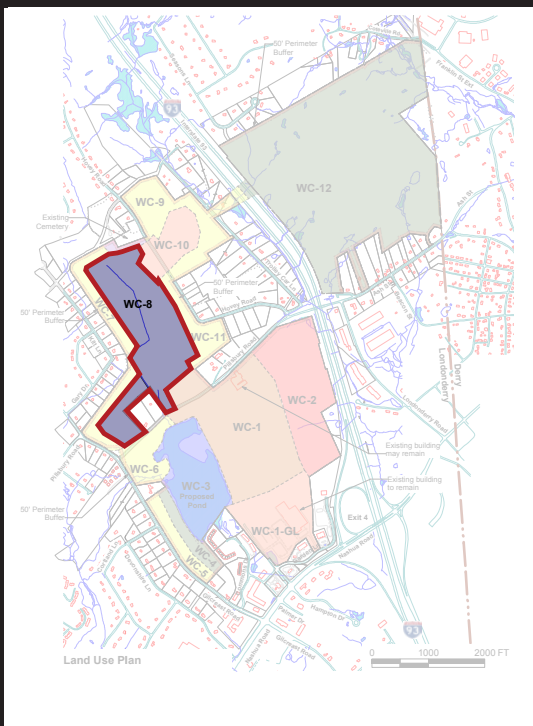
PARKING On-street parking shall be provided on the internal street network.

OPEN SPACES No minimum open space requirements are defined within this Subarea as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards



INTENT. The purpose of this Subarea is to allow for a mix of higher density residential uses with significant conserved green space. Development patterns will include a variety of residential buildings and lot sizes, integrated with a pedestrian-friendly central corridor that includes publicly accessible conserved green space. Some retail and commercial uses will be allowed near the village center in WC-1 and the neighborhood center in WC-10.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

| | | |
|---|----------------|---|
| 1 | Boulevard | A |
| 2 | Avenue | A |
| 3 | Two-way Street | A |
| 4 | One-way Street | A |
| 5 | Access Lane | A |

BLOCK TYPES (SECTION 2.3.2)

| | | |
|---|------------------------|---|
| 1 | Village Center | A |
| 2 | Neighborhood | A |
| 3 | Flex | |
| 4 | Perimeter | |
| 5 | Perimeter Neighborhood | |

OPEN SPACE TYPES (SECTION 2.3.6)

| | | |
|---|-----------------------|---|
| 1 | Conserved Green Space | A |
| 2 | Green | A |
| 3 | Square | A |
| 4 | Plaza | |
| 5 | Playing Field | A |
| 6 | Playground | A |
| 7 | Park | A |
| 8 | Shared Use Path | A |
| 9 | Recreation Trails | A |

BUILDING AND LOT TYPES (SECTION 2.4.2)

| | | |
|----|------------------------------|---|
| 1 | Single Family Detached | A |
| 2 | Single-Family Attached | A |
| 3 | Two-Family | A |
| 4 | Multi-family | A |
| 5 | Small Building | A |
| 6 | Medium Building | |
| 7 | Large Building | |
| 8 | Civic Building | A |
| 9 | Dedicated Office Building | |
| 10 | Large Format Retail Building | |
| 11 | Institutional Building | A |
| 12 | Parking Structure | |
| 13 | Flex Building | |

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA LAND USE CHARACTERISTICS

- Developable area, typical
- Possible trail connection
- Secondary Street – New: 2-way Street: 2-lane
- Primary Street – New: 2-way Street: 2-lane
- Primary Street – New Boulevard, Avenue or 2-way Street: 2-4-lane
- Shared Open Space – Passive with a trail system connecting to Green Space
- Conserved Green Space, with a trail system connecting to Subarea WC-3
- Secondary Street – New: 2-way Street: 2-lane
- Perimeter Buffer
- Pillsbury Road, possible relocation
- Pillsbury Road, possible new realignment
- Primary Street – Existing: Boulevard: 2-lane or Avenue: 2-lane

All locations of Allowable Land Use components are approximate
 Detention areas may be in other Subareas



SUBAREA WC-8

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street as identified in the Master TIA
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

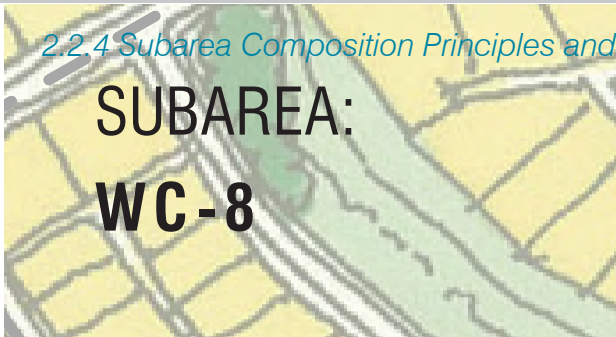
SUBAREA DEVELOPMENT

| | |
|------------------------------------|---------------|
| Subarea Development Maximums Total | |
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 230 UNITS |
| INSTITUTIONAL | 20,000 GSF |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | 5,000 GSF |

| | |
|---------------------------------|------|
| Subarea Open Space Minima Total | |
| SHARED OPEN SPACE | 1.0 |
| CONSERVED GREEN SPACE | 10.0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different residential uses supported by small neighborhood retail and service uses as the Subarea approaches WC-1 and WC-10. The overall composition of streets, blocks and open spaces will include the following principles:

- **Variety** – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses. The division of land and infrastructure should support this variety.
- **Circulation and Open Space Corridor** – This Subarea will include a corridor of conserved and Passive Open Space integrated with a Primary Street Network and a system of integrated recreation trails and independent use paths.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network includes Pillsbury Road and transitions into the neighborhood. Parking is allowed along these segments of the primary network.

SECONDARY STREET NETWORK The secondary network should be designed to create developable blocks or to outline planned open space. Parking can serve as a shared parking resource for commercial, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Primary Network** – The public frontages should be landscaped to reinforce the characteristics of the network in this area. The public frontages along Pillsbury Road should include a planting strip between the street and the sidewalk except when next to retail, mixed use or commercial uses.
- **Frontages along the Secondary Network** – The public frontages along the secondary network should be consistent

with the primary intended ground level use and its relationship to on-street parking. For segments intended to have predominately commercial, civic, accommodation or institutional uses with short term on-street parking, landscaped borders should be limited and sidewalk paving generally extended to the street edge. For predominately residential segments or areas where on-street parking is not expected to serve as a short-term supply, the frontages should be landscaped.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic accommodation or institutional uses. The pedestrian network should connect uses with open space.

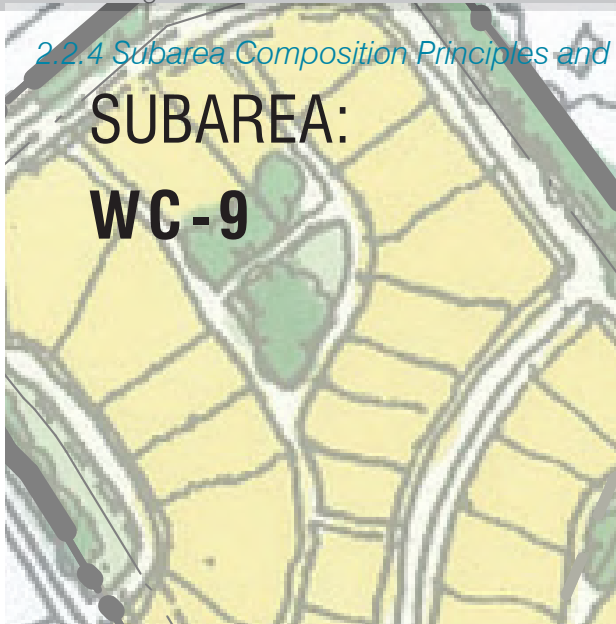
BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING On-street parking should be provided to allow for guest parking and public access to Shared Open Space. Off-street parking may be required for other uses.

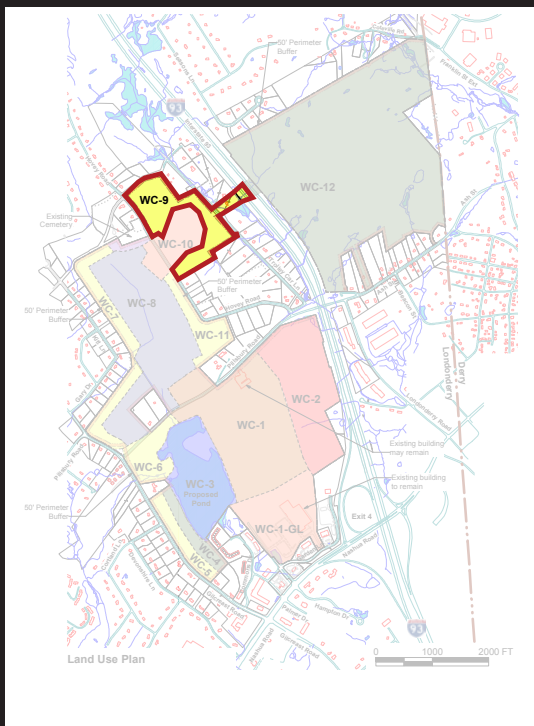
OPEN SPACES Shared open space should be comprised of at least two (2) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-9**



INTENT. The purpose of this Subarea is to allow for a residential neighborhood that is compatible with the context of existing residential properties. Development patterns would allow increasing density near the internal Subarea boundaries.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

| | | |
|---|----------------|---|
| 1 | Boulevard | |
| 2 | Avenue | |
| 3 | Two-way Street | A |
| 4 | One-way Street | A |
| 5 | Access Lane | A |

BLOCK TYPES (SECTION 2.3.2)

| | | |
|---|------------------------|---|
| 1 | Village Center | |
| 2 | Neighborhood | A |
| 3 | Flex | |
| 4 | Perimeter | |
| 5 | Perimeter Neighborhood | A |

OPEN SPACE TYPES (SECTION 2.3.6)

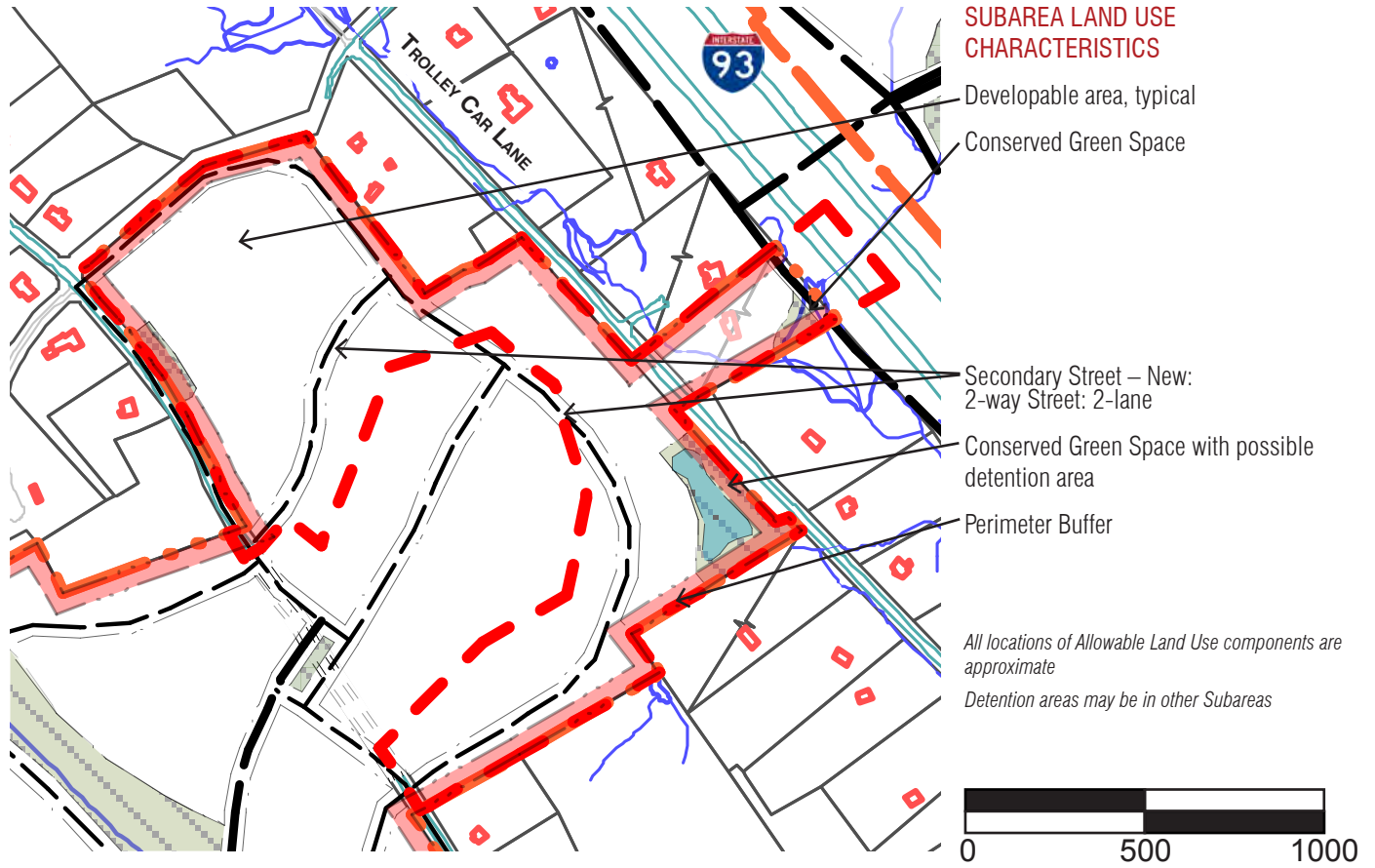
| | | |
|---|-----------------------|---|
| 1 | Conserved Green Space | A |
| 2 | Green | A |
| 3 | Square | A |
| 4 | Plaza | |
| 5 | Playing Field | A |
| 6 | Playground | A |
| 7 | Park | A |
| 8 | Shared Use Path | A |
| 9 | Recreation Trails | A |

BUILDING AND LOT TYPES (SECTION 2.4.2)

| | | |
|----|------------------------------|---|
| 1 | Single Family Detached | A |
| 2 | Single-Family Attached | A |
| 3 | Two-Family | A |
| 4 | Multi-family | A |
| 5 | Small Building | |
| 6 | Medium Building | |
| 7 | Large Building | |
| 8 | Civic Building | A |
| 9 | Dedicated Office Building | |
| 10 | Large Format Retail Building | |
| 11 | Institutional Building | |
| 12 | Parking Structure | |
| 13 | Flex Building | |

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-9

SUBAREA DEVELOPMENT

Subarea Development Maximums Total

| | |
|----------------|---------------|
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 52 UNITS |
| INSTITUTIONAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | NOT ALLOWABLE |

Subarea Open Space Minima Total

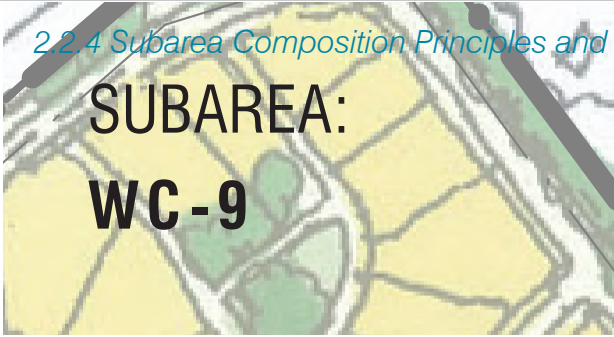
| | |
|-----------------------|-----|
| SHARED OPEN SPACE | 0.5 |
| CONSERVED GREEN SPACE | 0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- Compatible Use – This is an area that is restricted to residential uses of a similar scale and character to neighboring properties including single family and accessory residential units, two-family or multi-family.
- Compatible Height – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- Perimeter Buffers – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Uses not compatible with adjoining existing residential uses must be screened with landscaping.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Secondary Network – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

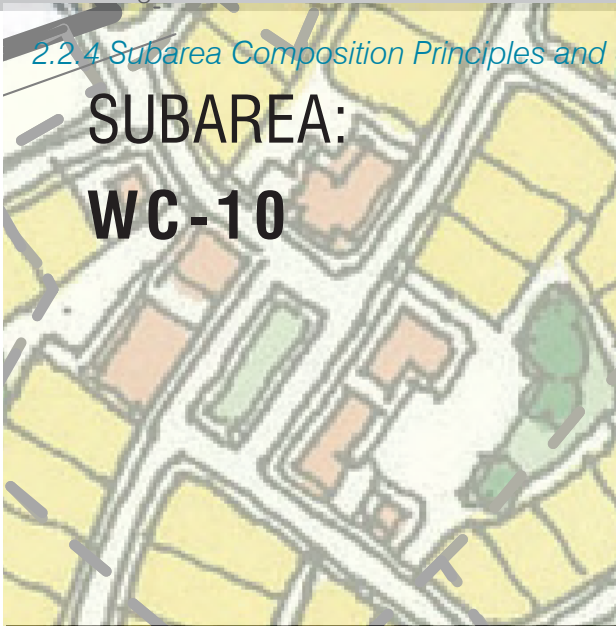
BICYCLE NETWORK Bike paths will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both.

PARKING On-street parking shall be provided on the internal street network.

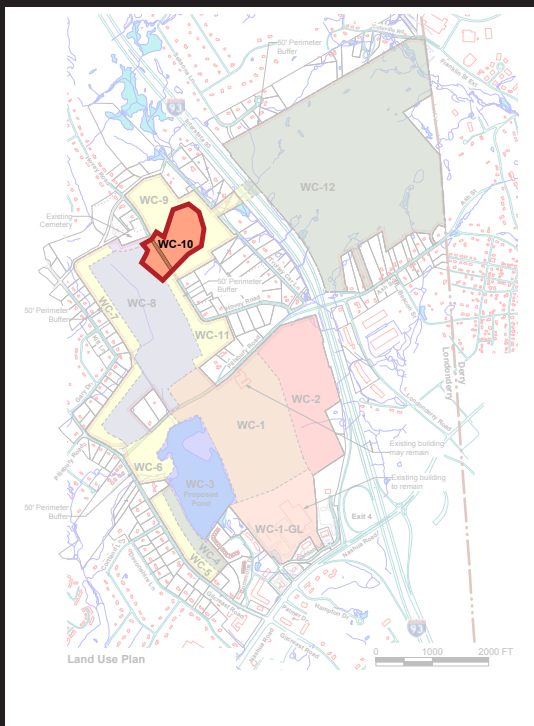
OPEN SPACES Shared open space should be comprised of at least two (2) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-10**



INTENT. The purpose of this Subarea is to allow for a mix of medium density residential uses with a cluster of neighborhood services, retail and commercial uses. The development pattern supports compact and well-connected pedestrian-oriented segments along the streets and sidewalks, with smaller retail, mixed-use or commercial uses.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard **A**
- 2 Avenue **A**
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center **A**
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood

OPEN SPACE TYPES (SECTION 2.3.6)

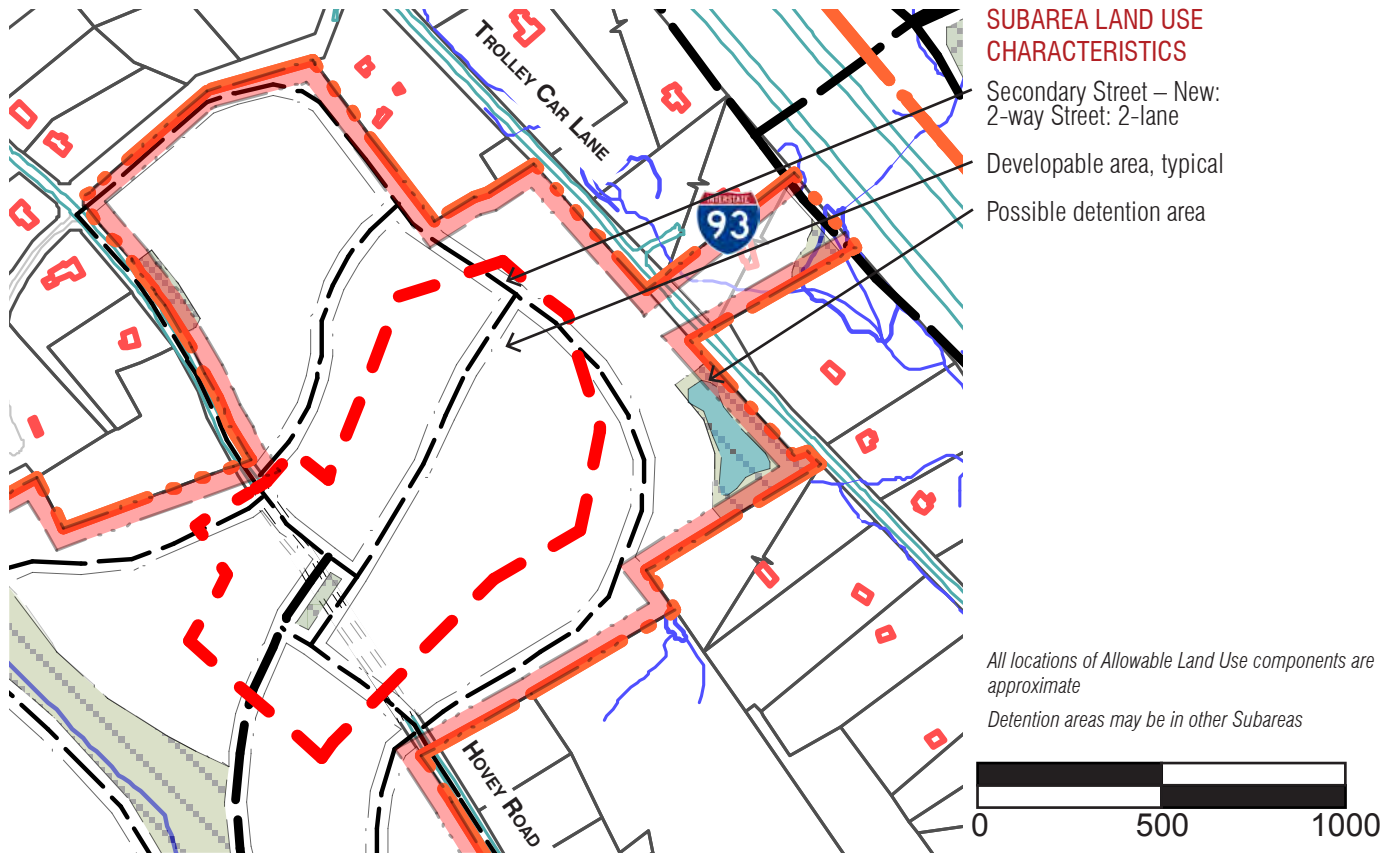
- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza **A**
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached **A**
- 3 Two-Family **A**
- 4 Multi-family **A**
- 5 Small Building **A**
- 6 Medium Building **A**
- 7 Large Building
- 8 Civic Building **A**
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building **A**
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-10

SUBAREA DEVELOPMENT

| Subarea Development Maximums Total | |
|------------------------------------|---------------|
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 60 UNITS |
| INSTITUTIONAL | 20,000 GSF |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | 10,000 GSF |

| Subarea Open Space Minima Total | |
|---------------------------------|-----|
| SHARED OPEN SPACE | 0.5 |
| CONSERVED GREEN SPACE | 0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



SUBAREA: WC-10

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different uses assembled to create a compact neighborhood center within the overall Woodmont Commons PUD. The overall composition of streets, blocks and open spaces will include the following principles:

- **Variety** – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses, and the division of land and infrastructure should support this variety.
- **Neighborhood Center** – The neighborhood center will allow small retail and commercial uses then transition to residential uses at the Subarea perimeter.
- **Compact and Connected Development Patterns for Retail, Commercial Uses and Mixed-use** – The development patterns should support compact and well-connected pedestrian-oriented segments along the streets and sidewalks, connected to the primary street network, and with smaller retail, commercial uses or mixed-use at the neighborhood center. The scale and intensity transition from a neighborhood center to a residential neighborhood.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary network should be designed to create developable blocks or to outline planned open space. Parking can serve as a shared parking resource for commercial, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to on-street parking. For segments intended to have predominately business, civic, accommodation or institutional uses with short term on-street parking, planting strips should be limited and sidewalk paving

generally extended to the street edge. For predominately residential segments or areas where on-street parking is not expected to serve as a short-term supply, the frontages should be landscaped.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic accommodation or institutional uses. The pedestrian network should connect uses with open space.

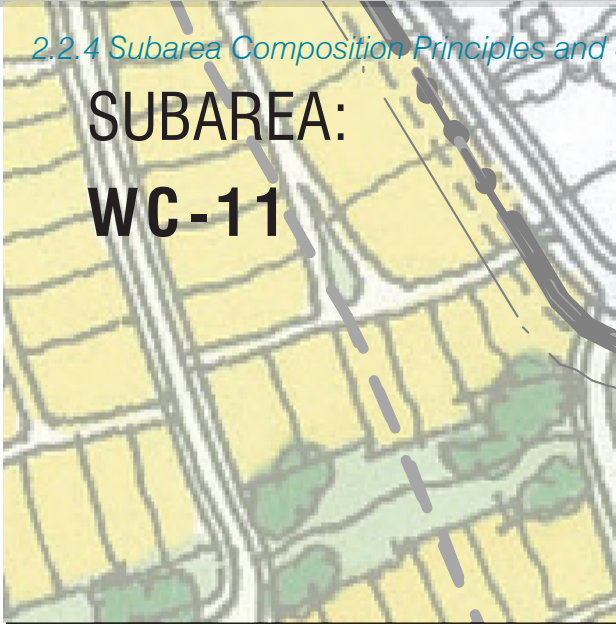
BICYCLE NETWORK Bike paths will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both.

PARKING On-street parking may be considered to contribute to fulfilling the parking requirements of development within 400 feet of the parking spaces for non-residential uses. For residential uses, on-street parking may be allocated for required visitor spaces.

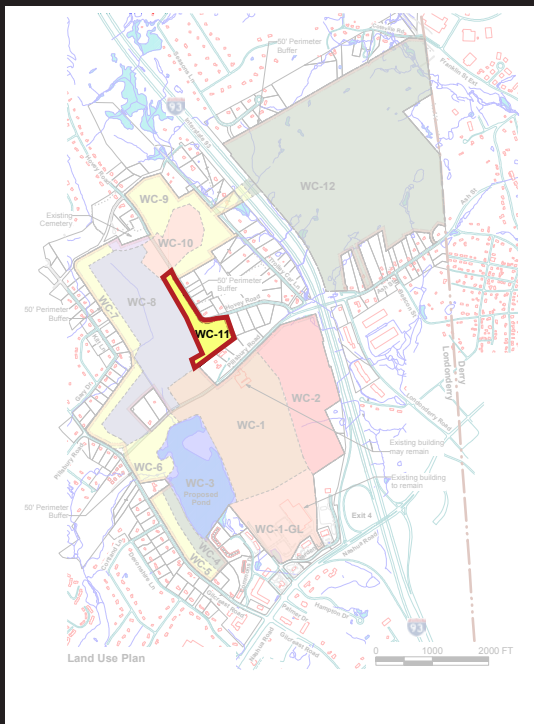
OPEN SPACES Shared open space should be comprised of at least one (1) separate location and one (1) separate publicly-accessible open space type as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-11**



INTENT. The purpose of this Subarea is to create a residential neighborhood that is compatible with the context of existing residential properties. Development patterns would allow increasing density near the internal Subarea boundaries.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached **A**
- 3 Two-Family **A**
- 4 Multi-family **A**
- 5 Small Building
- 6 Medium Building
- 7 Large Building
- 8 Civic Building **A**
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA LAND USE CHARACTERISTICS

- Secondary Street – New: 2-way Street: 2-lane
- Developable area, typical
- Open Space – Passive – Green with trail connection to WC-8
- Perimeter Buffer
- Secondary Street – New: 2-way Street: 2-lane

All locations of Allowable Land Use components are approximate
 Detention areas may be in other Subareas



SUBAREA WC-11

SUBAREA DEVELOPMENT

Subarea Development Maximums Total

| | |
|----------------|---------------|
| AGRICULTURAL | NO LIMIT |
| RESIDENTIAL | 24 UNITS |
| INSTITUTIONAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT |
| ACCOMMODATIONS | NOT ALLOWABLE |
| COMMERCIAL USE | NOT ALLOWABLE |

Subarea Open Space Minima Total

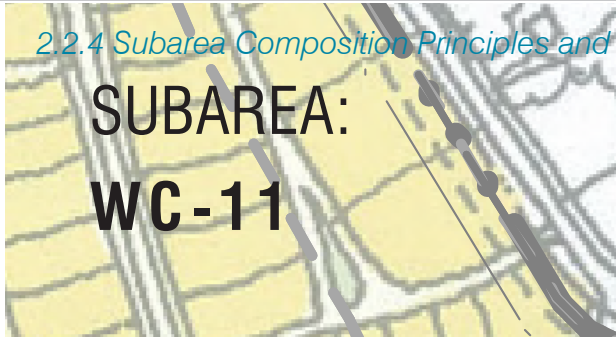
| | |
|-----------------------|---|
| SHARED OPEN SPACE | 0 |
| CONSERVED GREEN SPACE | 0 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- Compatible Use – This is an area that is restricted to residential uses of a similar scale and character to neighboring properties including single family and accessory residential units.
- Compatible Height – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- Perimeter Buffers – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Uses not compatible with adjoining existing residential uses must be screened with landscaping.

OPEN SPACES No minimum opens space requirements are defined within this Subarea as indicated by the Land Use Plan.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Secondary Network – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.

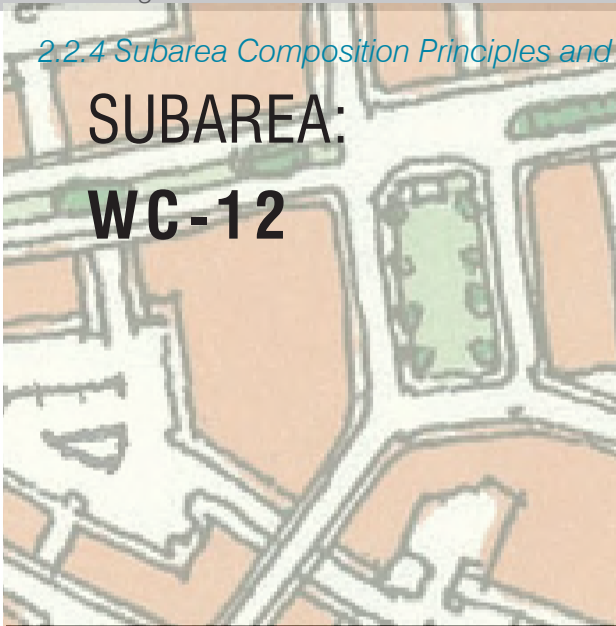
PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Bike paths will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both.

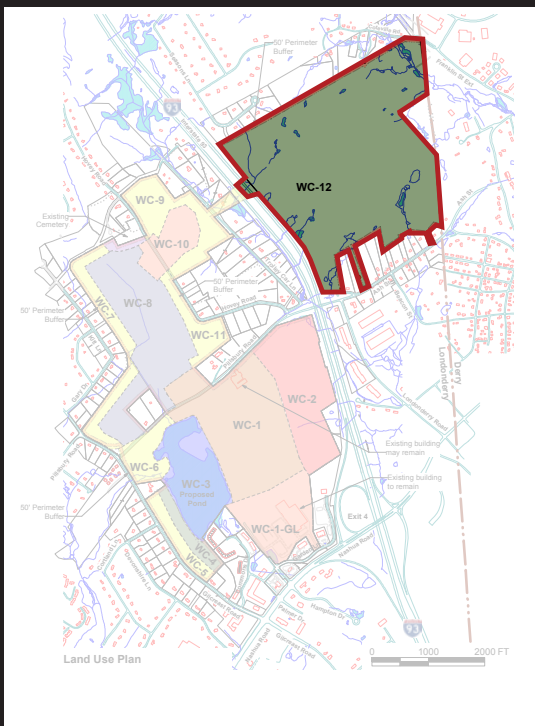
PARKING On-street parking shall be provided on the internal street network.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-12**



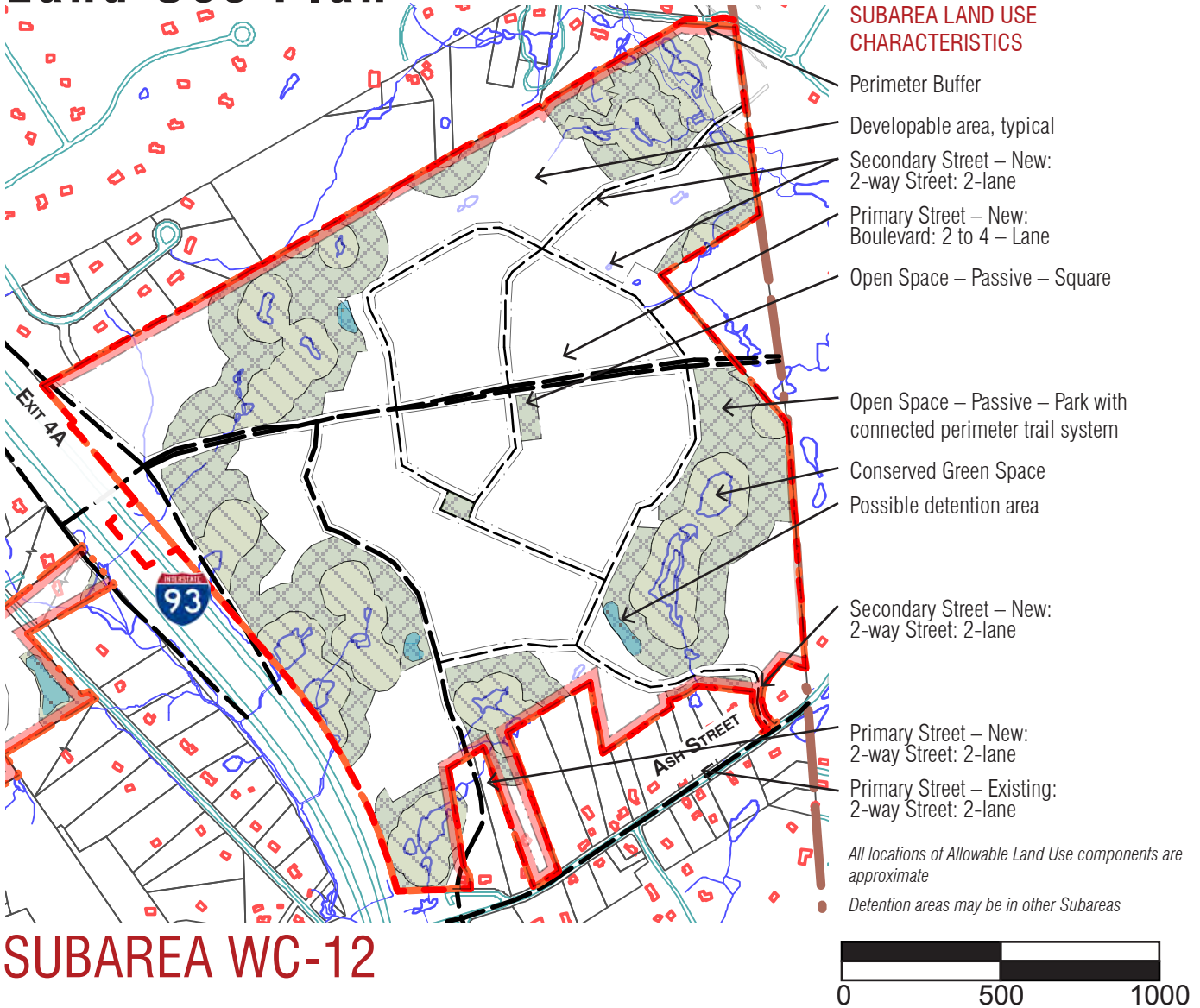
INTENT. The purpose of this Subarea is to create a regional mixed-use district supporting larger retail, commercial, institutional and office uses with some residential uses. Development patterns will be oriented toward connecting vehicular traffic to existing road networks to access offices and large retail, but pedestrian-oriented infrastructure will connect residential areas with publicly accessible open space and neighborhood supporting retail and services.



| Regulating Type | Allowed |
|---|---------|
| STREET TYPES (SECTION 2.3.3) | |
| 1 Boulevard | A |
| 2 Avenue | A |
| 3 Two-way Street | A |
| 4 One-way Street | A |
| 5 Access Lane | A |
| BLOCK TYPES (SECTION 2.3.2) | |
| 1 Village Center | A |
| 2 Neighborhood | A |
| 3 Flex | A |
| 4 Perimeter | A |
| 5 Perimeter Neighborhood | A |
| OPEN SPACE TYPES (SECTION 2.3.6) | |
| 1 Conserved Green Space | A |
| 2 Green | A |
| 3 Square | A |
| 4 Plaza | A |
| 5 Playing Field | A |
| 6 Playground | A |
| 7 Park | A |
| 8 Shared Use Path | A |
| 9 Recreation Trails | A |
| BUILDING AND LOT TYPES (SECTION 2.4.2) | |
| 1 Single Family Detached | A |
| 2 Single-Family Attached | A |
| 3 Two-Family | A |
| 4 Multi-family | A |
| 5 Small Building | A |
| 6 Medium Building | A |
| 7 Large Building | A |
| 8 Civic Building | A |
| 9 Dedicated Office Building | A |
| 10 Large Format Retail Building | A |
| 11 Institutional Building | A |
| 12 Parking Structure | A |
| 13 Flex Building | A |

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA DEVELOPMENT

Subarea Development Maximums Total

| | WITH EXIT 4A | WITHOUT EXIT 4A |
|----------------|------------------------------|-----------------|
| AGRICULTURAL | NO LIMIT | NO LIMIT |
| RESIDENTIAL | 353 UNITS | 350 UNITS |
| INSTITUTIONAL | 420,000 GSF/300 BED HOSPITAL | NOT ALLOWABLE |
| CIVIC | NO LIMIT | NO LIMIT |
| ACCOMMODATIONS | 200 HOTEL ROOMS | NOT ALLOWABLE |
| COMMERCIAL USE | 700,000 GSF | 400,000 GSF |

Subarea Open Space Minima Total

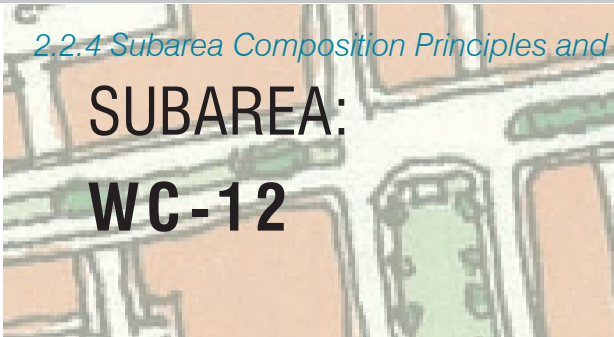
| | |
|-----------------------|------|
| SHARED OPEN SPACE | 40.0 |
| CONSERVED GREEN SPACE | 36.5 |

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



SUBAREA: WC-12

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different uses assembled to create a compact area within the overall Woodmont Commons PUD. The overall composition of streets, blocks and open spaces will include the following principles:

- Variety – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses, and the division of land and infrastructure should support this variety.
- Diversity of open spaces – There are a range of open space types that are located within the area to provide different types of opportunities and amenities.
- Development along I-93 – The development patterns may support large buildings for offices or other uses that transition in use and scale from the boundary with I-93 and along the primary street to more residential uses. Parking can be placed along this boundary.
- Expanding the Current Development Patterns for Retail, Commercial Uses and Mixed-use – The development patterns should support vehicular access to the large format retail, Flex Space, mixed-use and commercial uses within this Subarea.
- Perimeter Buffers – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Uses not compatible with adjoining existing residential uses must be screened with landscaping.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network provides an east/west connection to the proposed Exit 4A as a boulevard. Parking is not required along this segment of the primary network, but may be provided in either pockets or complete segments.

SECONDARY STREET NETWORK The secondary network should be designed to create developable blocks as indicated on the Land Use Plan or to outline planned open space and incorporate on-street parking where it can serve as a shared parking resource for commercial use, retail, mixed-use, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate

traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Primary Network – The public frontages will have street trees to reinforce the characteristics of the network in this area.
- Frontages along the Secondary Network – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to on-street parking. Areas within this Subarea will have vehicle-oriented uses. For segments intended to have predominately commercial use, retail, mixed-use, civic, accommodation, residential or institutional uses with short term on-street parking, planting strips should be limited and sidewalk paving generally extended to the street edge. For predominately residential segments or areas where on-street parking is not expected to serve as a short-term supply, the frontages should be landscaped.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic, accommodation or institutional uses. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING On-street parking may be considered to contribute to fulfilling the parking requirements of commercial or mixed-use development within 400 feet of the parking spaces for non-residential uses. For residential uses, on-street parking may be allocated for required visitor spaces. Off-street parking should have landscape screening along the public frontage.

OPEN SPACES Shared open space should be comprised of at least four (4) separate locations and up to four (4) separate publicly-accessible open space types as indicated by the Land Use Plan. A trail system around this Subarea may be connected in part by public sidewalks.

2.3 PUD SUBDIVISION REGULATIONS AND STANDARDS

2.3.1 General

The PUD Subdivision Regulations and Standards contain the rules used to review and approve proposals that will be submitted for PUD Subdivision approvals by the Town of Londonderry. Because PUD Subdivision Plans within the Woodmont Commons PUD are regulated by the PUD Master Plan and under the control of the Developer, submittals will require review and written approval for consistency with the PUD Master Plan by the Developer prior to being submitted to the Town for review. Submittals may also be made by the Developer, and as such, are subject to the same confirmation standards.

PUD Subdivision Plans include infrastructure projects that establish new property lines and appropriate access and service, prior to individual building or site-specific improvement projects, which are addressed in *Section 2.4 PUD Site Plan Regulations and Standards*. PUD Subdivision Plans are subject to the following:

- PUD Subdivision Plans may be submitted for portions of Subareas, for entire Subareas or for land contained within multiple Subareas.
- PUD Subdivision Plans may include buildings or other site-specific improvements as part of their submittal. In such cases, the submittals shall be governed by this Section and the buildings or other site-specific improvements shall be governed by *Section 2.4 PUD Site Plan Regulations and Standards*.
- PUD Subdivision Plans must comply with *Section 2.3 PUD Subdivision Regulations and Standards* and with the Allowable Uses and Use and Distribution Standards contained within *Section 2.2 Land Use Regulations*.
- All PUD Plans must address the requirements of this Section including how the submittal will address Open Space requirements.

- Submittals to or by the Developer shall be in accordance with this Section and follow the submittal process outlined in *Section 2.5 Administration*.

Once approval is received from the Developer, submittals to the Town shall be in accordance with the *Londonderry Site Plan Regulations* and/or the *Londonderry Subdivision Regulations*, current as of the date of the Submittal and as modified herein. No submittal to the Town will be accepted without Developer approval.

Note that the Compliance Alternatives in *Section 2.2.1 Description* apply to the PUD Subdivision Regulations and Standards.

2.3.2 Block Types

The Woodmont Commons PUD has five allowable Block Types. Each block has a separate purpose. The Block Types are as follows:

- Village Center;
- Neighborhood;
- Flex;
- Perimeter;
- Perimeter Neighborhood;

The following pages describe each Block Type and provide the relevant standards, including dimensional standards, the Subareas in which the Block Type is allowed, and how vehicles access the block from the street.

Block Assembly Rules

The Block Types are to be combined with the Street Types and Open Space Types to form a PUD Subdivision. The Block Types indicate which Subarea the Block Type is permitted to be used within. Perimeter Blocks and Perimeter Neighborhood Blocks are intended to be used along the PUD Boundary where connections to the adjacent property is restrained either by adjacent lots, a physical constraint (i.e. I-93) or the need to reduce the amount of connectivity to adjacent residential areas.

Open Space

Open space, as defined in *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*, including Conserved Green Space and Shared Open Space may be incorporated into any Block Type, or may be contained within an independent block without restrictions on the size and dimension of the block.

Surface Parking

Surface parking may be located within the Private Frontage on Flex blocks, or portions thereof, in Subareas WC-1-GL and WC-2, and in any block in

Subarea WC-12 subject to the following:

- Surface parking is not permitted within the Private Frontage, except as permitted in *Section 2.4.2 Buildings and Lots*, on a Primary street as delineated on the Land Use Plan in *Section 2.2.2 Land Use Plan* and *Section 2.2.4 Subarea Composition Principles and Standards*;
- The use associated with the surface lot must be non-residential or multi-family;
- The Lot Type setbacks for building placement on lot and parking placement on lot as noted in *Section 2.4.2 Buildings and Lots* shall be increased to allow for parking;
- New surface parking shall be set back from the right-of-way a minimum of six (6) feet;
- New Public Frontage shall meet the requirements of *Section 2.3.3 Transportation Network* for the use(s) being served;
- New surface parking may be located to the front of buildings in Flex Blocks and shall be screened and landscaped in accordance with *Section 2.4.6 PUD Site Plan Landscape*;
- Parking may be designed so that buildings may be accommodated over time.

Other Assembly Rules

Other assembly rules include the following:

- Village Blocks with block faces exceeding 500 feet shall have a Mid-Block crossing;
- Any block face over 1,000 feet shall include a Mid-Block pedestrian crossing or other speed management devices;
- Any block dimension can be increased by 50% if a Mid-Block crossing is connected to an internal Recreation Trail;
- Blocks may be separated by Recreation Trails rather than streets;
- Speed management devices on a Perimeter

Block or Perimeter Neighborhood Block may include a cross street that intersects with the street along the Block but that does not intersect with the Block itself;

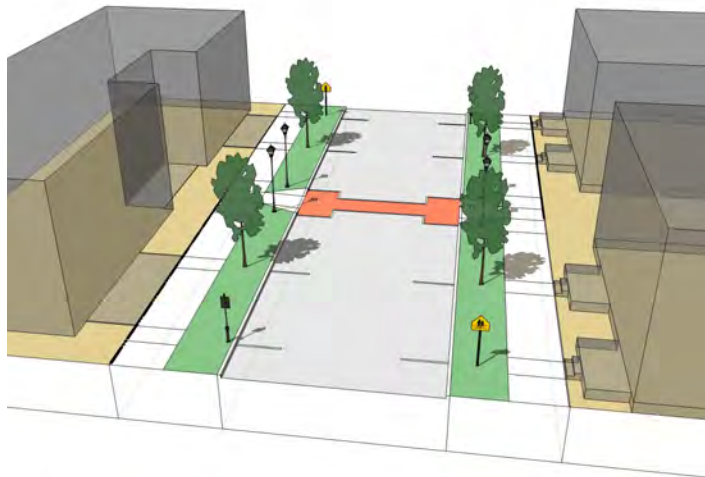
- Blocks abutting or containing Conserved Green Space may be increased in length to preserve the Conserved Green Space.

Speed Management Devices

Speed management devices include methods from *Designing Walkable Urban Thoroughfares: A Context Sensitive Approach, An ITE Recommended Practice, 2010*.

Active methods include:

- Lateral shifts or narrowing (curb extensions with a center island or other techniques that require vehicles to move out of a straight path or create neckdowns);
- Smaller curb-return radii to slow turning vehicles and the elimination of free-flow channelized right-turn lanes;
- Provision of on-street parking where adjacent land uses and activities will generate demand.
- Speed cushions or speed platforms (less impact



Example of a Mid-Block Crossing

A mid-block crossing provides a safe method for pedestrians to cross when a longer block is required. A mid-block crossing may connect to another Block Type, a Recreation Trail or a Shared Use Path.

on emergency vehicles than hump and tables);

- Narrowed travel lanes;
- Raised crosswalks combined with curb extensions to narrow street.

Passive methods include:

- Visually narrowing road using pavement markings;
- Visually enclosing street with buildings, landscaping and street trees.

Mid-Block Crossings

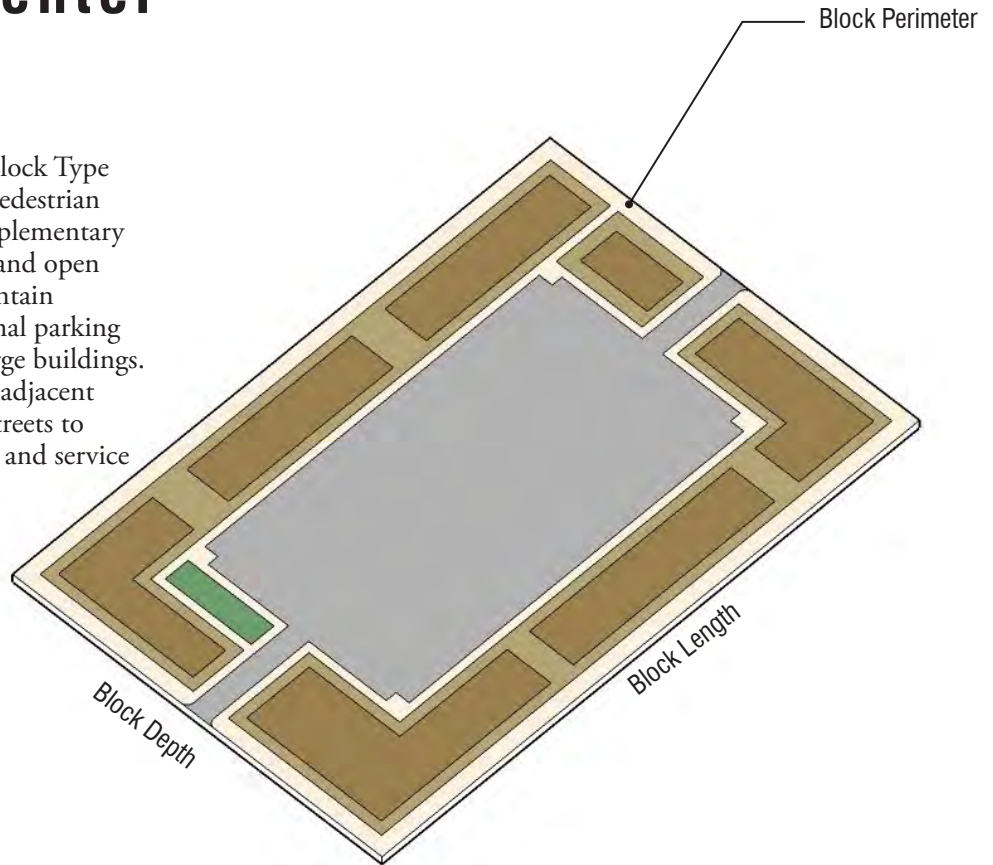
Mid-Block crossing criteria include:

- Mid-Block crosswalks should be located at least 100 feet from an intersection;
- May be located on streets with an average daily traffic volume (ATD) of 12,000 vehicles per day or less;
- May be located on multi-lane streets with less than 15,000 ATD if a raised pedestrian refuge median is provided;
- Serves a minimum pedestrian crossing volume of 25 pedestrians per hour for at least four hours of a typical day;
- Note that the requirement for speed management devices may be omitted if found by the Town to be in conflict with public safety access interests or public maintenance requirements.

2.3.2 Block Types

BLOCK TYPE: Village Center

DESCRIPTION This Block Type will create an attractive pedestrian environment with a complementary mix of buildings, streets and open space. The Block may contain perimeter lots with internal parking for a range of small to large buildings. Site circulation connects adjacent primary and secondary streets to internal parking, loading and service areas.

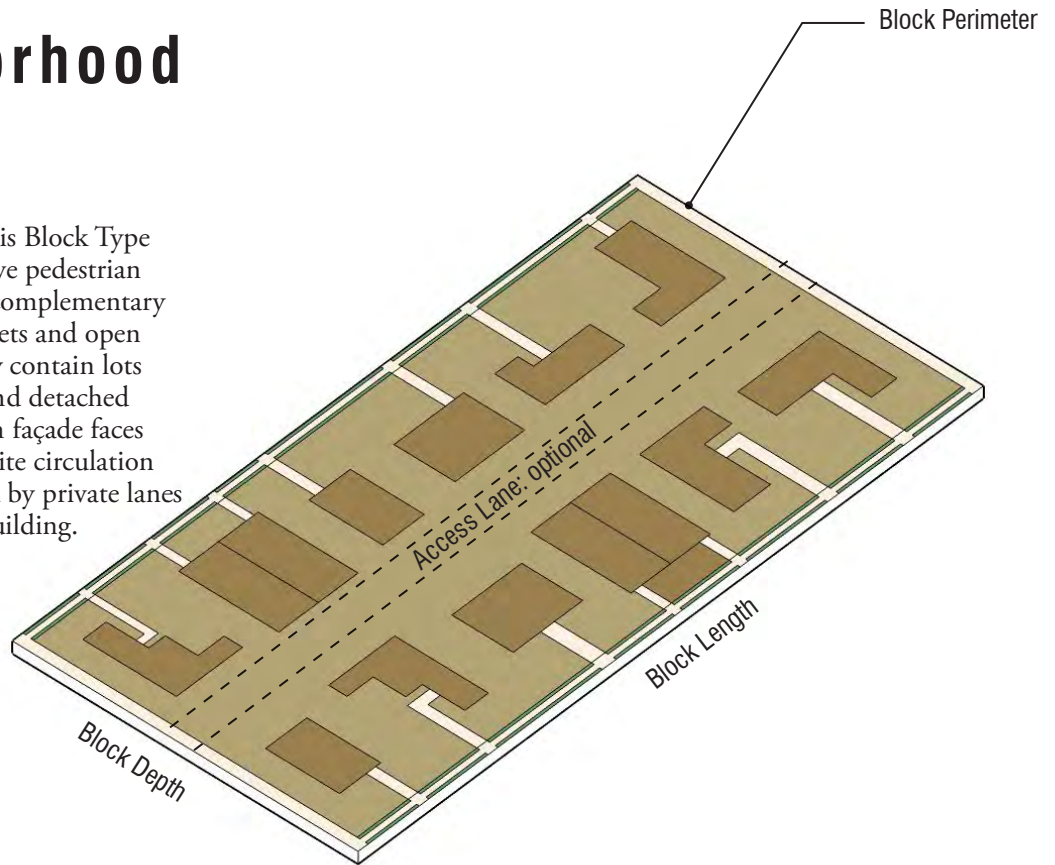


| SIZE AND DIMENSION | |
|---------------------------|--|
| Block Perimeter – Maximum | 2,400 linear feet 3,500 linear feet with structured parking in WC-12 only |
| Block Face – Maximum | 600 feet |
| ACCESS AND SERVICE | |
| Primary Entry Orientation | To street |
| Service Area/Route | Block can be permeated by Access Lanes and pedestrian passage On-street loading permitted |
| Pedestrian Circulation | Block length over 400' shall provide mid-block crossing/passage |
| OPEN SPACE | |
| Open Space Types Allowed | All - no limit |
| Open Space Required | Refer to PUD Subarea rules and requirements |
| SUBAREAS | |
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-8, WC-10, WC-12 |

2.3.2 Block Types

BLOCK TYPE: Neighborhood

DESCRIPTION This Block Type will create an attractive pedestrian environment with a complementary mix of buildings, streets and open space. The Block may contain lots with both attached and detached buildings whose main façade faces the principal street. Site circulation may be accomplished by private lanes located behind the building.

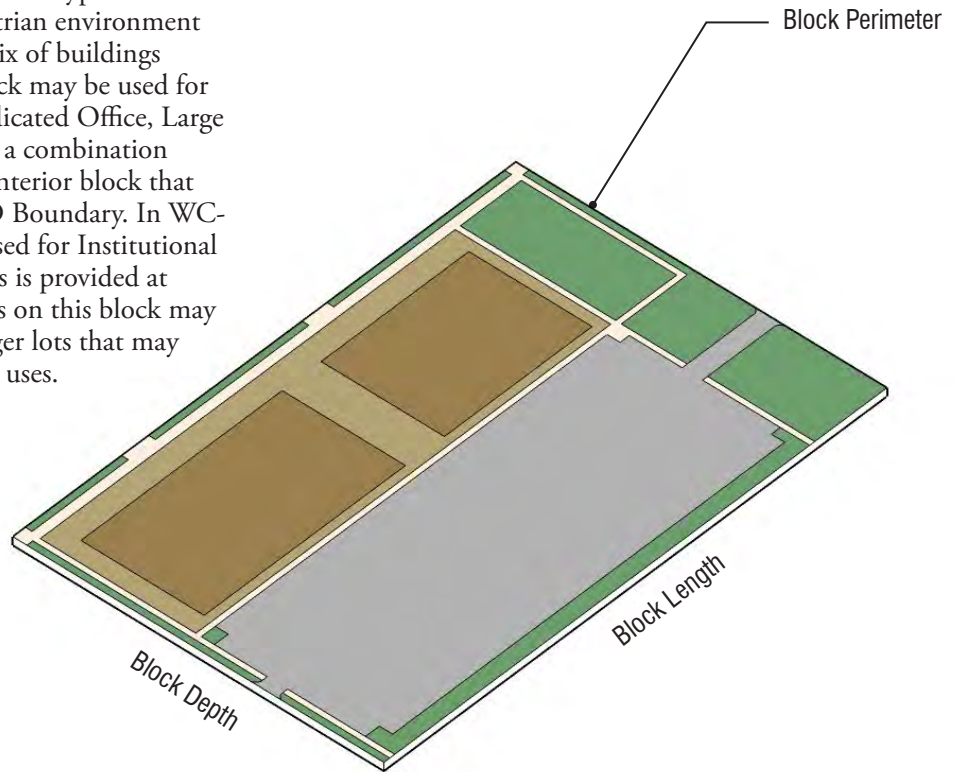


| SIZE AND DIMENSION | |
|---------------------------|---|
| Block Perimeter – Maximum | 3,000 linear feet |
| Block Face – Maximum | 750 feet |
| ACCESS AND SERVICE | |
| Primary Entry Orientation | To street |
| Service Area/Route | Block can be permeated by Access Lanes and pedestrian passage |
| Pedestrian Circulation | At block perimeter and intersections |
| OPEN SPACE | |
| Open Space Types Allowed | All - no limit |
| Open Space Required | Refer to PUD Subarea rules and requirements |
| SUBAREAS | |
| Allowed in Subarea(s) | WC-1, WC-2, WC-4, WC-6, WC-7, WC-8, WC-9, WC-10, WC-11, WC-12 |

2.3.2 Block Types

BLOCK TYPE: Flex

DESCRIPTION This Block Type will create an attractive pedestrian environment with a complementary mix of buildings and streetscapes. The Block may be used for Large Format Retail, Dedicated Office, Large Buildings, or blocks with a combination of uses. This block is an interior block that should not abut the PUD Boundary. In WC-12, Flex blocks may be used for Institutional Buildings. Vehicular access is provided at adjacent street edges. Lots on this block may be combined to form larger lots that may share building or parking uses.



SIZE AND DIMENSION

| | |
|---------------------------|-------------------|
| Block Perimeter – Maximum | 4,000 linear feet |
| Block Face – Maximum | 800 linear feet |

ACCESS AND SERVICE

| | |
|---------------------------|---|
| Primary Entry Orientation | To street |
| Service Area/Route | Off-street or Access Lane or Internal Parking |
| Pedestrian Circulation | At block perimeter and intersections |

OPEN SPACE

| | |
|--------------------------|---|
| Open Space Types Allowed | All - no limit |
| Open Space Required | Refer to PUD Subarea rules and requirements |

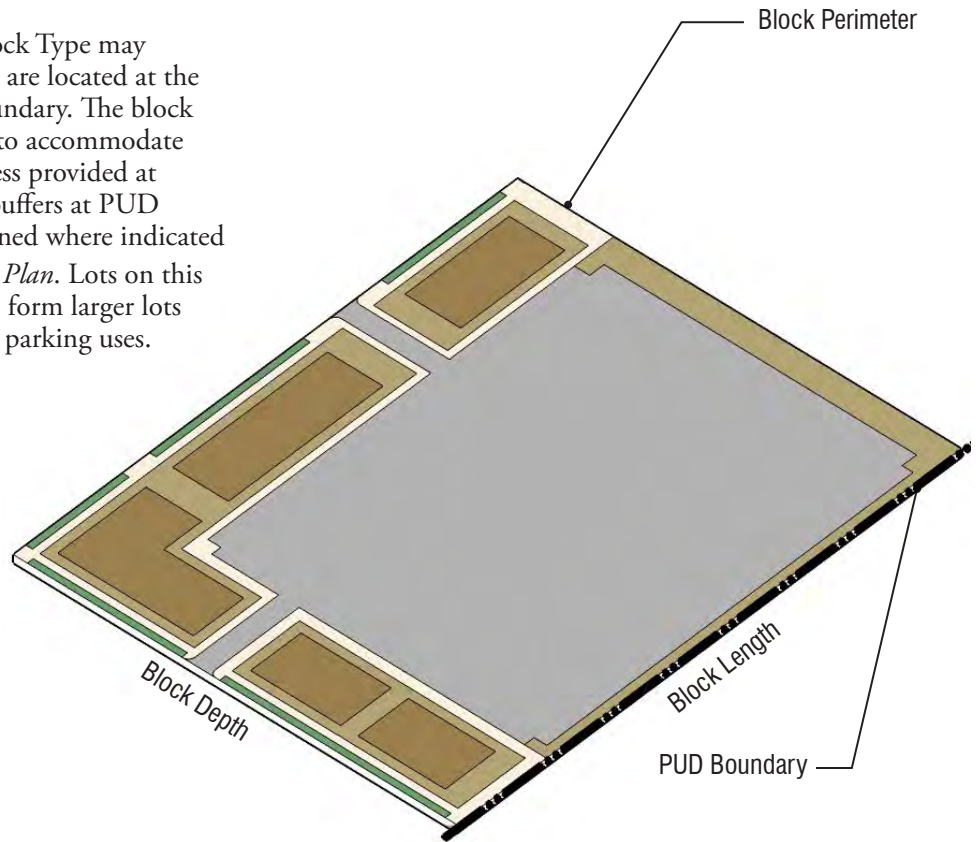
SUBAREAS

| | |
|-----------------------|----------------------------------|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-3, WC-12 |
|-----------------------|----------------------------------|

2.3.2 Block Types

BLOCK TYPE: Perimeter

DESCRIPTION This Block Type may contain perimeter lots that are located at the perimeter of the PUD Boundary. The block may be irregularly shaped to accommodate perimeter conditions. Access provided at adjacent street edges. 50' buffers at PUD Boundary shall be maintained where indicated in *Section 2.2.2 Land Use Plan*. Lots on this block may be combined to form larger lots that may share building or parking uses.

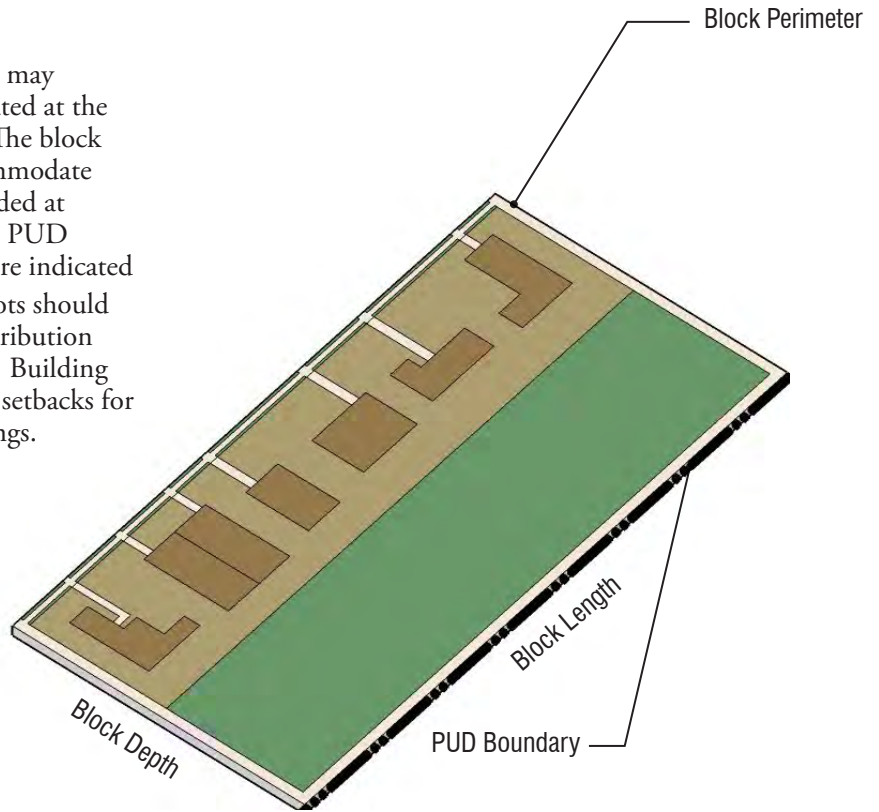


| SIZE AND DIMENSION | |
|---------------------------|---|
| Block Perimeter – Maximum | None |
| Block Face – Maximum | None |
| ACCESS AND SERVICE | |
| Primary Entry Orientation | To street |
| Service Area/Route | Off-street or Access Lane or Internal Parking |
| Pedestrian Circulation | At block perimeter and intersections |
| OPEN SPACE | |
| Open Space Types Allowed | All - no limit |
| Open Space Required | Refer to PUD Subarea rules and requirements |
| SUBAREAS | |
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-12 |

2.3.2 Block Types

BLOCK TYPE: Perimeter Neighborhood

DESCRIPTION This Block Type may contain perimeter lots that are located at the perimeter of the PUD Boundary. The block may be irregularly shaped to accommodate perimeter conditions. Access provided at adjacent street edges. 50' buffers at PUD Boundary shall be maintained where indicated in *Section 2.2.2 Land Use Plan*. Lots should be consistent with the size and distribution of adjacent existing residential lots. Building setbacks should be consistent with setbacks for adjacent existing residential buildings.



| SIZE AND DIMENSION | |
|---------------------------|--|
| Block Perimeter – Maximum | None |
| Block Face – Maximum | None |
| ACCESS AND SERVICE | |
| Primary Entry Orientation | To street |
| Service Area/Route | Block can be permeated by Access Lanes; Residential may be front-loaded |
| Pedestrian Circulation | At block perimeter and intersections |
| OPEN SPACE | |
| Open Space Types Allowed | All - no limit |
| Open Space Required | Refer to PUD Subarea rules and requirements |
| SUBAREAS | |
| Allowed in Subarea(s) | WC-1, WC-2, WC-4, WC-5, WC-6, WC-7, WC-9, WC-11, WC-12 |

2.3.3 Transportation Network

Performance and Operational Standards

The MTIA provided in *Section 4.2 Master Plan Traffic Impact Assessment* provides the baseline and future-year assessments of traffic accommodations for the PUD. The key assumptions are listed as they relate to the study area, trip generation, local and regional distribution of trips, the PUD's relationship to background improvement projects such as Exit 4A, and conceptualized mitigation measures. The planned roadway network within the PUD encourages connectivity between existing and proposed streets as a means of distributing trips and providing route options.

The Woodmont Commons PUD will be constructed over the next 20 years. As PUD Subdivisions and/or PUD Site Plans are proposed, applicants shall submit updated traffic data, if needed, and analysis if the project exceeds thresholds as described in *Section 3.1 Transportation*. The Planning Board shall use the MTIA in *Section 4.2 Master Plan Traffic Assessment* as a document of record by which future studies may be compared for conformance with the PUD Master Plan.

The Developer has identified traffic improvement measures for each PUD Subdivision that would reasonably mitigate the capacity impacts for those intersections that are overcapacity as defined by the level of service and volume-to-capacity ratio. Applicants for PUD Subdivision and/or PUD Site Plan approval and the Town shall work cooperatively to implement the improvement measures that reasonably satisfy the Town's historic corridor plans and the Developer's capacity needs as defined by the Development Agreement.

Streets

The PUD Master Plan defines a variety of allowable Street Types in combination with Public Frontage Types that are suitable for multimodal transportation

including pedestrian traffic. In order to maintain safety and accessibility for pedestrians and bicyclists, traffic must be slowed down, and most of the streets in the Woodmont Commons PUD will be designed to encourage lower vehicle speeds.

The transportation network will include regular geometric patterns of Street Types and Block Types arranged to provide comprehensible and diverse routes of travel within a hierarchy of streets – some varied and convenient for a balanced mix of both pedestrians and automobiles while others are designed to carry greater traffic and serve as the major boulevards, avenues, or commercial main streets.

The streets in the PUD will not be classified according to conventional functional classification, nor be subject to any requirements pertaining to such classifications. The existing system of functional classification defines three types of streets (local, collector, arterial) each for two types of environments: urban and rural. Within the Woodmont Commons PUD, the design of the streets will be informed by the context of the Subareas in which they reside. The actual street composition will be determined at the time of PUD Subdivision submittal in accordance with the design standards within this Section and any Supplemental TIA. Street types are to be combined with frontage types to make up the public right-of-way.

The Woodmont Commons PUD will include streets that are integral components of community design. Similar to the ITE's *Context Sensitive Solutions in Designing Major Urban Thoroughfares for Walkable Communities*, context sensitive solutions are used in the planning and design of streets for walkable communities. This results in streets that serve all users and are compatible with the surroundings through which they pass. The street design should complement buildings, public spaces and landscape, as well as support the human and economic activities associated with adjacent and surrounding land uses.

The intent is to provide enough capacity for traffic

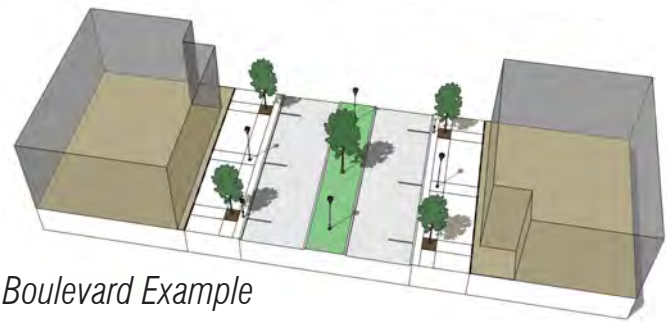
2.3.3 Transportation Network

related to the use and density of each Subarea and for the PUD as a whole. Each Street Type has its own set of standards, for example, total width, curb radius, travel lane width, presence of a median, on-street parking. Street Types and Public Frontage Types, in assembled combinations, also include pedestrian infrastructure, bicycle infrastructure, and landscaping to meet the needs of different areas.

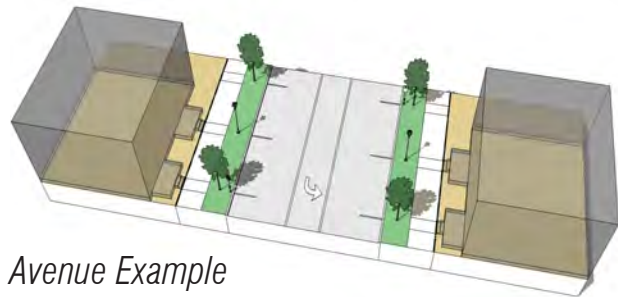
Street Types

The components of the Street Types in the Woodmont Commons PUD include travel lanes, medians, bike lanes, on-street parking and curbs. The minimum lane widths are shown within the Street Types diagrams. However, the actual lane widths may be wider and the number of lanes may be increased as determined by Supplemental TIAs to accommodate the transportation and emergency access needs. Street Types include the following:

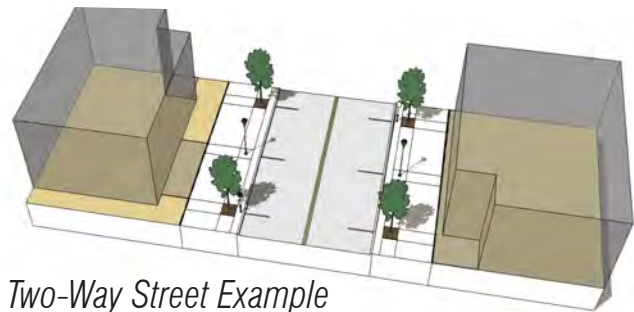
- A Boulevard is generally a larger multi-lane and generally urban corridor with a central, planted median, its designator is “B.”
- An Avenue is a connector which may have multiple lanes that runs through a community; its designator is “A.”
- A Street may allow one-way or two-way vehicular travel, is of closed-section (curb and gutter) design, will predominately have sidewalks on both sides, does not have a central median or “refuge” area, and is the most common corridor in the Woodmont Commons PUD; its designator is “2W” for two-way streets and “1W” for one-way streets.
- A Access Lane is found to the rear of lots, has no sidewalks or setbacks, is designed as the service routes for the lots they abut, and is typically constructed with inverted crowns and center drainage, but may be constructed otherwise where lower density residential lots only are serviced; its designator is “L.”



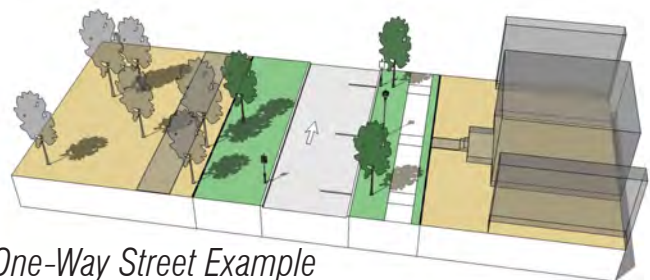
Boulevard Example



Avenue Example



Two-Way Street Example



One-Way Street Example



Access Lane Example

2.3.3 Transportation Network

Public Frontage Types

The public frontage is the layer between the lot line and the edge of the vehicular lanes. The public frontage may include sidewalks, street planters, trees and other vegetated landscaping, utilities, benches, street signage, lampposts, and other street furniture. There are six Public Frontage Types as defined on pages 126-127.

Sidewalk space shall be managed in a way that promotes pedestrian safety, efficiency and comfort and provides adequate space for street trees. Sidewalks should enhance the visual character of streets with landscaping used to reduce the impacts of vehicle traffic. In commercial areas, the sidewalk zone closest to the building may be used for merchandising and outdoor dining and seating. The sidewalk zone along the curb includes the area for street trees and furniture, however, in all cases, a clear pedestrian zone of five (5) feet minimum must be maintained along the path of travel.

Buildings on corner lots shall have two public frontages.

Private Frontage Types

The private frontage is the privately owned layer between the building façade and the lot line. Private frontages may include encroachments and yards. Private frontages are an important part of the context when designing for and creating visually cohesive streets. This frontage is based upon the use located within the lot. There are seven Private Frontage Types as defined on pages 128-129.

Buildings on corner lots shall have two private frontages.

Intersections

Offset intersections: Street intersections from opposite sides are permitted to be offset.

- A. Angles at intersections – Streets may be designed to intersect at up to a 85 degree angle.
- B. Right-of-way radii – Not required.
- C. Pavement corner radii – Minimum edge of pavement radii for intersection rounding corners shall be in accordance with *Section 2.3.3 Street Assembly Examples*.
- D. Intersection sight triangles – The minimum sight triangle for stop conditions at street intersections shall be 70 feet along the major road right of way and 10 feet along the minor road right of way. The intersection sight triangle shall be permanent right of way. This is permitted to be reduced for lower design (below 25 mph) speeds on streets.
- E. On-street parking – On-street parking shall be held back from intersections in order to increase the effective turning radius. Parking shall be restricted by posting, marking or striping.
- F. Intersection Sight Distance Guidelines – Intersection Sight Distance (ISD) represents the length of the roadway visible to a driver waiting to exit a driveway or minor street. ISD at intersections within the Woodmont Commons PUD should meet American Association of State Highway and Transportation Office (AASHTO) guidelines for desirable ISD for the design speed wherever practicable. In denser locations where on-street parking or buildings located close to the roadway may restrict sight lines, the ISD shall meet the guidelines for minimum Stopping Sight Distance (SSD) for the design speed. ISD shall be measured from an eye height of 3.5 feet to an object height of 3.5 feet, and measured from a distance 15 feet off the edge of the travel-way of the major

2.3.3 Transportation Network

roadway to represent a driver waiting to exit a driveway or minor roadway.

1. On-street parked vehicles will not be considered an obstruction to ISD so long as on-street parking is prohibited within 20 feet of the minor street and at least 12 feet of travel-way is provided adjacent to the on-street parking stalls to allow a vehicle exiting a driveway or minor street to pull slightly into the roadway to see beyond a parked vehicle. On-street parking closer than 20 feet to the minor street will be considered an obstruction to ISD.
2. For signalized intersections or unsignalized intersections with all-way STOP control, the ISD measured in either direction on each approach shall extend to a point at least 10 feet behind the STOP line on each of the other approaches to ensure that a driver stopped on any approach can see a vehicle stopped on any of the other approaches.
3. For unsignalized intersections with STOP control on one or more approaches and free-flowing traffic on one or more approaches, ISD exiting the STOP-controlled approaches should meet the following guidelines for the given design speed.

| Operating Speed (Designed) | AASHTO Guidelines (in feet) |
|-------------------------------|--------------------------------|
| 10 mph | 88 |
| 15 mph | 80 |
| 20 mph | 115 |
| 25 mph | 155 |
| 30 mph | 200 |
| 35 mph | 250 |

- G. Sight distances do not apply at signalized intersections. Vertical curves and superelevation are not needed (or desired) for design speeds under 35 mph.

Street Assemblies

The Street Types, and Public Frontage Types and Private Frontage Types can be assembled along with the appropriate Block Types and Building/Lot Types for the related Subarea and particular proposed uses. Context is the primary method of assembling the streets. The design should complement the buildings, public spaces and landscape, as well as support the human and economic activities associated with adjacent and surrounding land uses. The intent is to build streets that are integral components of community design within the Woodmont Commons PUD. The streets serve as primary pedestrian access to buildings and spaces and should be scaled for pedestrians. Capacity needs identified in the MTIA and any Supplemental TIAs should also be considered in the assembly.

Each street will have its own dimensional requirements. For example, an expanded sidewalk with landscape strip might be paired with 2-way street – 2-lane Section in a multi-family residential area. An expanded sidewalk would not be paired with a 2-way street in a single family residential area.

2.3.3 Transportation Network

Materials, Treatments and Construction

The dimensional standards for the streets and sidewalks within the Woodmont Commons PUD shall be in accordance with the PUD Master Plan. The materials and construction methods for the streets and sidewalks shall be in accordance with the *Londonderry Subdivision Regulations* and the *Typical Details for Site and Roadway Infrastructure* except that streets and sidewalks may have treatments that are not asphalt or concrete and may include brick or cobblestone. Vertical granite curbs will be used within the PUD in accordance with the *Londonderry Subdivision Regulations* and the *Typical Details for Site and Roadway Infrastructure*. Other treatment materials and/or treatments may also be used as appropriate for the neighborhood and shall be subject to review at the time of PUD Site Plan or PUD Subdivision submittal.

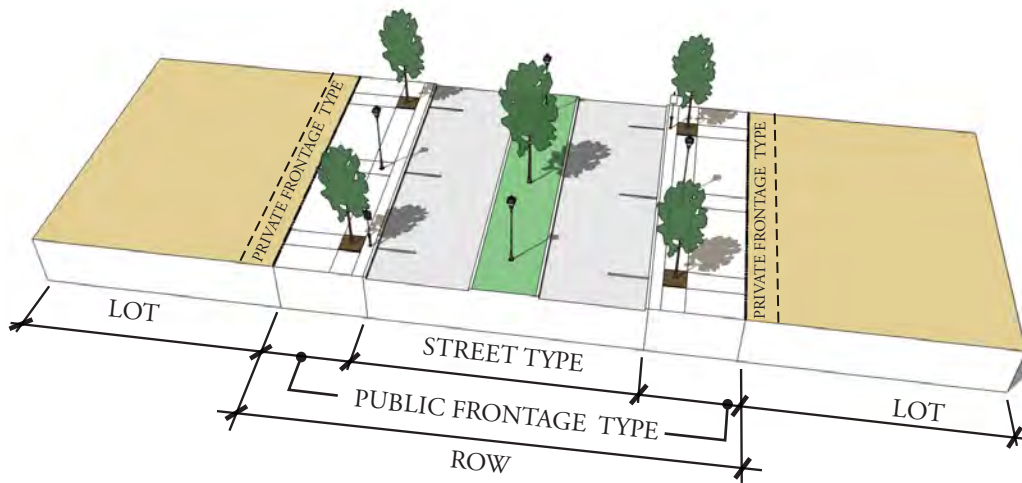
Streets that are intended to be accepted as public streets must be a minimum of twelve (12) feet wide and conform to the standards within the Town of Londonderry *Typical Details for Site and Roadway Infrastructure* dated May 2009. The structural capacity of streets shall meet or exceed Town of Londonderry standards.

Any construction details pertinent to the Woodmont Commons PUD which are not specifically included in the Londonderry or NHDOT standards shall be subject to review at the time of PUD Site Plan or PUD Subdivision submittal.

2.3.3 Transportation Network

STREETS | COMPONENTS

Standards



STREET DIAGRAM

DESCRIPTION Street rights-of-way are composed of the Street Types and Public Frontages. They are assembled from the following components based on their context, the traffic capacity they need to serve and the uses that are located on the lots along the streets. The Street Type dimension is to the back of the curb. The Street Types and Public Frontages may change along the street length according to the following:

- A. The number of travel lanes may be reduced or increased after an intersection, as long as the traffic capacity and emergency access needs are served;
- B. Turn lanes may be incorporated into medians;
- C. A one-way street or median-divided lane of more than 300 feet in length should have a minimum width of 16-18 feet;
- D. The public frontage may change after a street intersection when the uses warrant a different type;
- E. The selection of the public frontage type is based upon the building uses and their private frontages;
- F. The public frontages may differ from one side of the street to the other depending upon the uses served;
- G. Bike lanes may be added to Street Types. Bike way types may vary along the street length depending upon vehicular speed and the availability of alternate trails or means of connection;
- H. Parking may be unmarked in single family residential areas. On-street parking may vary depending on street location and frontage adjacencies;
- I. The location of on-street parking may vary along the street length.

MINIMUM STREET DESIGN STANDARDS

| | |
|-----------------------|---|
| Travel Lanes (feet) | 10 minimum; where striped, lanes can be greater than or equal to 10 |
| Parking Lanes (feet) | 8 minimum |
| Median Width (feet) | 6 minimum |
| Bike Way Width (feet) | 5 where applicable |

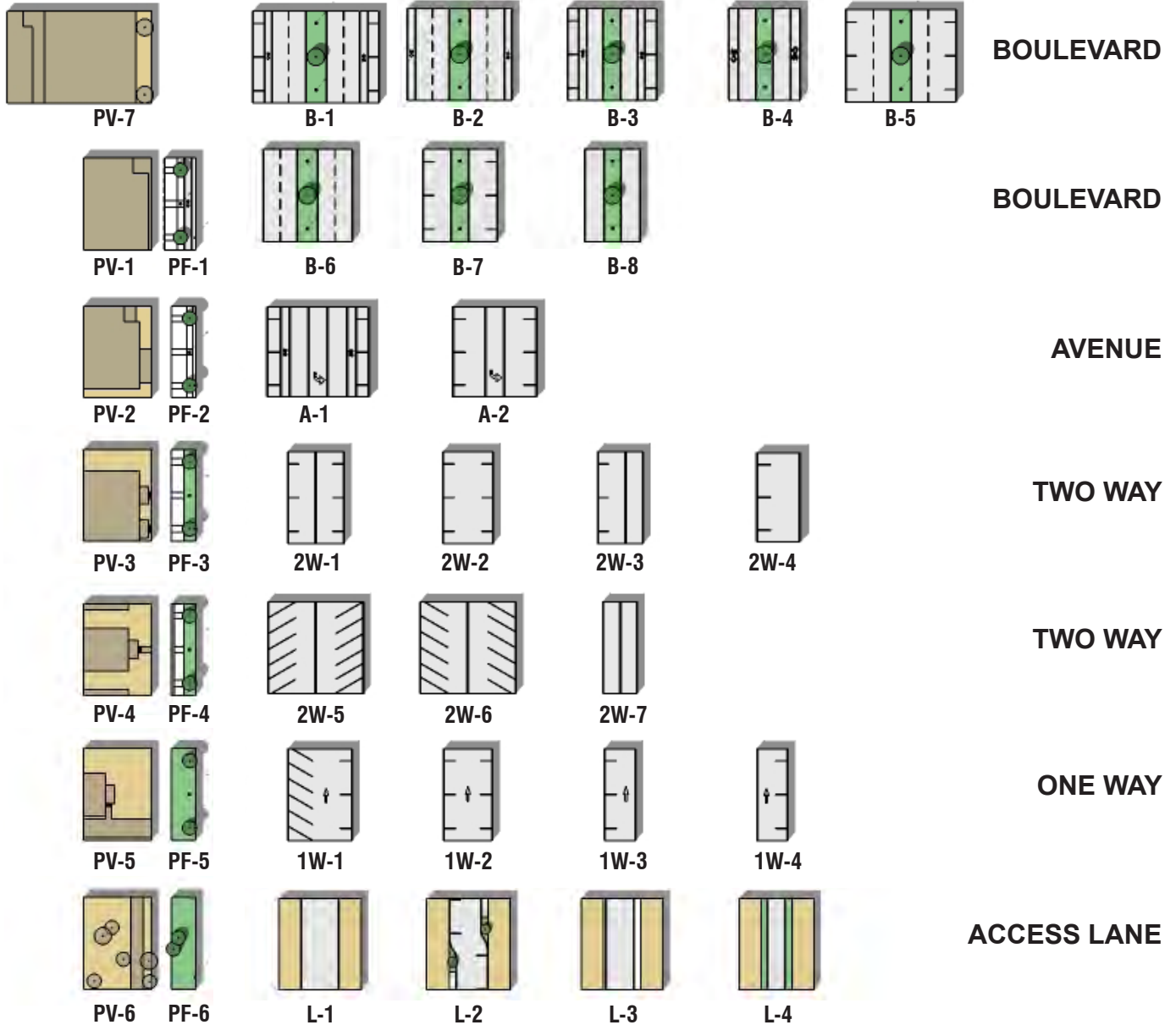
2.3.3 Transportation Network

STREETS | COMPONENTS

Types

FRONTAGE TYPES

STREET TYPES



PRIVATE

PUBLIC

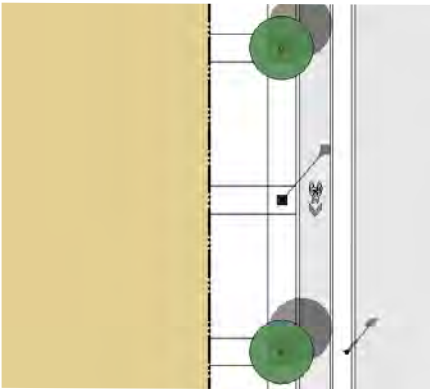
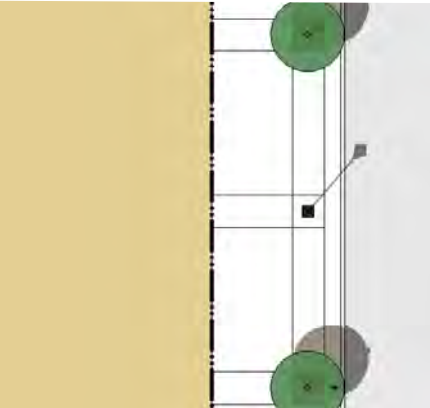
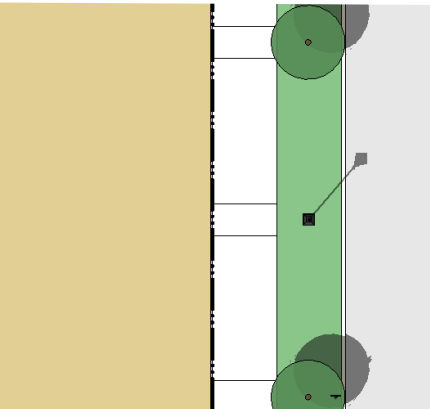
DESCRIPTION The illustrations above show examples of each type of assembled street. Street Assemblies are to be made up of the components above, the Public Frontage Types shown on pages 126-127, and the Private Frontage Types on pages 128-129.

Three-dimensional examples of assembled streets and the related dimensional standards are found on pages 130-141. These street examples are not intended to limit the number and types of streets in Woodmont Commons.

2.3.3 Transportation Network

STREETS | COMPONENTS

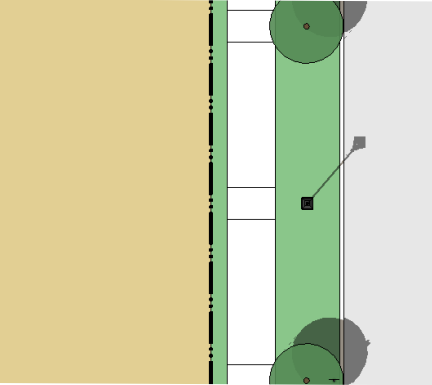
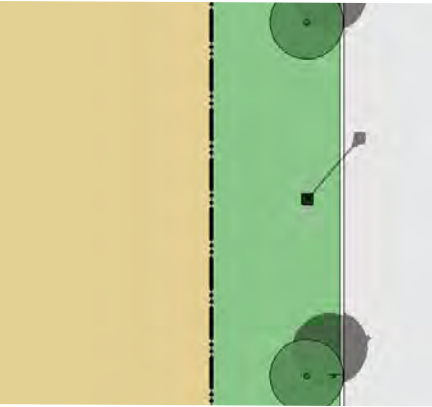
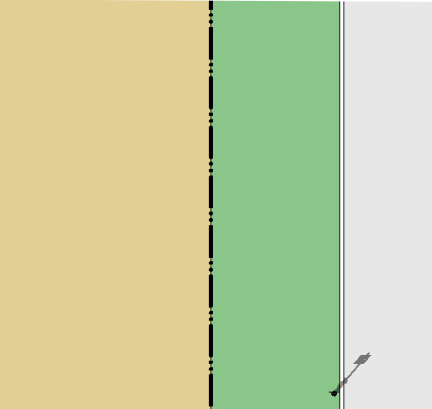
Public Frontage Types

| Plan | | Frontage Description |
|---|------------------------|---|
| LOT PRIVATE FRONTAGE | ROW PUBLIC FRONTAGE | |
|  | | <p>Public Frontages Type 1.</p> <p>Location: This Frontage is to be used with Primary Streets where a raised cycletrack may be used. The Frontage consists of the walk and cycletrack with street trees in tree wells.</p> <p>Uses: Commercial Uses and Residential, Civic and Accommodation Uses when located within a mixed-use area or mixed-use building.</p> <p>Dimensions: Typically eight (8) to ten (10) feet of walk and seven (7) to eight (8) feet of cycletrack and signage zone at the curb.</p> |
|  | | <p>Public Frontages Type 2.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of the walk with street trees.</p> <p>Uses: Commercial Uses and Residential, Civic and Accommodation Uses when located within a mixed-use area or mixed-use building.</p> <p>Dimensions: Typically twelve (12) to sixteen (16) feet of walk.</p> |
|  | | <p>Public Frontages Type 3.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of the walk with street trees in a continuous planting strip.</p> <p>Uses: Residential Uses and Commercial, Civic and Accommodation Uses not located within a mixed-use area.</p> <p>Dimensions: Typically seven (7) to eight (8) feet of walk and seven (7) to eight (8) feet of continuous planting strip at the curb.</p> |

2.3.3 Transportation Network

STREETS | COMPONENTS

Public Frontage Types

| Plan | | Frontage Description |
|---|------------------------|--|
| LOT PRIVATE FRONTAGE | ROW PUBLIC FRONTAGE | |
|  | | <p>Public Frontages Type 4.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of the walk with street trees in a continuous planting strip. The planting strip may include some stormwater features such as rain gardens.</p> <p>Uses: Residential Uses and Commercial, Civic and Accommodation Uses not located within a residential Subarea.</p> <p>Dimensions: Typically two (2) to three (3) feet of planting strip at the right-of-way line, five (5) to six (6) feet of walk and six (6) to eight (8) feet of continuous planting strip at the curb.</p> |
|  | | <p>Public Frontages Type 5.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of street trees in a continuous planting strip. The planting strip may include some stormwater features such as swales or rain gardens.</p> <p>Uses: Residential Uses facing on to existing streets where no sidewalk is desired, and Shared Open Space areas.</p> <p>Dimensions: Typically ten (10) to sixteen (16) feet of continuous planting strip.</p> |
|  | | <p>Public Frontages Type 6.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of a continuous planting strip. The planting strip may include some stormwater features such as swales or rain gardens.</p> <p>Uses: Residential Uses facing on to existing streets where no sidewalk is desired and trees are located within a perimeter buffer; Shared Open Space areas with trees located within the lot.</p> <p>Dimensions: Typically ten (10) to sixteen (16) feet of continuous planting strip at the curb.</p> |

General Notes

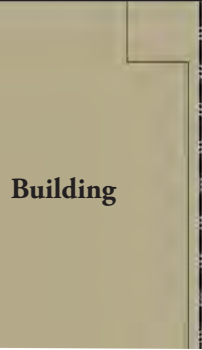



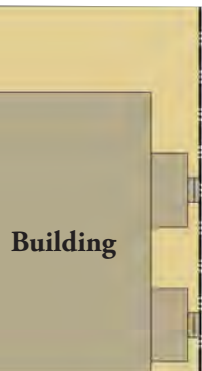

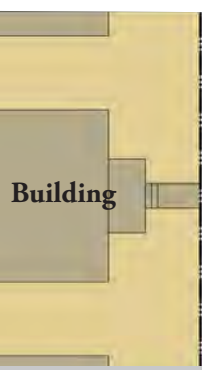

1. The Public Frontages contribute to the character and walkability of the Subareas. Increased dimensions are permitted for any Frontage Type. Decreased dimensions may be permitted on Secondary Streets of limited length where not required for connectivity.
2. Access Lanes do not require Public Frontages.
3. Street trees, where provided, may vary in species. See *Section 2.3.9 PUD Subdivision Landscape* for additional requirements pertaining to street trees.

2.3.3 Transportation Network

STREETS | COMPONENTS

Private Frontage Types

Private Frontages – General. The Private Frontage is the area between the building façade and the lot line.

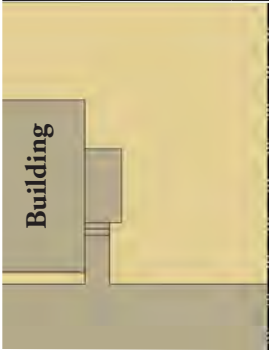

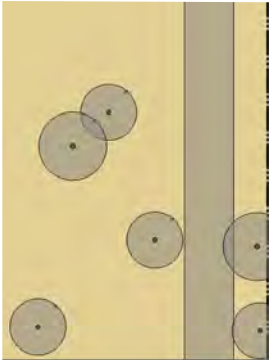

| Plan | | Frontage Description |
|---|---|---|
| LOT PRIVATE FRONT-AGE | ROW PUBLIC FRONTAGE | |
|  |  | <p>Private Frontages Type 1. Description: A frontage wherein the façade is at or aligned close to the lot line with the building entrance at sidewalk grade. The private frontage should be predominantly paved but may include intermittent sections of landscaping. This frontage may have encroachments into the right-of-way. Use with: Public Frontage Types 1, 2 Uses: Commercial Uses, particularly retail, and Residential, Civic and Accommodation Uses when located within a mixed-use area or mixed-use building, medium and large building.</p> |
|  |  | <p>Private Frontages Type 2. Description: A planted frontage wherein the façade may be set back from the lot line. A wall at the lot line may be used to maintain the street spatial definition. This frontage may have encroachments into the setback. Use with: Public Frontage Types 1, 2 or 3. Uses: Dedicated Office Buildings, Commercial Uses and Residential, Civic and Accommodation Uses when located within a medium to small single use building or a medium to small mixed-use area or mixed-use building.</p> |
|  |  | <p>Private Frontages Type 3. Location: A frontage wherein the façade is aligned close to the lot line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing, stoop or porch. This type is recommended for ground-floor residential use. Use with: Public Frontage Type 3. Uses: Residential Uses for densities above fifteen (15) dwelling units per acre.</p> |
|  |  | <p>Private Frontages Type 4. Location: A planted frontage wherein the façade is set back from the lot line. The front yard created is visually continuous with adjacent yards, supporting a common landscape. The setback provides a buffer where the higher speed streets occur. Use with: Public Frontage Type 4 or 5. Uses: Residential Uses, and Small Buildings with Commercial, Civic and Accommodation Uses.</p> |



2.3.3 Transportation Network

STREETS | COMPONENTS

Private Frontage Types

Private Frontages – General. The Private Frontage is the area between the building façade and the lot line.

| Plan | | Frontage Description |
|--|--|---|
| LOT PRIVATE FRONTAGE | ROW PUBLIC FRONTAGE | |
|  |  | <p>Private Frontages Type 5.</p> <p>Location: A planted frontage wherein the façade is set back from the lot line and access to the lot is from the front street. The front yard created is visually continuous with adjacent yards, supporting a common landscape. The setback provides a buffer where the higher speed streets occur.</p> <p>Use with: Public Frontage Type 4, 5 or 6.</p> <p>Uses: Residential Uses facing on to existing streets where no sidewalk is desired, and Shared Open Space areas.</p> |
|  |  | <p>Private Frontages Type 6.</p> <p>Location: A natural area or public area. The frontage may include a trail or shared use path.</p> <p>Use with: Public Frontage Type 4 or 5.</p> <p>Uses: Shared Open Space or Conserved Green Space areas.</p> |
| LOT PRIVATE FRONTAGE | R.O.W. PUBLIC FRONTAGE | Frontage Description |

| | | |
|---|---|---|
|  |  | <p>Private Frontages Type 7.</p> <p>Location: This Frontage is to be used with Side Streets. The frontage consists of a continuous eight (8) foot planting strip for screening where surface parking occurs in the frontage. The planting strip may include some stormwater features such as swales or rain gardens.</p> <p>Use with: Public Frontage Type 3 or 4.</p> <p>Uses: Large Format Retail, Flex Buildings, and other uses located on a Side Street.</p> |
|---|---|---|

General Notes

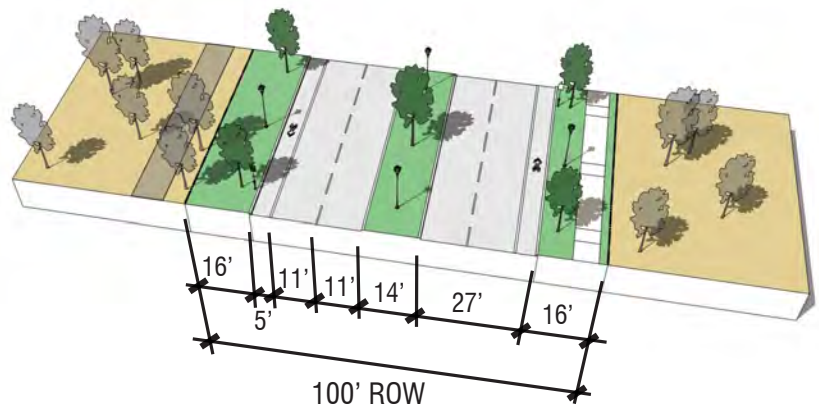
1. See Lot Types for specific building placement requirements.
2. The uses listed are a guideline for the private frontage type. Other uses may be appropriate depending upon the Subarea principles and standards.
3. See *Section 2.4.6 PUD Site Plan Landscape* for screening and landscaping requirements for surface parking lots.

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Entrance Boulevard

STREET TYPE FRONTAGE TYPE
PUBLIC PRIVATE

- B-2 PF-5/PF-4 PV-6



DESCRIPTION This street is intended for connection to a major route such as Route 102. It is a wider street that accommodates both non-motorized and vehicular traffic with a landscaped median and landscaped pedestrian realm.

STREET DESIGN STANDARDS

| | |
|------------------------------------|--------------------------------|
| Travel Lanes | 11' (Turning Lanes 12') |
| Parking Lanes | None |
| ROW Width (feet) | 100' |
| Pavement Width | 27' on each side of the median |
| Greenscape Buffer without sidewalk | Up to 16' |
| Traffic Flow | Two Way |
| Curb Type | Vertical |
| Curb Radius (feet) | 20' |
| Vehicular Design Speed | 35 mph |
| Pedestrian Crossing Time | 7 seconds curb to median |
| Road Edge Treatment | Curb |
| Bike Way Type | Dedicated Lane |
| Bike Way Width (feet) | 5' |

PUBLIC FRONTAGE

| | |
|---------------------------|---------------------------------------|
| Planter Type | Continuous |
| Planting Pattern | Trees at 44' O.C. average |
| Planter Strip / Box Width | See frontage |
| Tree Type | Vary species, drought / salt tolerant |
| Utilities | Underground |
| Street Light Type | Pedestrian scale ornamental |
| Street Light Spaces | No limit |
| Sidewalk Placement | One side |
| Sidewalk Width (feet) | 6' |
| Sidewalk Encroachment | None |

SUBAREAS

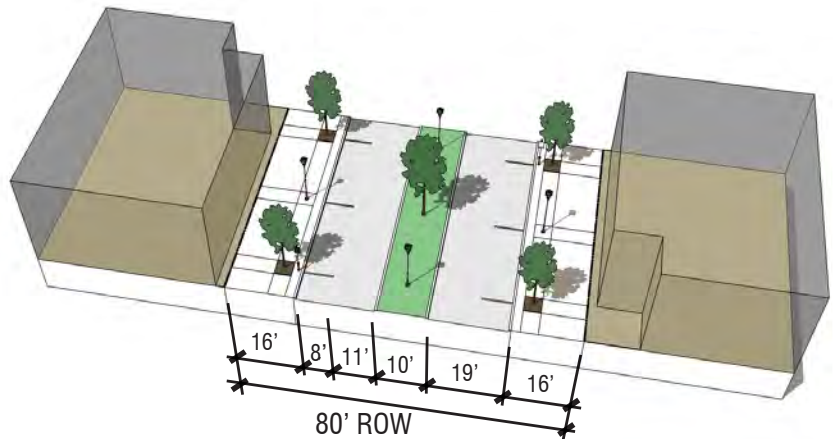
| | |
|-----------------------|----------------|
| Allowed in Subarea(s) | WC-1-GL, WC-12 |
|-----------------------|----------------|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Commercial Boulevard

| STREET TYPE | FRONTAGE TYPE | |
|-------------|---------------|---------|
| | PUBLIC | PRIVATE |
| • B-7 | PF-3 | PV-1 |



DESCRIPTION This street is intended for major connecting routes in commercial and mixed-use areas such as the Village Center. It accommodates both non-motorized and vehicular traffic with a landscaped median and expanded pedestrian realm.

STREET DESIGN STANDARDS

| | |
|--------------------------|--------------------------------|
| Travel Lanes | 11' |
| Parking Lanes | Parallel, both sides, lined |
| ROW Width (feet) | 80' |
| Pavement Width | 19' on each side of the median |
| Traffic Flow | Two Way |
| Curb Type | Vertical |
| Curb Radius (feet) | 16' |
| Vehicular Design Speed | 25 mph |
| Pedestrian Crossing Time | 5 seconds curb to median |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width (feet) | None |

PUBLIC FRONTAGE

| | |
|---------------------------|--|
| Planter Type | Individual in frontage, continuous at median |
| Planting Pattern | Trees at 44' O.C. average |
| Planter Strip / Box Width | 4' x 4' with expandable grates |
| Tree Type | Vary species, drought / salt tolerant |
| Utilities | Underground |
| Street Light Type | Pedestrian scale ornamental |
| Street Light Spaces | 44' interval (as per light level) |
| Sidewalk Placement | Both sides |
| Sidewalk Width (feet) | 16' (extension onto lot permitted) |
| Sidewalk Encroachment | Seating and signage as allowed |

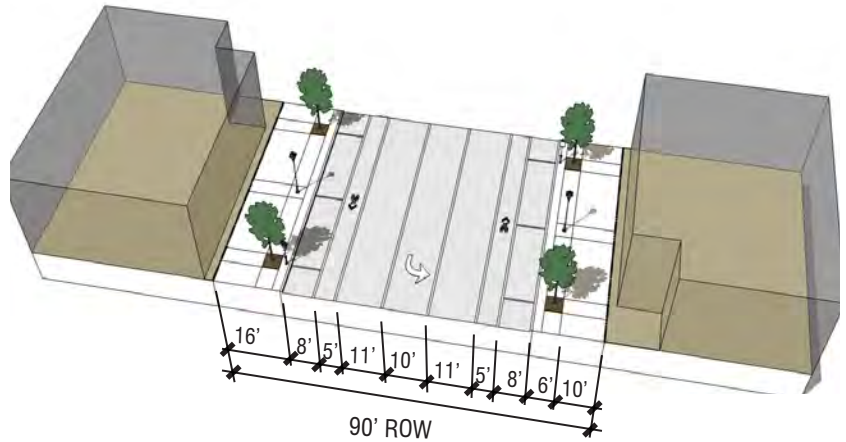
SUBAREAS

| | |
|-----------------------|---|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-8, WC-10, WC-12 |
|-----------------------|---|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Commercial Avenue

| STREET TYPE | FRONTAGE TYPE | |
|-------------|---------------|---------|
| | PUBLIC | PRIVATE |
| • A-1 | PF-1 | PV-1 |



DESCRIPTION This is a primary street intended for less intense connections, a simple avenue with variable dimensions.

STREET DESIGN STANDARDS

| | |
|--------------------------|-----------------------------|
| Travel Lanes | 11' |
| Parking Lanes | Parallel, both sides, lined |
| ROW Width | 90' |
| Pavement Width | 58' |
| Traffic Flow | Two way |
| Curb Type | Vertical |
| Curb Radius (feet) | 20' |
| Vehicular Design Speed | 30 mph |
| Pedestrian Crossing Time | 15 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Bicycle lanes |
| Bike Way Width | 5' |

PUBLIC FRONTAGE

| | |
|---------------------------|---|
| Planter Type | Individual in Sidewalk, intermittent median |
| Planting Pattern | Trees at 44' O.C. average |
| Planter Strip / Box Width | 4' x 4' with expandable grates |
| Tree Type | Vary species, drought / salt tolerant |
| Utilities | Underground |
| Street Light Type | Pedestrian scale ornamental |
| Street Light Spaces | 44' interval (as per light level) |
| Sidewalk Placement | Both sides |
| Sidewalk Width | 8' (extension onto lot permitted) |
| Sidewalk Encroachment | Seating and signage as allowed |

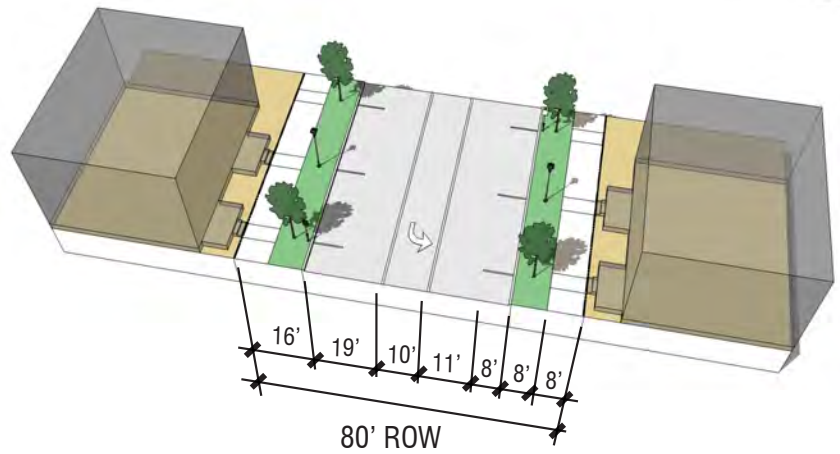
SUBAREAS

| | |
|-----------------------|----------------------------------|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-8, WC-12 |
|-----------------------|----------------------------------|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Mixed-Use Avenue

| STREET TYPE | FRONTAGE TYPE | FRONTAGE TYPE |
|-------------|---------------|---------------|
| | PUBLIC | PRIVATE |
| • A-2 | PF-1 | PV-2 |



DESCRIPTION This is a primary or secondary street intended for well-traveled connections such as Pillsbury Road with frequent site access requirements. It is intended for medium density residential areas and neighborhood service areas.

STREET DESIGN STANDARDS

| | |
|--------------------------|-----------------------------|
| Travel Lanes | 11' |
| Parking Lanes | Parallel, both sides, lined |
| ROW Width | 80' |
| Pavement Width | 48' |
| Traffic Flow | Two way |
| Curb Type | Vertical |
| Curb Radius (feet) | 15' |
| Vehicular Design Speed | 25 mph |
| Pedestrian Crossing Time | 12 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width | - |

PUBLIC FRONTAGE

| | |
|---------------------------|--|
| Planter Type | Continuous planting strip, intermittent median |
| Planting Pattern | Trees at 44' O.C. average |
| Planter Strip / Box Width | 8' |
| Tree Type | Vary species, drought / salt tolerant |
| Utilities | Underground |
| Street Light Type | Pedestrian scale ornamental |
| Street Light Spaces | 44' interval (as per light level) |
| Sidewalk Placement | Both sides |
| Sidewalk Width | 8' (extension onto lot permitted) |
| Sidewalk Encroachment | Seating and signage as allowed |

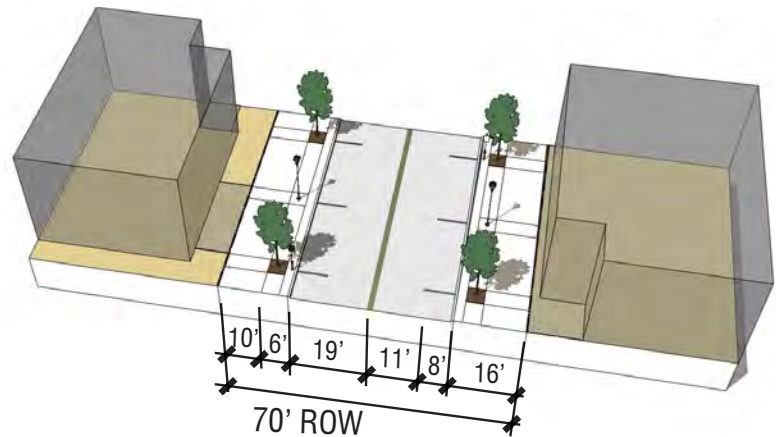
SUBAREAS

| | |
|-----------------------|---|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-8, WC-10, WC-12 |
|-----------------------|---|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Commercial Street

| | |
|-------------|---------------------|
| STREET TYPE | FRONTAGE TYPE |
| | PUBLIC PRIVATE |
| • 2W-1 | PF-2 PV-1/PV-2 |



DESCRIPTION This is a secondary street for commercial and mixed-use areas with two-way travel in two dedicated lanes intended for a medium capacity street.

STREET DESIGN STANDARDS

| | |
|--------------------------|-----------------------------|
| Travel Lanes | 11' |
| Parking Lanes | Parallel, both sides, lined |
| ROW Width | 70' |
| Pavement Width | 38' |
| Traffic Flow | Two Way |
| Curb Type | Vertical |
| Curb Radius (feet) | 15' |
| Vehicular Design Speed | 20-25 mph |
| Pedestrian Crossing Time | 10 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width | - |

PUBLIC FRONTAGE

| | |
|---------------------------|---------------------------------------|
| Planter Type | Individual in Sidewalk |
| Planting Pattern | Trees at 44' O.C. average |
| Planter Strip / Box Width | 4' x 4' with expandable grates |
| Tree Type | Vary species, drought / salt tolerant |
| Utilities | Underground |
| Street Light Type | Pedestrian scale ornamental |
| Street Light Spaces | 44' interval (as per light level) |
| Sidewalk Placement | Both sides |
| Sidewalk Width | 16' (extension onto lot permitted) |
| Sidewalk Encroachment | Seating and signage as allowed |

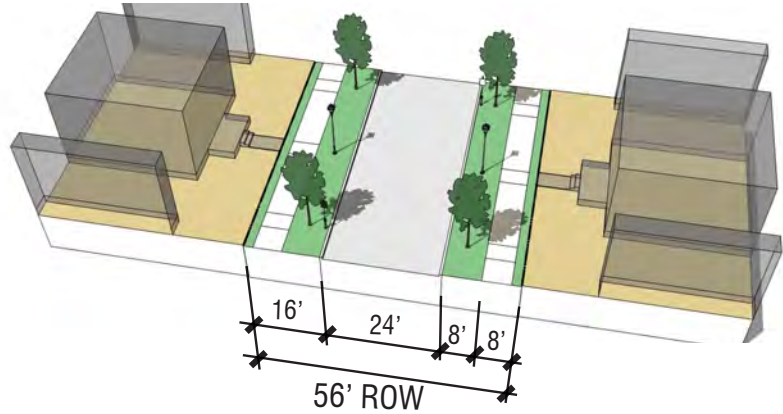
SUBAREAS

| | |
|-----------------------|--------------------------|
| Allowed in Subarea(s) | All Subareas except WC-3 |
|-----------------------|--------------------------|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Residential Street

| | | |
|-------------|---------------|---------|
| STREET TYPE | FRONTAGE TYPE | |
| | PUBLIC | PRIVATE |
| • 2W-2 | PF-3 | PV-4 |



DESCRIPTION This is a secondary street for lower density residential uses with two-way travel in a shared yield lane intended for a lower capacity street.

STREET DESIGN STANDARDS

| | |
|--------------------------|--|
| Travel Lanes | Yield Street |
| Parking Lanes | Intermittent parallel, both sides, unlined |
| ROW Width | 56' |
| Pavement Width | 24' |
| Traffic Flow | Two Way |
| Curb Type | Vertical |
| Curb Radius (feet) | 5-20' |
| Vehicular Design Speed | 20-25 mph |
| Pedestrian Crossing Time | 6 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width | - |

PUBLIC FRONTAGE

| | |
|---------------------------|--|
| Planter Type | Individual in Sidewalk |
| Planting Pattern | Trees at 44' O.C. average |
| Planter Strip / Box Width | Continuous |
| Tree Type | Vary species, drought / salt tolerant |
| Utilities | Underground |
| Street Light Type | Pedestrian scale ornamental |
| Street Light Spaces | 44' interval (as per light level) May not be needed on all streets. |
| Sidewalk Placement | Both sides |
| Sidewalk Width | 8' (extension onto lot permitted) |
| Sidewalk Encroachment | Per Subarea |

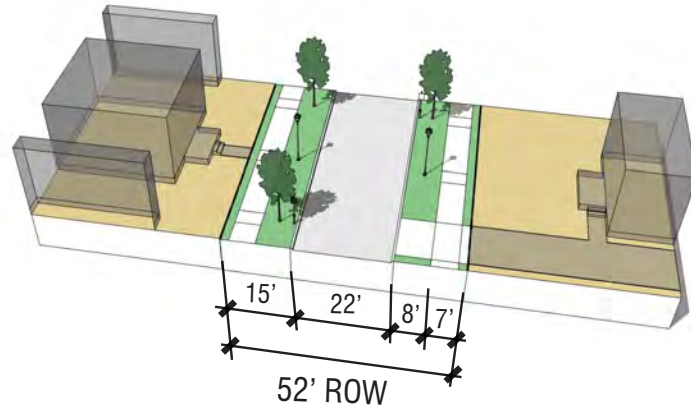
SUBAREAS

| | |
|-----------------------|--------------------------|
| Allowed in Subarea(s) | All Subareas except WC-3 |
|-----------------------|--------------------------|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Residential Street

| | | |
|-------------|---------------|-----------|
| STREET TYPE | FRONTAGE TYPE | |
| | PUBLIC | PRIVATE |
| • 2W-5 | PF-4 | PV-4/PV-4 |



DESCRIPTION This is a secondary street for low density residential uses with front loaded lots and along a PUD Boundary and with two-way travel in a shared yield lane intended for a lower capacity street.

STREET DESIGN STANDARDS

| | |
|--------------------------|-----------------------------|
| Travel Lanes | 22' |
| Parking Lanes | Parallel, one sides, posted |
| ROW Width | 52' |
| Pavement Width | 22' |
| Traffic Flow | Two Way |
| Curb Type | Vertical |
| Curb Radius (feet) | 5-20' |
| Vehicular Design Speed | 20-25 mph |
| Pedestrian Crossing Time | 6 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width | - |

PUBLIC FRONTAGE

| | |
|---------------------------|--|
| Planter Type | Continuous |
| Planting Pattern | Trees at 44' O.C. average |
| Planter Strip / Box Width | Continuous |
| Tree Type | Vary species, drought / salt tolerant |
| Utilities | Underground |
| Street Light Type | Pedestrian scale ornamental |
| Street Light Spaces | 44' interval (as per light level) May not be needed on all streets. |
| Sidewalk Placement | Both sides |
| Sidewalk Width | 6' |
| Sidewalk Encroachment | None |

SUBAREAS

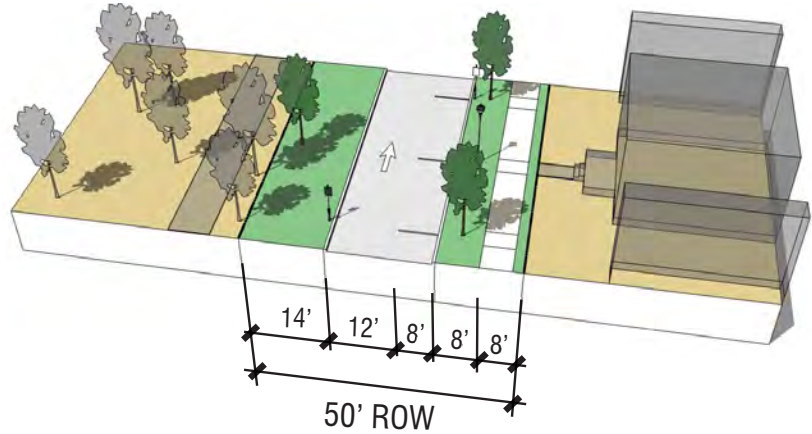
| | |
|-----------------------|--------------------------|
| Allowed in Subarea(s) | All Subareas except WC-3 |
|-----------------------|--------------------------|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Residential One-way Street

| | | |
|-------------|---------------|-----------|
| STREET TYPE | FRONTAGE TYPE | |
| | PUBLIC | PRIVATE |
| • 1W-2 | PF-6/PF-4 | PV-6/PV-4 |



DESCRIPTION This is a secondary street for single family residential uses along an open space or green area with internal trees and with one-way travel in one dedicated lane intended for a lower capacity street.

STREET DESIGN STANDARDS

| | |
|--------------------------|------------------------------|
| Travel Lanes | 12' |
| Parking Lanes | Parallel, one sides, unlined |
| ROW Width | 50' |
| Pavement Width | 20' |
| Traffic Flow | One way |
| Curb Type | Vertical |
| Curb Radius (feet) | 5-20' |
| Vehicular Design Speed | 15 mph |
| Pedestrian Crossing Time | 5 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width | - |

PUBLIC FRONTAGE

| | |
|---------------------------|---------------------------------------|
| Planter Type | Continuous |
| Planting Pattern | Trees at 44' O.C. average |
| Planter Strip / Box Width | Continuous |
| Tree Type | Vary species, drought / salt tolerant |
| Utilities | Underground |
| Street Light Type | Pedestrian scale ornamental |
| Street Light Spaces | 44' interval (as per light level) |
| Sidewalk Placement | One side |
| Sidewalk Width | 6' |
| Sidewalk Encroachment | None |

SUBAREAS

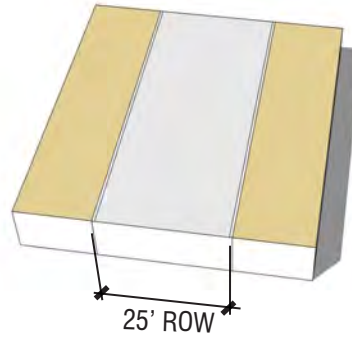
| | |
|-----------------------|--------------------------|
| Allowed in Subarea(s) | All Subareas except WC-3 |
|-----------------------|--------------------------|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Commercial Access Lane

| STREET TYPE | FRONTAGE TYPE | |
|-------------|---------------|---------|
| | PUBLIC | PRIVATE |
| • L-1 | NA | NA |



DESCRIPTION This is a very slow speed access lane located at the rear of commercial, multifamily and mixed-use buildings and internal to blocks for vehicular parking access, loading and service access.

STREET DESIGN STANDARDS

| | |
|--------------------------|-------------|
| Travel Lanes | 25' |
| Parking Lanes | - |
| ROW Width | 25' |
| Pavement Width | 25' |
| Traffic Flow | Two way |
| Curb Type | Vertical |
| Curb Radius (feet) | 5-20' |
| Vehicular Design Speed | 15 mph |
| Pedestrian Crossing Time | 7 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width | - |

PUBLIC FRONTAGE

| | |
|---------------------------|---|
| Planter Type | - |
| Planting Pattern | - |
| Planter Strip / Box Width | - |
| Tree Type | - |
| Utilities | - |
| Street Light Type | - |
| Street Light Spaces | - |
| Sidewalk Placement | - |
| Sidewalk Width | - |
| Sidewalk Encroachment | - |

SUBAREAS

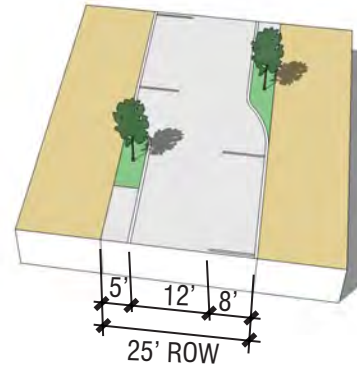
| | |
|-----------------------|----------------------------------|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-8, WC-12 |
|-----------------------|----------------------------------|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Residential Access Lane

| STREET TYPE | FRONTAGE TYPE | |
|-------------|---------------|---------|
| | PUBLIC | PRIVATE |
| • L-2 | NA | NA |



DESCRIPTION This is a very slow speed access lane located at the rear of residential uses and small and medium buildings, internal to blocks for vehicular parking access, loading and service access.

STREET DESIGN STANDARDS

| | |
|--------------------------|-----------------------|
| Travel Lanes | 12' yield |
| Parking Lanes | Parallel, alternating |
| ROW Width | 25' |
| Pavement Width | 25' |
| Traffic Flow | Two Way |
| Curb Type | Vertical |
| Curb Radius (feet) | 5-20 |
| Vehicular Design Speed | 10 mph |
| Pedestrian Crossing Time | 5 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width | - |

PUBLIC FRONTAGE

| | |
|---------------------------|----|
| Planter Type | - |
| Planting Pattern | - |
| Planter Strip / Box Width | 5' |
| Tree Type | - |
| Utilities | - |
| Street Light Type | - |
| Street Light Spaces | - |
| Sidewalk Placement | - |
| Sidewalk Width | - |
| Sidewalk Encroachment | - |

SUBAREAS

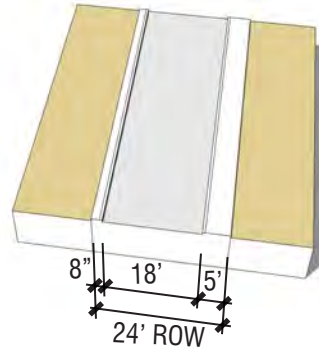
| | |
|-----------------------|--------------|
| Allowed in Subarea(s) | All Subareas |
|-----------------------|--------------|

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Commercial Access Lane

| STREET TYPE | FRONTAGE TYPE | |
|-------------|---------------|---------|
| | PUBLIC | PRIVATE |
| • L-3 | NA | NA |



DESCRIPTION This is a very slow speed access lane located at the rear of buildings and internal to blocks for vehicular parking access, loading and service access.

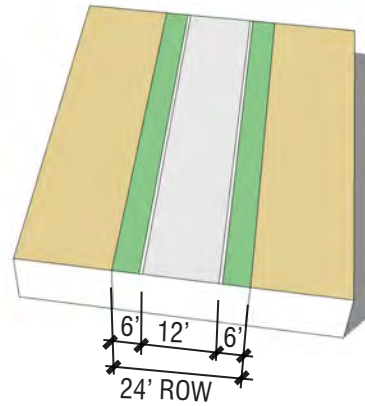
| STREET DESIGN STANDARDS | |
|---------------------------|---|
| Travel Lanes | 18' |
| Parking Lanes | - |
| ROW Width | 25' |
| Pavement Width | 18' |
| Traffic Flow | Two way |
| Curb Type | Vertical |
| Curb Radius (feet) | 5-20' |
| Vehicular Design Speed | 10 mph |
| Pedestrian Crossing Time | 3 seconds |
| Road Edge Treatment | Space for Curbing |
| Bike Way Type | Shared lane |
| Bike Way Width | - |
| PUBLIC FRONTAGE | |
| Planter Type | - |
| Planting Pattern | - |
| Planter Strip / Box Width | - |
| Tree Type | - |
| Utilities | - |
| Street Light Type | - |
| Street Light Spaces | - |
| Sidewalk Placement | - |
| Sidewalk Width | 5' |
| Sidewalk Encroachment | - |
| SUBAREAS | |
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-8, WC-10, WC-12 |

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Residential Access Lane

| STREET TYPE | FRONTAGE TYPE | |
|-------------|---------------|---------|
| | PUBLIC | PRIVATE |
| • L-4 | NA | NA |



DESCRIPTION This is a very slow speed access lane located behind single family residential and internal to blocks for vehicular parking access.

STREET DESIGN STANDARDS

| | |
|--------------------------|-------------|
| Travel Lanes | 12' yield |
| Parking Lanes | - |
| ROW Width | 24' |
| Pavement Width | 12' |
| Traffic Flow | Two Way |
| Curb Type | Flush |
| Curb Radius (feet) | 5-20 |
| Vehicular Design Speed | 10 mph |
| Pedestrian Crossing Time | 3 seconds |
| Road Edge Treatment | Curb |
| Bike Way Type | Shared lane |
| Bike Way Width | - |

PUBLIC FRONTAGE

| | |
|---------------------------|---|
| Planter Type | - |
| Planting Pattern | - |
| Planter Strip / Box Width | - |
| Tree Type | - |
| Utilities | - |
| Street Light Type | - |
| Street Light Spaces | - |
| Sidewalk Placement | - |
| Sidewalk Width | - |
| Sidewalk Encroachment | - |

SUBAREAS

| | |
|-----------------------|--------------|
| Allowed in Subarea(s) | All Subareas |
|-----------------------|--------------|

2.3.4 Parking and Loading Standards

Purpose & Intent

This Section provides a comprehensive set of parking and loading criteria in order to.

- Mitigate environmental, economic, and social impacts associated with parking;
- Reduce the amount of land devoted to parking;
- Reduce inefficient, dispersed, and single-use reserved parking;
- Maximize on-street parking opportunities;
- Encourage the use of alternative transportation modes and reduce vehicle use;
- Improve community livability, walkability, workability and character;
- Provide flexibility in accommodating changes in demand and use.

On-street parking will be utilized in most parts of the Woodmont Commons PUD to reduce the size of surface lots, slow the speed of vehicles and to separate vehicle traffic from pedestrian traffic

Vehicle Access

Vehicle access to all lots shall be from public or private rights-of-way by one or more curb cuts/driveways.

The location and design of curb cuts/driveways shall minimize traffic hazards and not unduly retard traffic flow in public rights-of-way.

The width of any driveway, except for radii, shall not exceed twenty-four (24) feet for most uses unless truck activity for a specific tenant area requires a wider apron or except for where the turn lanes are required by a Supplemental TIAs.

Each driveway providing site access from a Street Type or other public right-of-way shall be designed and constructed in compliance with the following:

- A. Number of driveways:
 1. Single-family dwellings and duplexes shall have one driveway per dwelling unit.
 2. Multi-family and nonresidential projects.
 - a. A multi-family or nonresidential development project on a parcel of two acres or less shall be limited to a maximum of two (2) driveways, unless the Planning Board determines that more than two (2) driveways are required to accommodate the traffic for the project.
 - b. Whenever a property has access to more than one (1) street, access should be generally be directed to the lower volume street.
- B. Driveways and sidewalks – Driveways shall not be designed to allow parked vehicles to back across a sidewalk, unless it is a residential use or is an infrequently used and signed service driveway for other uses.

2.3.4 Parking and Loading Standards

- C. Distance from street corners – Each driveway should be separated from the nearest street intersection as follows, except where the Planning Board allows less separation:
1. A minimum of 100 feet from the nearest intersection of two streets, as measured from the centerline of the driveway to the centerline of the nearest travel lane of the intersecting street; and
 2. For parcels with frontages of less than 150 feet, the minimum distance shall be seventy-five (75) feet.
- D. Driveway spacing – Driveways should be separated along a street frontage as follows.
1. For Single-family and duplex residential developments, driveways should be shared or separated by six (6) feet.
 2. For Multi-family and nonresidential developments, where two (2) or more driveways serve the same or adjacent multi-family or nonresidential development, the centerline of the driveways should be separated by approximately forty (40) to fifty (50) feet. The Planning Board may approve exceptions to this standard. This guideline does not apply to parking structures or lots located inside blocks.
- E. Driveway dimensions – Dimensions and sight distances shall adhere to reasonable safety protocols and this PUD Master Plan.
- F. Clearance from obstructions – The nearest edge of a driveway curb cut shall be at least two (2) feet from a property line unless it is a common driveway and three (3) feet from the centerline of a fire hydrant, light standard, traffic signal, utility pole, or other similar facility.
- G. Traffic safety visibility areas – Trees, streetlights, pedestrian wayfinding systems and other amenities are encouraged along all streets in the Project. There are no limitations on the placement of these features or buildings near any signalized intersections. Around unsignalized intersections, structures or landscaping are allowed, provided they do not create significant obstructions to pedestrians or excessively block drivers' vision; trees are always permitted with their lower canopy trimmed up to a minimum of five (5) feet from the ground within seventy-five (75) feet of an unsignalized intersection.
- H. Intersection Spacing – Intersections may be as close as seventy-five (75) feet for two-way streets and fifty (50) feet for one-way streets, measured centerline to centerline.
- I. Street Rights of Way – In order to allow buildings to better shape the spaces along streets and around intersections, street rights-of-way may meet without connecting radii.

Off –Street Parking Facilities

Location

- A. Parking Facilities:
1. Every lot shall contain an off-street parking area or access to off-site parking.
 2. All off-street parking shall have dedicated access to on-site lanes to a public right-of-way.
 3. All mixed-use or commercial uses shall have parking areas within twenty (20) feet of any structure with a curb or substantial bumper not less than four (4) feet from the structure, unless access is interrupted by a loading zone, fire hydrant, or other instances where circumstances render said restriction impractical.

2.3.4 Parking and Loading Standards

- B. Off-Site Parking – All projects may locate required parking away from the site of the proposed use.
1. Location of Parking – Permanent off-site parking should be located within 600 feet of residential uses and within 1,200 feet of nonresidential uses if on parking lots or within 1,500 feet of a shared parking structure for any use, unless otherwise approved by the Planning Board.
 2. Evaluation of an Alternate Proposal – In considering a request for off-site parking at distances greater than those above, the Planning Board may consider whether adequate provisions, such as shuttle service, adequate pedestrian facilities and street crossings, have been provided to bring people from the parking to the site.
 3. Guarantee of Continued Availability – Long-term required parking spaces that are off-site may be committed by a recordable lease, deed, or other easement agreement, acceptable to the Town, and which shall run with the land.
- C. Phased Development, Temporary and Off-site Parking – Due to the 20-year buildout time period expected for the Woodmont Commons PUD, future sites of buildings may be graded and prepared for use as temporary shared parking areas. Any such temporary parking areas that are assigned to earlier phases of development in the Woodmont Commons PUD may substitute for future structured or other parking as may be designated, assigned and shown to the Planning Board by the Applicant. This provision is specifically intended to allow early and especially smaller phases of development to have smaller amounts and even no off-street parking areas on the developed parcel.
- D. Buildout Ratios/Park-Once – In Use Zones WC-1 and WC-2, as well as in any future areas of similar character, uses will change over time and parking shall not be required to be updated for every change of use provided: the overall parking ratio is in the range of 2.2 or more spaces per thousand square feet of commercial GLA; the residential density does not exceed 15 (fifteen) units/acre; parking is shared; and no use is more than 1,500 feet from a shared off-street parking facility. The purpose of this is to encourage the establishment of future “park-once” areas where patrons and others arriving by vehicle will park one time and then walk amongst many uses.

Dimensional Requirements

- A. Parking Space and Lot Dimensions: Parking lots and stalls shall comply with the vehicle parking space dimensions.
- B. Loading Space Dimensions:
1. Minimum dimensions for a commercial loading space shall be determined by evaluation of the size of anticipated delivery vehicles. Standards for Off-street Loading Areas – Off-street loading areas should be sized as follows: Dimensions – Loading spaces should be eight (8) to twelve (12) feet in width, twenty (20) to forty (40) feet in length, with ten (10) to fourteen (14) feet of vertical clearance.
 2. Shared on-street loading areas are permitted and encouraged in the denser parts of the Project, in particular Subareas WC-1-GL, WC-1, WC-2, WC-10 and WC-12 and any other areas of similar character.

2.3.4 Parking and Loading Standards

3. Off-street loading spaces are encouraged. The Planning Board may modify these guidelines, where it is determined that the operating, shipping, and delivery characteristics of the use do not require the number or type of loading spaces required by this Section, or that adequate shared on-street loading spaces are provided.

Table: Vehicle Parking Space Dimensions

| Angle of Parking | Space Width | Curb Length | Space Depth | One-Way Aisle Width |
|------------------|-------------|-----------------------|---------------------|---------------------|
| (in degrees) | (in feet) | (in feet per vehicle) | (in feet from curb) | (in feet) |
| Parallel* | 7-8.5 | 20-22 | 7-8.5 | 9-11 |
| 30° | 7.5-8.5 | 16-18 | 14.5-15.5 | 10-12 |
| 45° | 7.5-8.5 | 10-14 | 16-18 | 10-12 |
| 60° | 7.5-8.5 | 10-12 | 18-20 | 10-14 |
| 90° | 7.5-8.5 | 7-9 | 17-19 | 10-14 |

* The use of 7-foot wide parallel parking lanes may be accompanied by a wider bike lane in areas of high bicycle traffic.

Off-Street Parking and Loading Standards

- A. Off-site parking facility types include the following:
 1. Surface Parking
 2. Enclosed and Covered Parking
 3. Structured Parking
- B. On-Site Parking and Access to Parking
 1. Except in Subareas WC-1-GL, WC-12 and on Side Streets, surface parking lots shall generally not abut any streets other than lanes and should generally be located behind buildings.
 2. Except in Subareas WC-1-GL, WC-12 and on Side Streets, parking, including parking garages, should generally be accessed from a lane or secondary frontage when practical.
 3. Pedestrian entrances to all parking lots and parking garages should be directly accessible from the sidewalk and/or a building.
- C. Access to Adjacent Sites – Adjacent off-street parking areas on adjacent properties are encouraged to have vehicle connections to provide for convenience, safety, and efficient circulation. Shared pedestrian access between adjacent properties is also strongly encouraged.
- D. Tandem Parking – Tandem parking (vehicles parked front bumper to back bumper) is permissible in private parking lots, areas or garages where all tandem parking is attendant directed on a full-time, year-round basis. Vehicles may be parked in tandem where the parking area is serving a single dwelling (including dwellings in multiple-family developments when both tandem spaces are assigned to the same

2.3.4 Parking and Loading Standards

unit) and where the tandem parking is not more than two cars in depth. The Planning Board may grant approval of tandem parking for nonresidential developments if the Applicant demonstrates that it will not interfere with orderly parking and movement of vehicles.

1. Mechanical Parking Lifts – The Planning Board may also approve mechanical parking lifts when an attendant or valet parking agreement is provided. Proposals for mechanical parking lifts shall be on a case-by-case basis to assess their functionality and safety.

Minimum Parking and Loading Required

- A. Number of Parking Spaces – The number of parking spaces allocated for each land use shall be determined as follows:
 1. Basic Space Requirement – Each land use shall provide the number of parking spaces outlined in the Parking Space Guidelines Table, as applicable, except where the requirement is reduced or otherwise changed in compliance with the Reduction of Parking Requirements or Alternative Parking Standards. On-street parking shall be considered public parking and may count towards the number of public parking spaces provided. In the event that the Town or State removes such on-street parking that was allowed to count towards the minimum required, the existing use will not be required to make up the difference and the use will not be made non-conforming. Single-family detached, duplex and town-house dwellings shall provide two (2) spaces per dwelling. Accessory Dwelling Units shall provided with one (1) space per dwelling unit.
 2. Public or Reserved Parking – Parking shall be provided in accordance with either the Public or the Reserved ratios listed in the Parking Space Guidelines but the two types shall not be combined together except as follows. Up to ten (10) percent of the shared parking may be reserved and still count towards the public parking totals. These reserved spaces may be utilized for, but not limited to, car sharing, carpooling, or electric vehicle charging stations. Reserved parking may incorporate on-site shared public parking areas for visitors, guests, etc. The public parking will count towards the reserved parking totals.
 - a. Public – If a land use shares its parking publicly (i.e. not reserved), it shall provide parking consistent with the “public” requirement rows in the Parking Space Guidelines Table.
 - b. Reserved – If a land use reserves its parking exclusively for that use alone, it must provide parking consistent with the “reserved” requirement rows in the Parking Space Guidelines Table.
 3. Parking Space Allocation – For other than public parking, parking spaces may be considered allocated to a particular Lot, or use, if those spaces are entirely on that Lot or the use of the spaces has been assured by assignment through easement or other legal guaranty. On-street parking may be allocated to a particular Lot when a particular on-street space abuts a particular Lot. Where an on-street space abuts more than one Lot, a particular parking space may be allocated to a particular Lot only if more than 66% of the space abuts that Lot or if that space has otherwise been legally assigned to that Lot or use. On-street parking shall be counted toward fulfilling parking requirements. See also the phased development provisions for temporary parking in the Reduction of Parking Requirements below.
 - a. Use with Accessory Components – A single use with accessory components shall provide parking for each component. For example, a hotel with a restaurant shall provide the parking spaces re-

2.3.4 Parking and Loading Standards

quired by the Parking Space Guidelines Table for a hotel (e.g., the guest rooms), and for a restaurant, except where the requirement is reduced or otherwise changed in compliance with the Reduction of Parking Requirements below.

- b. Expansion of Structure – For additions or enlargements of existing buildings or uses that would increase the number of parking spaces required, the additional parking shall be required only for such addition, enlargement or change, and not for the pre-existing building or use.
- c. Change in Use – For any change of occupancy or manner of operation that may increase the number of parking spaces required, no additional parking shall be required unless the Planning Board determines that the change in use without added parking would create a negative impact for neighboring properties.
- d. Excessive Parking – The Developer will discourage a use from providing the maximum number allowed in the Parking Space Guidelines Table, in order to avoid the inefficient use of land, unnecessary pavement, and excessive storm water runoff from paved surfaces. The maximum number of spaces allowed varies based on whether spaces are to be reserved for on-site tenants and visitors, or to be shared parking.
- e. Floor Area – Where the Parking Space Guidelines Table indicates a parking requirement based on gross leasable area (for example: 1 space for each 1,000 GLA), the term “gross leasable area” shall mean gross leasable square footage of floor area. Gross leasable floor area is the total building area designed for tenant occupancy and exclusive use, including any basements, mezzanines or upper floors. It is the area for which tenants pay rent; it is the area that produces income, as defined by the Institute of Transportation Engineers (ITE).
- f. Parking Type – Where the Parking Space Guidelines Table indicates a parking type, the term “public” shall mean parking that is publicly available and “reserved” shall mean parking that is reserved exclusively for the use of those occupying the land use.
- g. Use Zones – Where the Parking Space Guidelines Table references Subareas, see the Land Use Plan for locations.
- h. Rounding of Calculations – If a fractional number is obtained in calculations performed in compliance with this Chapter, one (1) additional parking space shall be required for a fractional unit of 0.50 or above, and no additional space shall be required for a fractional unit of less than 0.50.

2.3.4 Parking and Loading Standards

Table: Parking Space Guidelines

| Land Use Category | Metric | Type | WC-1-GL and WC-12 | | Other Subareas | |
|-------------------------------|---------------|----------|-------------------------------|---------------|----------------|---------------|
| | | | Minimum Ratio | Maximum Ratio | Minimum Ratio | Maximum Ratio |
| Residential | | | | | | |
| Multi-family | Dwelling Unit | Public | 0.75 | 2 | 0.25 | 2 |
| | | Reserved | 1 | 3 | 0.50 | 2 |
| Non-residential | | | | | | |
| Office and General Work Space | 1,000 GLA | Public | 1 | 3 | 0.75 | 3 |
| | | Reserved | 2 | 3.50 | 2 | 3.50 |
| General Commercial | 1,000 GLA | Public | 1 | 4 | 0.75 | 3 |
| | | Reserved | 2 | 5 | 2 | 4 |
| Restaurant and Event Venues | 1,000 GLA | Public | 6 | 12 | 5 | 10 |
| | | Reserved | 7 | 15 | 6 | 14 |
| Hotel | Rooms | Public | 0.5 | 2 | 0.40 | 1.5 |
| | | Reserved | 1.25 | 1.75 | 1 | 1.75 |
| Light Industrial | 1,000 GLA | Public | 0.15 | 1.25 | 0.25 | 1.25 |
| | | Reserved | 0.40 | 1.75 | 0.30 | 1.5 |
| All Other Uses | | Public | ULI <u>Shared Parking</u> | | | |
| | | Reserved | As agreed with Planning Board | | | |

* Urban Land Institute's Shared Parking, as amended, incorporated herein by reference.

- B. Motorcycle Parking – A parking lot with 100 or more vehicle parking spaces shall provide motorcycle parking spaces conveniently located near the main entrance to the primary structure and accessed by the same access aisles that serve the vehicle parking spaces in the parking lot.
1. Number of Spaces Required – A minimum of one (1) motorcycle parking space shall be provided for each 100 vehicle spaces or fraction thereof.
 2. Space Dimensions – Motorcycle spaces shall have minimum dimensions of four (4) feet by seven (7) feet.
- C. Number of Loading Spaces – Off-street loading spaces guidelines are as follows:
1. Guidelines for uses not listed shall be determined by the Planning Board based upon the *Londonderry Zoning Ordinance* for comparable uses. Loading zones in mixed-use areas may be shared.

2.3.4 Parking and Loading Standards

Table: Loading Spaces

| Use | Size | Loading Required |
|--|------------------------------|--|
| Retail commercial, industrial, manufacturing, research and development, institutional, service uses, and other allowed nonresidential uses | 10,000 to 25,000 square feet | 1 |
| | 25,001+ square feet | 1 for each additional 25,000 square feet |
| Office uses | 10,000 to 35,000 square feet | 1 |
| | 35,001+ square feet | 1 for each additional 25,000 square feet |

2. Location – Loading spaces should be:
 - a. As near as possible to the main structure and limited to the rear two-thirds of the parcel, if feasible;
 - b. Situated to ensure that loading and unloading can take place on-site without the need for vehicles to back onto public streets;
 - c. Situated to avoid adverse impacts upon neighboring residential properties.
3. Loading Ramps – Plans for loading ramps or truck wells shall be accompanied by a profile drawing showing the ramp, ramp transitions, and overhead clearances.
4. Specific Uses - These include the following:
 - a. A minimum of five (5) stacking spaces shall be required for restaurants with drive-up window. The distance shall be measured from the drive-up window.
 - b. A minimum of two (2) stacking spaces per car wash bay shall be required.
 - c. A minimum of four (4) stacking spaces shall be required for financial institutions having one drive-thru window and three (3) stacking spaces for each additional window.
 - d. For other uses not specifically provided herein, the Planning Board (with a recommendation from the Zoning Officer) shall make a determination regarding the number of stacking spaces required.

On-Street Parking and Loading Standards

- A. On-Street Parking – Except in WC-1-GL, needed parallel or, in WC-1, WC-2 and WC-12 parallel or continuous angled parking will be allowed along streets where Commercial uses predominate. Parallel parking is permitted on any other streets. On-street parking may count towards the parking requirements for a particular use or block when located in front of the use or along the block.
- B. On-Street Loading – Shared on-street loading areas are permitted and encouraged in the denser parts of the PUD, in particular, Subareas WC-1, WC-2, WC-10 and any other areas of similar character. On-street loading may occur in dedicated and posted loading zones.

2.3.4 Parking and Loading Standards

Design & Construction Standards

Parking

Surface parking design and construction to be in accordance with *Section 3.10.13 Design & Construction Standards* of the *Londonderry Zoning Ordinance*. Parking spaces within Structured Parking shall be in accordance with the following:

- A. All parking spaces shall be so marked by use of painted lines or other permanent striping materials;
- B. Circulation areas shall be designed so that vehicles can proceed safely without posing a danger to pedestrians or other vehicles and without interfering with parking areas.

Loading

- A. Striping
 - 1. Loading spaces shall be striped, and identified for loading only.
 - 2. The striping and “loading only” notations should be continuously maintained in a clear and visible manner in compliance with the approved plans.
- B. Surfacing
 - 1. All loading areas may be surfaced with asphalt, concrete pavement, or comparable material as determined by the Planning Board and shall be graded to drain all surface water.

Alternative Parking Standards

Reduction of Parking Requirements – The number of parking spaces required by the Parking Space Guidelines Table on page 148 may be reduced or modified through the following measures or any combination of measures.

- A. Shared Parking – Where two or more uses utilizing the same parking have distinct and different hours of peak parking demand (such as a theater and a bank), the required minimum number of parking spaces may be reduced by up to the number of spaces required for the least intensive use.
- B. Reduction of Parking – The Planning Board may reduce or waive the minimum number of parking spaces required based on quantitative information provided by the Applicant that documents the need for fewer spaces (e.g. sales receipts, documentation of customer frequency, information on parking standards required for the proposed land use by other cities, etc.).
- C. Waiver of Parking: Off-Hour Uses – The Planning Board may reduce or waive the minimum number of parking spaces required for a land use that has a peak parking demand that does not coincide with the general peak parking demand (generally considered the evening peak period).
- D. Waiver of Parking – The Planning Board may reduce or waive the minimum number of parking spaces required based on quantitative information provided by the Applicant that documents a reduction in parking demand.

2.3.4 Parking and Loading Standards

- E. Other alternative transportation methods may be proposed including those outlined in *Section 3.10.12.5 Alternative Transportation of the Londonderry Zoning Ordinance*.

Parking for Disabled Persons

Parking for the disabled shall be in compliance with *Section 3.10.14 of the Londonderry Zoning Ordinance* and *Section 3.11(d) of the Londonderry Site Plan Regulations*. Parking spaces required for the disabled shall count toward compliance with the number of parking spaces required by this Section.

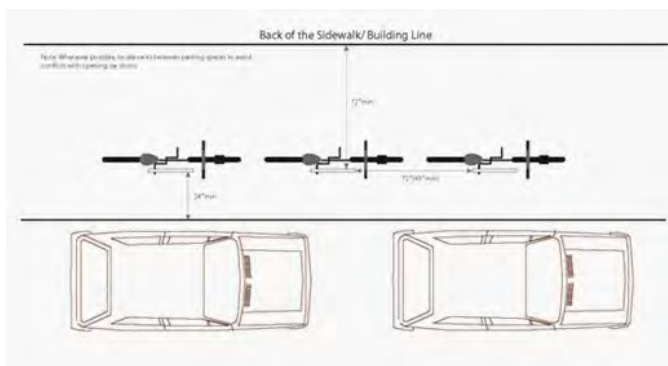
“Unbundled” Parking

All off-street parking spaces for new multi-family projects, or in future new conversions of multifamily projects, may be leased or sold separately from the rental or purchase fees for the life of the units if approved by the Planning Board through the permitting process.

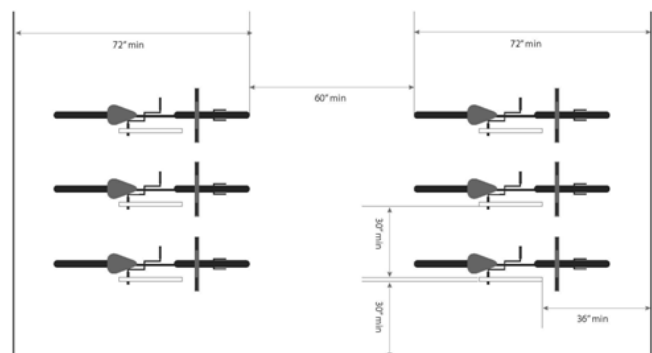
Bicycle Parking Standards

Each multi-family residential use and all nonresidential uses shall provide bicycle parking in compliance with this section.

- A. Bicycle Parking Standards – Bicycle parking should be designed for both short-term and long-term needs. The bicycle parking requirement may be met on site or may be exempted based on nearby shared facilities.
1. Short-Term Parking – This is intended for bicycle trips where bicycles will be left for a short time period (generally less than 2 hours). It requires a high degree of convenience and should be located as close to destinations as possible. Short-term bicycle parking may be located within the public right-of-way or on an internal sidewalk as per the Short-term parking diagram.
 2. Long-Term Parking – This is intended for bicycle trips where bicycles will be left for a longer periods of time (generally for several hours, overnight, or seasonally). Long-term bicycle parking is intended to serve the needs of commuters and residents, but not retail patrons. All bicycle parking stalls shall be located in accordance with the off-street or off-site parking requirements of the PUD Master Plan.



Short-term Bicycle Parking



Long-term Bicycle Parking

2.3.4 Parking and Loading Standards

- B. Number of Bicycle Spaces – Off-street bicycle parking is required whenever any new use is established or any existing use is enlarged for which more than ten (10) automobile parking spaces are required.

TABLE - BICYCLE PARKING

| Auto Spaces Required | Bicycle Stalls Required |
|----------------------|-------------------------|
| 10-50 | 10% of auto |
| 51-100 | 5% of auto |
| 100+ | 3% of auto |

1. Requirements are additive. Once a threshold is met the previously existing requirement shall be maintained and additional stalls shall be provided at the new level until the next threshold is met.
2. A minimum of two (2) and a maximum of twenty (20) stalls shall be provided.
3. In calculating stall requirements all decimals and fractions shall be rounded up to the nearest whole number.
4. All bicycle parking stalls shall be located within 100 feet of the primary use or as close as the closest auto space.
5. A bicycle “stall” shall include a delineated and safe parking area, and an appropriate structure to which bicycles can be locked.
6. Appropriate structure means a stand or other device constructed so as to enable the user to secure by locking the frame and one wheel of each bicycle parked therein. Racks must be easily usable with both U-locks and cable locks. Racks should support the bikes in a stable upright position so that a bike, if bumped, will not fall or roll down. Racks that support a bike primarily by a wheel, such as standard ‘wire racks’ are damaging to wheels and thus are not acceptable.
7. Bicycle parking racks, shelters or lockers must be securely anchored to the ground or to a structure.
8. Outdoor bicycle parking areas shall be surfaced with hard-surfacing material having a minimum depth of two inches, such as pavers, asphalt or concrete, unless otherwise permitted by the Planning Board.
9. Provision of additional amenities including but not limited to covered parking and shower facilities is encouraged for lots over one-hundred (100) automobile spaces but not required.

Public Safety Standards

- A. Emergency vehicle access shall be provided to buildings and occupied structures and access routes and clearances shall be indicated on the Site and/or PUD Subdivision Plan drawings at the time of submittal.
- B. Access streets and routes must be designed to withstand the live loads of fire apparatus.
- C. The Developer shall consult with the Fire Department with regards to the number of fire hydrants and the street layout and connectivity to facilitate response times within the Fire Department’s standard response district.

2.3.5 PUD Perimeter Buffers

The *Londonderry Zoning Ordinance* requires a fifty (50) foot setback for streets and structures from the perimeter of the Woodmont Commons PUD boundary which abuts existing residential uses. This Perimeter Buffer, as indicated in *Section 2.2.2 Land Use Plan*, may be crossed by streets, driveways, paths or sidewalks, may include pervious and impervious surfaces, and may include Shared Use Paths or Recreation Trails. If the adjoining uses are incompatible with the uses in the Woodmont Commons PUD, then landscaping and other screening devices are required to enhance the buffer. An example would be a non-residential project within the Woodmont Commons PUD abutting existing residential uses outside the PUD perimeter.

No screening is required for adjoining compatible uses (i.e. single-family next to single-family).

Along Gilcreast Road, the landscaping within the perimeter is proposed to be the existing orchard-style trees, three rows deep. Acceptable species include those defined in the Landscape Design Standards within the *Londonderry Site Plan Regulations*.

2.3.6 Conserved Green Space and Shared Open Space Standards

The PUD Master Plan has the following allowable Open Space Types within the PUD. These Types all contribute to the minimum Shared Open Space and Conserved Green Space requirements in *Section 2.2.3 Land Use Standards*.

- Conserved Green Space;
- Green;
- Square;
- Plaza;
- Playing Field;

- Playground;
- Park.

Other Open Space Types may be permitted when the type is mutually beneficial to the Town and the PUD as determined by the Planning Board and the Developer. Each Open Space Type has different requirements and eligible features, which are described on the following pages. These Types are to be assembled with the Block and Street Types at the PUD Subdivision and/or PUD Site Plan level. As stated in *Section 2.2 Land Use Regulations*, submittals shall be as follows:

- All PUD Subdivision and/or PUD Site Plan submittals shall indicate the types and locations of Conserved Green Space and Shared Open Space(s) within the submittal in general accordance with the Land Use Plan. The locations may be adjusted according to the PUD Subdivision and/or PUD Site Plan requirements;
- At least one half acre of Shared Open Space or publicly accessible Conserved Green Space must be provided within one quarter mile of the front door of any approved building. At least one half acre of Active Open Space must be provided within one half mile of the front door of any approved building. Publicly accessible Shared Open Space and Conserved Green Space must be connected to a public street or sidewalk network;
- Shared Open Space shall be identified at the PUD Subdivision and/or PUD Site Plan level. Qualifying publicly accessible Shared Open Space and Conserved Green Space for each specific PUD Subdivision or PUD Site Plan must be identified prior to the release of certificates of occupancy for any PUD Site Plan within the PUD Subdivision;
- Shared Use Paths and Recreation Trails are included in this Section as they are components of Open Space and are not within public rights-of-way.

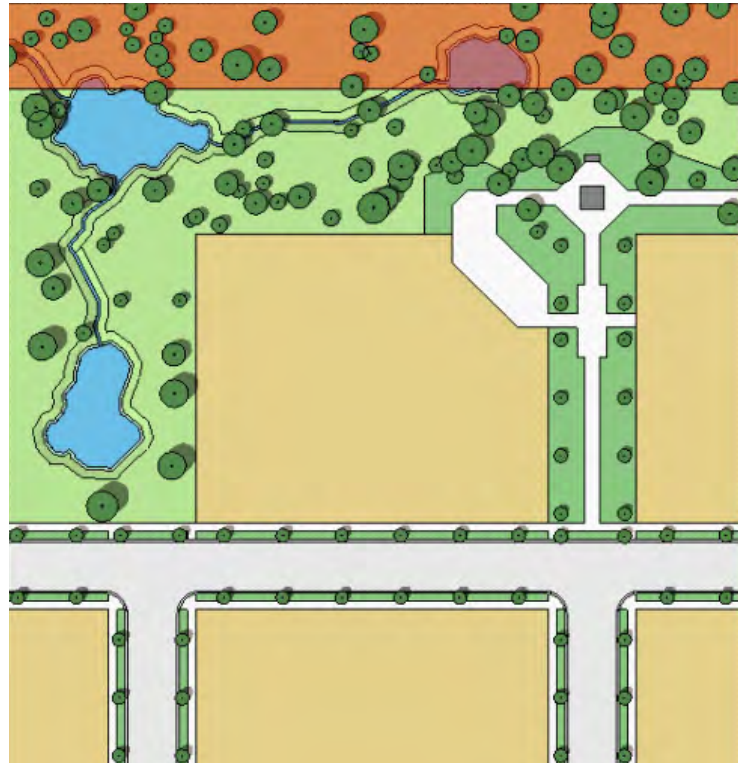
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Conserved Green Space

DESCRIPTION Conserved Green Space may or may not be publicly accessible. A preserve may be independent of surrounding building frontages or located completely behind buildings with no public frontage. Accessory Structures are allowable if they meet Paragraph 2.6.3.3.2.10 of Section 2.6.3 Conservation Overlay District of the Londonderry Zoning Ordinance.



Character Examples



| LOCATION | |
|--------------------------------|--|
| Characteristics | May be linear, following the trajectories of natural corridors |
| REQUIREMENTS | |
| Minimum Size | None |
| Suggested Frontage on at Least | |
| Publicly Accessible | Not required |
| Accessway required | No |
| Accessway(s) allowed | Trail, independent shared use path |
| ELIGIBLE FEATURES | |
| Include: | Meadows, water bodies, wetlands, woodlands, passive recreation, buffers at perimeter of property, perimeter sidewalks, bikeways, paths, trails, access, crossing streets, easements, and stormwater features |
| SUBAREAS | |
| Allowed in Subarea(s) | All Subareas |

2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Green

DESCRIPTION Open space that is publicly accessible and available for recreation.



Character Examples



| LOCATION | |
|--------------------------------|--|
| Characteristics | Spatially defined by landscaping and determined by configuration of street and block network |
| REQUIREMENTS | |
| Minimum Size | 1/4 acre (4 acre maximum) |
| Suggested Frontage on at Least | 1 Street |
| Publicly Accessible | Yes |
| Accessway required | Yes |
| Accessway(s) allowed | Perimeter sidewalk, trail, independent shared use path |
| ELIGIBLE FEATURES | |
| Include: | Naturalistically disposed lawn and trees, perimeter sidewalks, paths, trails, pocket park, civic uses, open shelters, fenced dog parks, playgrounds, community gardens, playing fields, crossing streets, easements, stormwater features |
| SUBAREAS | |
| Allowed in Subarea(s) | All Subareas |

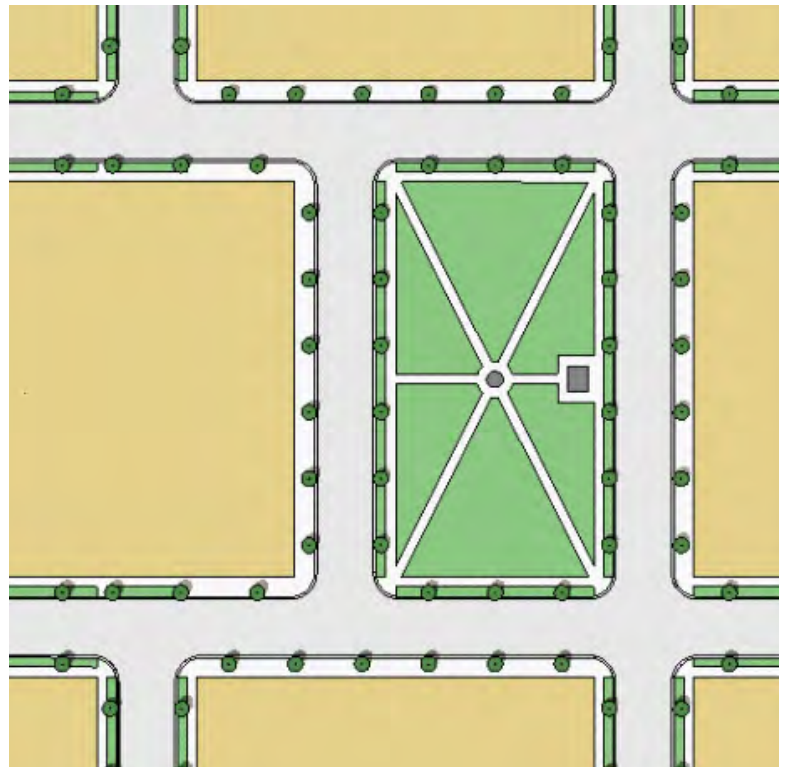
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Square

DESCRIPTION A stand-alone and independent publicly accessible open space available for recreation and civic purposes.



Character Examples



| LOCATION | |
|--------------------------------|--|
| Characteristics | Spatially defined by building frontages and located at intersections of the primary street network |
| REQUIREMENTS | |
| Minimum Size | 1/4 acre (2 acre maximum) |
| Suggested Frontage on at Least | Street |
| Publicly Accessible | Yes |
| Accessway required | Yes |
| Accessway(s) allowed | Sidewalk, independent shared use path |
| ELIGIBLE FEATURES | |
| Include: | Paths, lawns, and trees formally disposed, pocket park |
| SUBAREAS | |
| Allowed in Subarea(s) | All Subareas |

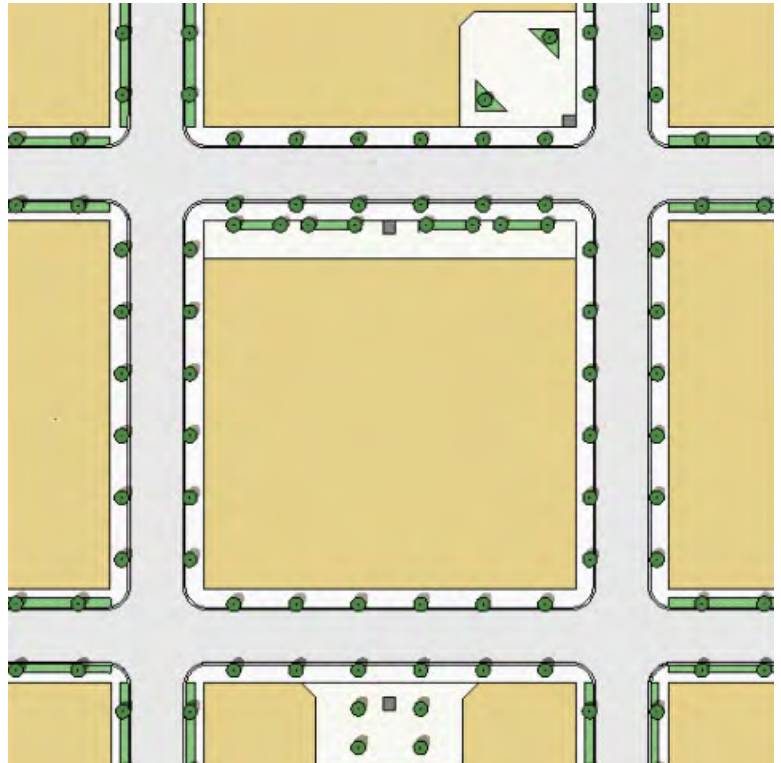
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Plaza

DESCRIPTION A publicly accessible open space integrated into public realm or building entries available for civic purposes and commercial activities.



Character Examples



| LOCATION | |
|--------------------------------|--|
| Characteristics | Spatially defined by building frontages and located at intersections of the primary street network |
| REQUIREMENTS | |
| Minimum Size | 1/8 acre (2 acre maximum) |
| Suggested Frontage on at Least | 1 Street |
| Publicly Accessible | Yes |
| Accessway required | Yes |
| Accessway(s) allowed | Sidewalk, independent shared use path |
| ELIGIBLE FEATURES | |
| Include: | Hardscape, pavement, landscape features, trees are optional |
| SUBAREAS | |
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-10, WC-12 |

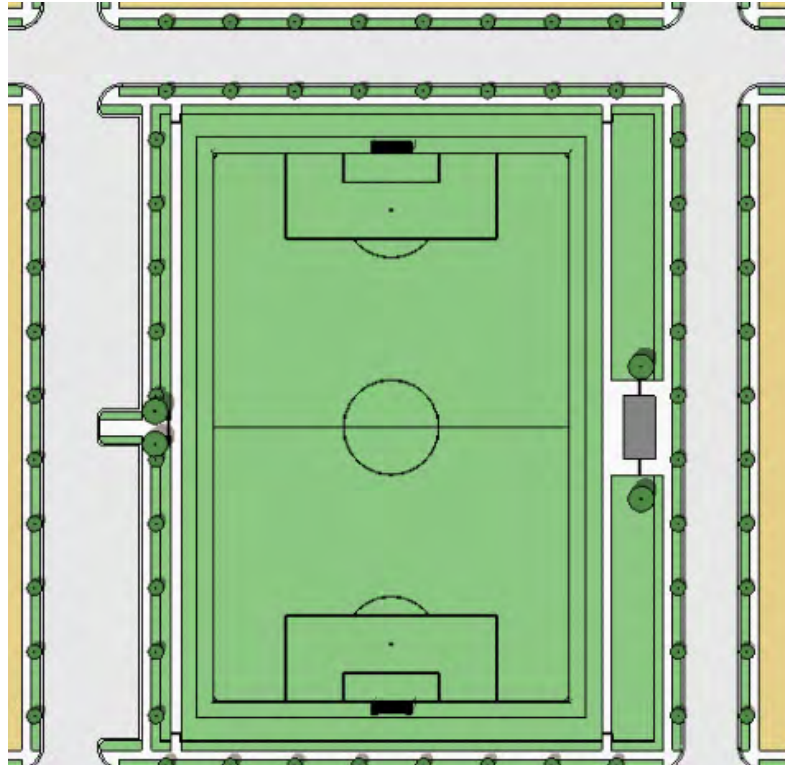
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Playing Field

DESCRIPTION A publicly accessible open space designed and equipped for active recreation.



Character Examples



| LOCATION | |
|--------------------------------|--|
| Characteristics | Located where active recreation uses would be easily accessed |
| REQUIREMENTS | |
| Minimum Size | None |
| Suggested Frontage on at Least | 1 Street |
| Publicly Accessible | Yes |
| Accessway required | Yes |
| Accessway(s) allowed | Sidewalk, independent shared use path |
| ELIGIBLE FEATURES | |
| Include: | Streets, parking, associated structures and shelters, concessions and lighting. Fields may be fenced and may be included within parks. |
| SUBAREAS | |
| Allowed in Subarea(s) | WC-1, WC-2, WC-8, WC-9, WC-12 |

2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Playground

DESCRIPTION A publicly accessible open space designed and equipped for recreation of children.



Character Examples



| LOCATION | |
|--------------------------------|--|
| Characteristics | Located where active recreation uses would be easily accessed, interspersed within residential areas and may be placed within a block or accessed via a lane |
| REQUIREMENTS | |
| Minimum Size | None |
| Suggested Frontage on at Least | 1 Street |
| Publicly Accessible | Yes |
| Accessway required | Yes |
| Accessway(s) allowed | Sidewalk, trail, independent shared use path |
| ELIGIBLE FEATURES | |
| Include: | Open shelter, fence, playground equipment, parking, hardscape, and landscape features. Playgrounds may be included within parks and greens. |
| SUBAREAS | |
| Allowed in Subarea(s) | All Subareas |

2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Park

DESCRIPTION Open space available for passive recreation. A park may be independent of surrounding building frontages. Its landscape may consist of meadows, water bodies, wetlands, and woodlands, all naturalistically disposed.



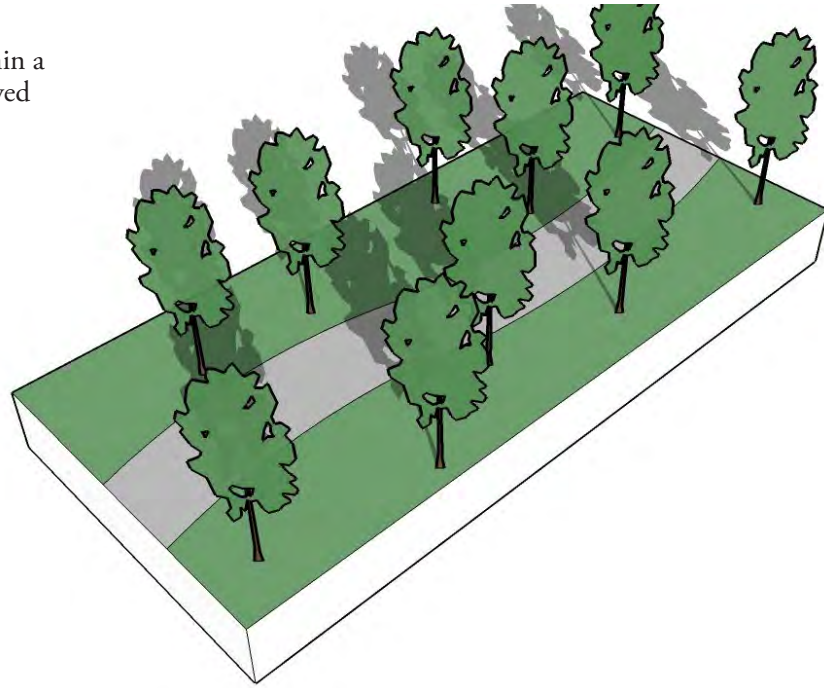
Character Examples

| LOCATION | |
|--------------------------------|---|
| Characteristics | Located where regulated natural features create an opportunity |
| REQUIREMENTS | |
| Minimum Size | 2 acres |
| Suggested Frontage on at Least | May be independent of street network |
| Publicly Accessible | Yes |
| Accessway required | No |
| Accessway(s) allowed | Perimeter sidewalks, trail, independent shared use path |
| ELIGIBLE FEATURES | |
| Include: | Conserved Green Space, water bodies, wetlands, active recreation and buffers that may be lineal following natural corridors, boat launch, civic uses, open shelters, retail and food kiosks, fenced dog parks, playgrounds and playing fields, community gardens, access and crossing streets, easements, parking and stormwater features |
| SUBAREAS | |
| Allowed in Subarea(s) | All Subareas |

2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE COMPONENT: Shared Use Path

DESCRIPTION A trail for pedestrian and bicycle use within a Shared Open Space or Conserved Green Space.



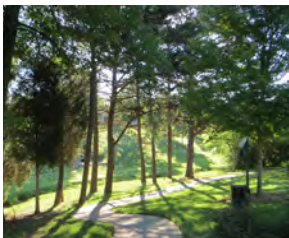
Character Examples

| LOCATION | |
|-----------------------|---|
| Characteristics | A combination of a pedestrian and bicycle trail in a Shared Open Space or Conserved Green Space |
| REQUIREMENTS | |
| Minimum Width (feet) | 10 minimum/14 maximum |
| Publicly Accessible | Yes – connected to the street and pedestrian network |
| ELIGIBLE FEATURES | |
| Include: | All Open Space Types |
| SUBAREAS | |
| Allowed in Subarea(s) | All Subareas |

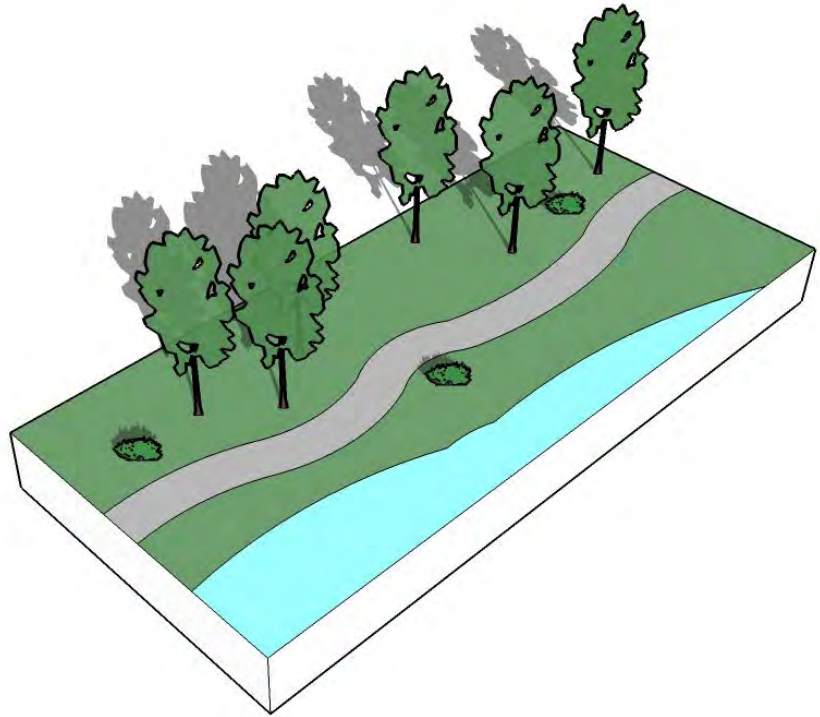
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Recreation Trails

DESCRIPTION A path for pedestrian use within a Shared Open Space or Conserved Green Space.



Character Examples



| LOCATION | |
|-----------------------|--|
| Characteristics | Pedestrian trail in a Shared Open Space or Conserved Green Space |
| REQUIREMENTS | |
| Maximum Width (feet) | 6 |
| Publicly Accessible | Yes – connected to the street and pedestrian network |
| ELIGIBLE FEATURES | |
| Include: | All Open Space Types |
| SUBAREAS | |
| Allowed in Subarea(s) | All Subareas |

2.3.7 PUD Subdivision Signage

PUD Subdivision Signage

The following parameters pertain to the installation, use, maintenance, and lighting standards of signs for PUD Subdivision submittals.

- A. Permitted Project Signs – The following signs are permitted in the Project, unless limited by the later provisions of this Section:
 1. Construction signs;
 2. Street control devices;
 3. Development signage;
 4. Signs not requiring a permit in accordance with *Section 3.11.5 of the Londonderry Zoning Ordinance*.
- B. Approval Authority – All Project signage requiring a permit shall be reviewed for conformity to this PUD Master Plan by the Zoning Administrator or appropriate authority.
- C. Appeals – The petitioner shall be entitled to appeal staff denials to the Planning Board.
- D. Measurement and calculation of area – The calculation shall be in accordance with *Section 3.11.5 of the Londonderry Zoning Ordinance* except that sign height may exceed ten (10) feet as indicated below.
- E. Submittal Requirements – All individual signage applications independent of PUD Subdivision submittals shall include the following:
 1. Written approval from the Developer for proposed signage;
 2. Front, side and top elevations with sign drawn to scale;
 3. Color and material descriptions;
 4. Illumination method for the sign (if any);
 5. Landscaping for the sign (if any).

Design, Construction and Maintenance

- Signs or their supports shall not be placed in such position or manner as to obstruct or interfere, either physically or visually, with any fire alarm, police alarm, traffic signal or sign, or any devices maintained by or under public authority; or with vehicular or pedestrian ingress or egress to or from any public or private right-of-way, roadway, driveway, or sidewalk.
- All signs shall be constructed and secured so as to ensure the safety of pedestrians.
- The material and construction of any sign shall be in accordance with the requirements above and the Building Code.
- Permanent signs shall be made of durable materials.
- All signs shall be maintained in a safe condition and, together with their structural elements, shall be kept in good repair.

Street Control Devices

- Traffic Control Devices – In accordance with NHDOT Standard Specifications and the Manual on Uniform Traffic Control Devices, current as of the date of PUD Site Plan submittal, for the design, size and placement of traffic control devices. Any such signs to be placed near a roadway or an overhead sign structures shall be designed in accordance with the AASHTO standard specification for structural supports for highway traffic signs, luminaries, and traffic signals.
- Street Identification Signs – Design and sized in accordance with the Town of Londonderry standards, placement in accordance with the Manual on Uniform Traffic Control Devices, current as of the date of PUD Site Plan submittal. The Developer may select decorative sign poles and accent trim for Woodmont Com-

2.3.7 PUD Subdivision Signage

mons or for a Subarea within the development. Street signs also may include custom Woodmont Commons logos. The style, materials and finish shall be submitted to the Town for review and approval at the time of the first PUD Site Plan submittal for the Development or the Subarea.

- Regulatory, Warning & Guide Signs – Included stop signs, speed limit signs, parking signs, route signs, etc. in accordance with NHDOT Standard Specifications and the Manual on Uniform Traffic Control Devices, current as of the date of PUD Site Plan submittal, for the design, size and placement of signage. The Developer may select decorative sign poles and accent trim for Woodmont Commons or for a Subarea within the development. The style, materials and finish shall be submitted to the Town for review and approval at the time of the first PUD Site Plan submittal for the Development or the Subarea.
- Pavement Marking – In accordance with the Manual on Uniform Traffic Control Devices, current as of the date of PUD Site Plan submittal, for the design, size and placement of pavement marking for streets.



Example of Gateway Sign

Development Signage

Identification Signage

- Gateway signs – These signs are permitted for the primary commercial entrances into the Woodmont Commons Development. The signs shall be freestanding ground signs and may include the Woodmont Commons name and or logo or other community branding. The sign area shall not exceed sixteen (16) square feet and signs may be located on each side of an entrance or within a median. Signs shall not be permitted at perimeter entrances abutting AR-I zoning. The signs may be illuminated using ground-mounted lights.

2.3.7 PUD Subdivision Signage

- Pole mounted directory signs – Pole mounted directory signs shall not exceed eight (8) square feet per side, not including the decorative pole and frame. The sign shall be at least one (1) foot from the edge of the street curb and seven feet above any walking surface. They shall be spaced at least twenty (20) feet apart.
- The copy on the directional signage may include the Woodmont Commons name and/or logo, directional information and business names and logos. The number of signs shall be indicated on PUD Site Plan submittals. Directional signs may be externally illuminated. The Developer shall be responsible for the maintenance of all directional signage.



Example of Pole Mounted Directory Sign

- Ground mounted directory signs – Ground mounted directory signs shall not exceed eighteen (18) square feet per side, not including the decorative stanchions. The sign shall be at least one (1) foot from the edge of the street curb. Signs shall be limited to eight (8) feet high, not including decorative stanchions.
- Wayfinding – Vehicular directional signage for wayfinding within the Development may be included within the public right-of-way within the WC-1-GL, WC-1, WC-2, WC-8 and WC-12 Subareas.
- Banners – The Developer or their assignee may establish a banner program for the street light poles within the WC-1-GL, WC-1, WC-2, WC-8 and WC-12 Subareas. The banners shall be a minimum of seven (7) feet above any walking surface and may project out eighteen (18) inches from the pole. The copy on the banners may include the Woodmont Commons name and or logo or other community branding and seasonal or event information. Banners shall not be illuminated. The Developer shall be responsible for the maintenance of all banners. Banners shall not require a permit.



Example of Ground Mounted Directory Sign

2.3.8 PUD Subdivision Lighting

The standards are designed to reduce the impacts of glare, light trespass and over lighting and to promote safety, security and energy conservation. Lighting should emphasize both public and private features and destinations by using a minimum amount of light to meet these objectives. It should provide needed illumination of the site and, at the same time, prevent undesired off-site glare.

The purpose of the exterior lighting standards is to create a well-balanced, integrated lighting plan for rights-of-way and public properties that enhances vehicular and pedestrian visibility while minimizing lighting glare and contrast. The intent of the PUD Subdivision Lighting Standards is to provide consistent systems of pedestrian lighting that add to the character, aesthetic appeal, and safety, and thereby promote greater pedestrian activity. Outdoor lighting for PUD Subdivision Plans shall consist primarily of pedestrian-scale Streetlights.

Applicability

These outdoor lighting standards for PUD Subdivision and/or PUD Site Plans apply to the installation of new outdoor lighting fixtures or the replacement of any existing outdoor light fixtures located within the public rights-of-way for the Woodmont Commons PUD. The standards are designed to reduce the impacts of glare, light trespass and over lighting and to promote safety, security and energy conservation.

Guidelines

Street lighting shall be designed to cast light downward. Cut-off and/or shielded fixtures shall be used to reduce the impacts of glare on adjacent uses. The maximum mounting height of a non-building mounted lighting fixture above the ground shall be twenty (20) feet in residential areas and twenty-five (25) feet in nonresidential areas.

The placement of fixtures should provide a coordinated and organized appearance that works with

placement of street trees, curb cuts, signage and other features to contribute to the overall continuity of the streetscape.

Light sources should generally be metal halide or LED. Low wattage high-pressure sodium may be desirable in some residential settings where glare may be an issue.

Outdoor lighting is used to illuminate flags, monuments, signs, landscapes and other features must use cut-off or directionally shielded light fixtures that are aimed and controlled so that the light is substantially confined to the object to be illuminated. Such lighting must be shielded to prevent:

- Glare for drivers;
- Light trespass beyond the Woodmont Commons property line.

All outdoor lighting shall be subject to the Outdoor Lighting Standards of the *Londonberry Site Plan Regulations* except as amended herein:

- Internally lit signs are acceptable provided that they meet the requirements of the PUD Master Plan.
- Adjacent to residential property at the perimeter of the Woodmont Commons PUD, no direct light source will be visible at the property line at ground level or above except as permitted for street lights and traffic control and safety devices, for streets, paths and trails;
- Development signage lighting may remain on after business hours.

Athletic Field Lighting

Sports field lighting systems are designed and evaluated based on existing standards that are readily available and generally accepted. These standards are:

- Illuminating Engineering Society of North America (IESNA);

2.3.8 PUD Subdivision Lighting

- Sports and Recreational Area Lighting (IESNA RP-6);
- Lighting for Exterior Environments (IESNA RP-33);
- Guide for Photometric Measurements of Area and Sports Lighting Installations (IESNA LM-5);
- Light Trespass: Research, Results and Recommendations (IESNA TM-11);
- National Little League Association Standards and Safety Audit.

If fields are to be illuminated, the requirements are summarized below.

- A. Illuminating Engineers Society of North America (IESNA) specifications shall become the standard for onfield performance, and that a qualified engineer with IESNA experience shall be retained to prepare the design the lighting.
- B. With respect to off-field lighting performance, the lighting shall be designed as per the following:
 1. That spill light shall be limited to the maximum allowed by IESNA publications TM-11 and RP-33 using the methods for measurement outlined in these publications;
 2. That glare shall be controlled by limiting the amount of candlepower as calculated from the worst-case aimed fixture;
 3. That sky glow shall be controlled by limiting the amount of uplight from the fixtures to that allowed from an IESNA cutoff classification.

2.3.9 PUD Subdivision Landscape

Landscaping within each of the public rights-of-way is used to provide an attractive setting for the Woodmont Commons PUD, enhance the quality of the environment, provide shade for pedestrians, reduce heat island effects, slow the vehicular traffic, and soften building and structure edges.

PUD Subdivision Plan landscaping shall be in accordance with the Landscape Design Standards of the *Londonderry Site Plan Regulations* except as modified herein and as follows:

The landscaping along the streets and travelled ways in the Woodmont Commons PUD include trees, landscaped medians and planting strips.

Street trees shall be planted within the street right-of-way generally and approximately as are illustrated in *Section 2.3.3 Transportation Network*. However, street trees may be shifted or eliminated along any street section as long as the number trees equals an average spacing of fifty (50) feet on center. No street trees shall be required within twenty-five (25) feet of an intersection. The size of trees to be planted in this area shall be a minimum of 2 ½ inch caliper at time of planting. Branching height of street trees shall not be less than seven (7) feet above grade when planted. Street trees must not be closer than twenty-five (25) feet apart at time of planting. Street trees may be located within planting strips or planting wells with tree grates. Trees shall not be required along sides of streets abutting tree preservation areas or open space where internal trees are planted and visible from the street.

In addition, landscaping will be governed by the following principles:

- Landscaping may be laid out in formal rows or informal drifts.
- Plant material located within twenty (20) feet of any road or other paved area will generally consist of species which are tolerant of roadway deicing methods and otherwise planted and maintained so as to maintain a pleasing public realm.

- Landscaping shall be designed so that it does not substantially interfere with vehicular or pedestrian sight distances as to create a public safety hazard.
- Alleys and lanes do not require street trees.
- Street trees are not required in areas where orchard-like planting is preserved or planted.
- Street trees are not required where existing trees are preserved in quantities sufficient to satisfy the street tree requirements.
- Median trees are not required but may be planted. There is no minimum or maximum spacing requirements.
- Invasive plant species will not be permitted.

Medians and Planting Strips – In addition to trees, medians and planting strips shall be covered with a minimum thickness of four (4) inches of suitable topsoil and be subsequently planted with grass seed, sod, or other vegetative cover.

Signage Landscaping – The ground mounted signage landscaping area shall be a minimum of a four (4) foot wide area surrounding each monument or pedestal sign base.

Street tree sizes and layout shall be in accordance with this Section.

All areas disturbed by construction and slated to remain vegetated shall be covered with a minimum thickness of four (4) inches of suitable topsoil and be subsequently planted with grass seed, sod, or other vegetative cover. Trees may be introduced into open spaces and may be used to satisfy the number of street trees required above.

2.3.10 Cultural Resources

The cultural resources on the Woodmont Common property include stone walls along existing property lines. The walls along the Woodmont Common exterior boundary will be preserved. Relocation of internal walls shall be permitted. Portions of stone walls disturbed by development shall be rebuilt in accordance with *Section 3.09c under 11h* of the *Londonderry Site Plan Regulations* so that they are visible by the public.

A portion of the site's agricultural heritage will be preserved within the buffer along Gilcreast Road.

2.3.11 Utility Infrastructure

The Woodmont Commons PUD shall be served by public and private utilities as required for the specific tenant uses identified in the PUD Subdivision and/or PUD Site Plan applications. As described in *Section 3.05* of the *Londonderry Subdivision Regulations* dated June 10, 2009, the Developer shall provide a Utility Clearance Letter from each applicable non-municipal utility provider. Other accommodations with municipally owned utilities shall be coordinated directly with staff from the Department of Public Works. The location of planned public and private utilities shall not be subject to the location requirements at the end of *Section 3.05*; the utilities shall be located within the planned roadway or utilities rights-of-way or easements, whether publicly or privately held, in a manner that allows for continued maintenance of the utility system.

Water

- Approved Methods – The applicant shall identify water service to or through each phase of the PUD Subdivision upon submission of a PUD Subdivision application as identified in *Section 3.06* of the *Londonderry Subdivision Regulations*. This shall be coordinated with Pennichuck Water Works Company (PWWC) or any other licensed entity providing municipal water service to the Woodmont Commons PUD.
- Performance and Operational Standards – The applicant shall provide written evidence that the domestic water supply volume and pressure and fire flow requirements are satisfactory for the proposed PUD Subdivision and does not compromise the minimum service requirements for abutting residences and businesses as defined by PWWC and the Londonderry Fire Department.

Sewer

- Approved Methods – The applicant shall identify a municipal sanitary sewer system service to each area of the PUD Subdivision as required in *Section 3.07* of the *Londonderry Subdivision Regulations* and consistent with the conceptual layout provided in the PUD Master Plan.
- Performance and Operational Standards – The applicant shall provide the sewer capacity analysis for each PUD Subdivision unless waived by the Londonderry Department of Public Works due to site-specific conditions.

Stormwater Management

- Approved Methods – The applicant shall identify a storm drain system for PUD Subdivision as identified in *Section 3.08* of the *Londonderry Subdivision Regulations* and consistent with site layout and treatment measures provided in the PUD Master Plan.
- Performance and Operational Standards – The applicant shall provide analysis and stormwater treatment, infiltration, and/or detention measures that mitigate the identified impacts from the PUD Subdivision. The documentation provided to the Town of Londonderry shall be coordinated with the documents submitted to the New Hampshire Department of Environmental Services for those portions of the project that require their review based on the State's written thresholds for an Alteration of Terrain (AOT) Permit, if applicable.

2.4 PUD SITE PLAN REGULATIONS AND STANDARDS

2.4.1 General

The PUD Site Plan Regulations and Standards regulate how the land within the Woodmont Commons PUD will be developed at the building and lot level. These regulations and standards apply to the developable areas as indicated on the Land Use Plans.

Note that the Compliance Alternatives in *Section 2.2.1 Description* apply to the PUD Site Plan Regulations and Standards.

2.4.2 Buildings and Lots

The placement and location of building and parking shall be regulated by the Lot Types and the vertical built form is regulated by the Building Types. In addition, the Building Types include both architectural standards and architectural guidelines. The standards are mandatory but the guidelines are not. The guidelines are provided in order to inform developers of the design objectives and guide the review of all submittals with the goal of creating building designs that are sensitive to their context within the community.

Specific Lot Types permitted in Subareas are indicated for each type, but the uses permitted in Subareas are regulated by the Allowable Uses table.

The building placement on the lot is controlled by both the front setback and the frontage occupancy. The front facade of the structure must be between the minimum and maximum setback lines. Encroachments may be permitted into the setback and in some cases, over the setback in accordance with *Section 2.4.3 Encroachment and Shared Use Agreements*.

The placement of parking on the lot indicates how far behind the primary façade the parking space, lot or structure must be located. It is contingent upon the building's placement within the setback. Note that for single family and for duplex dwellings the parking

setback is for the garage or regular overnight parking and that short term parking is permitted within the setback on front loading driveways.

Lots

There may be special circumstances affecting lots that may require a deviation from the lot standards. This includes naturally occurring conditions such as but not limited to slope, unstable soil conditions and preservation of existing trees. Any such circumstances shall be clearly noted on the site plan submittal for consideration by the staff and Planning Board. Approval of any modification to the site plan requirements is at the discretion of the Planning Board.

Architectural Guidelines

Building design should reinforce community character of the Woodmont Commons PUD and should be visually and physically compatible with adjacent uses. Public façades, ground floors, entrances, storefront windows, and signage and lighting should be designed to articulate the pedestrian scaled architecture and create visual interest. Buildings should transition in scale and height as they approach properties of smaller scale.

The Architectural Standards and Guidelines are included with each Building Type and supersede *Section 3.12 Building and General Appearance Design Standards* of the *Londonderry Site Plan Regulations*.

Rooftop mechanical equipment shall be screened from view from streets and public frontages. Screening may be provided by extending the parapet of the building; by a roof; or by enclosing the mechanical equipment with a roof equipment screen.

Scale and Proportion Guidelines

Scale in buildings is relative to size. It refers to how we perceive the size of the building elements relative to other forms, and to the human body. There are two

2.4.2 Buildings and Lots

types of scale addressed by the guidelines. They are overall scale and human scale.

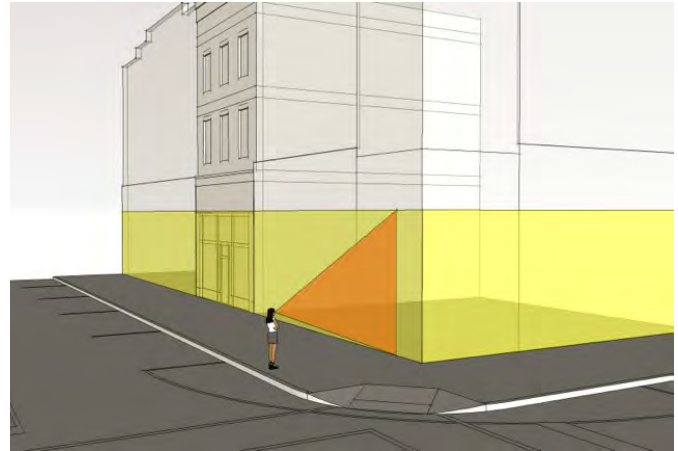
Overall scale is the perception of a building from a distance. For example, a building may be segmented into a base, middle and top to improve the perception of the building from a distance. Human scale is the legibility of elements when one is very close to a building. The detailing of the storefront in a commercial building is a good example. Good buildings should incorporate both types of scale simultaneously with an emphasis on human scale.

It is important that up close, buildings possess a level of refinement that is tangible. Detaining within the pedestrian cone of vision not only provides comfort by allowing one to judge the size of a space, but also give the building a human scale.

Proportion refers to the relationship of the parts to each other or to the whole. This can include the relationship of two ratios such as height to width. When building elements are in proportion, their relative sizes are the same.

A building's architectural features can be used to organize the perceived mass of larger buildings. Building features such as columns, piers, roof lines and brick patterns can divide and create vertical orientation on a large surface. This can refer to the overall building facade as well as openings for windows and doors within it.

Vertical proportions are preferred and consistent proportioning systems are encouraged, preserving a width-to-height ratio along a facade. This ratio can differ among facades, but once chosen should be used for the entire facade. Once these proportions have been established, the windows and doors should reinforce the vertical orientation of the composition.



Human Scale

Refinement most effective up close within the pedestrian cone of vision



Vertical Proportion

An example of a vertical proportioning system used for the facade and the windows.

2.4.2 Buildings and Lots

Accessory Structures

Accessory Structures may be allowed on any Lot Type in accordance with the definition of Accessory Structure in *Section 4.7 Definitions* of the *Londonderry Zoning Ordinance*.

| List of Building and Lot Types | | |
|--------------------------------|----------|-----|
| | BUILDING | LOT |
| SINGLE-FAMILY DETACHED | 174 | 175 |
| SINGLE-FAMILY ATTACHED | 176 | 177 |
| TWO-FAMILY | 178 | 179 |
| MULTI-FAMILY | 180 | 181 |
| SMALL BUILDING | 182 | 183 |
| MEDIUM BUILDING | 184 | 185 |
| LARGE BUILDING | 186 | 187 |
| CIVIC BUILDING | 188 | 189 |
| DEDICATED OFFICE | 190 | 191 |
| LARGE FORMAT RETAIL | 192 | 193 |
| INSTITUTIONAL BUILDING | 194 | 195 |
| PARKING STRUCTURE | 196 | 197 |
| FLEX | 198 | 199 |

2.4.2 Buildings and Lots

BUILDING TYPE: Single-Family Detached

DESCRIPTION Dwelling units that are consistent with traditional residential development patterns. Cottage Courts are allowed. Façade orientation to the street, walkways to entrances and landscaping to define street edges and open spaces contribute to the neighborhood scale. Site may have a detached or attached accessory structure.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

| | |
|--|----------------------|
| Ground Floor Limitations | Residential |
| Building Height | |
| Primary Building Height (maximum) | 3 stories / 35' |
| Ground Floor Height (feet) | Not applicable |
| Upper Floor Height (feet) | Not applicable |
| Finished Floor Elevation (above grade at front façade) | 18" minimum |
| Main Roof Pitch | 4:12 min / 12:12 max |
| Flat Roofs Permitted | No |

TRANSPARENCY AND ARTICULATION

| | |
|---|----------------|
| Ground Floor Transparency (% of façade) | Not applicable |
| Upper Floor Transparency (% of façade) | Not applicable |
| Street Frontage Wall Length Without Offset (feet) | Not applicable |
| Street Frontage Wall Offset – length/depth (feet) | Not applicable |

BUILDING ENTRANCES

| | |
|---------------------------------|----------------|
| Street Facing Entry Required | Yes |
| Entrance Spacing (maximum feet) | Not applicable |

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry or to a common court in the case of Cottage Court units. Vehicular access may be provided at the frontyard, sideyard or rear yard

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street including the building entry. Semi-private outdoor space shall be oriented to the street if provided at the frontyard

TREATMENT OF GROUND FLOOR

None

SCALE AND PROPORTION

Massing of building shall be consistent with neighboring homes and existing development. Buildings shall have a pedestrian scale in building height and bulk

VERTICAL AND HORIZONTAL EMPHASIS

None

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street. The building massing shall reinforce the prominence of the entry as a building feature

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern of punched openings. Windows and doors shall be scaled to reinforce a residential and pedestrian character

ROOF FORM

Main roof shall be pitched. Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

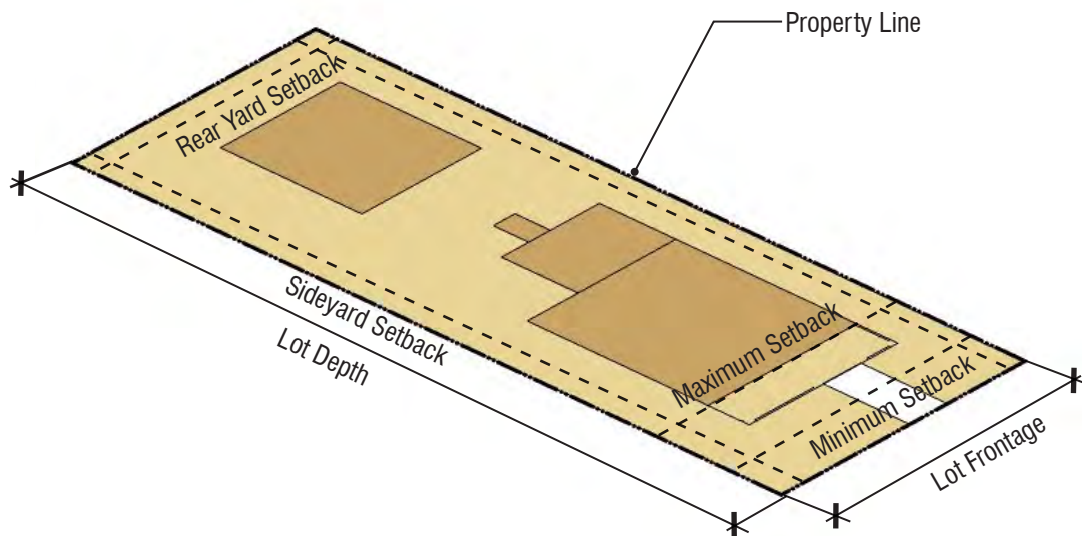
Balcony, bay window, porches, stoops, stairs

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Single-Family Detached



LOT STANDARDS (APPLY AS SHOWN BELOW UNLESS AFFECTED BY A PERIMETER BUFFER AS SHOWN IN SECTION 2.2.2 LAND USE PLAN)

| | |
|------------------------|---------------|
| Lot Frontage (feet) | 40 minimum |
| Lot Depth (feet) | 80 minimum |
| Lot Area (square feet) | 3,200 minimum |

BUILDING PLACEMENT ON LOT

| | |
|--|-----------------------|
| Front Yard Setback (feet) | 10 minimum/25 maximum |
| Side Yard Setback (feet) | 10 minimum |
| Rear Yard Setback (feet) | 5 minimum |
| Side-Street Yard (feet on corner lots) | 20 minimum |
| Frontage Occupancy (%) | 30% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|------------|
| Front Parking Setback from primary façade (feet) | 20 minimum |
| Side and Rear Parking Setback (feet) | 5 minimum |

PARKING PLACEMENT ON SIDE STREETS

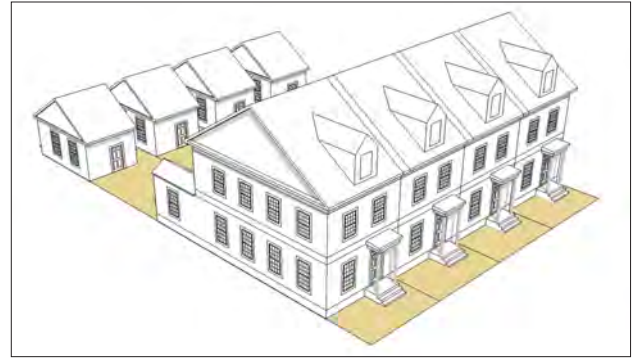
| | |
|--------------------------|----------------|
| Front Lot Setback (feet) | Not applicable |
|--------------------------|----------------|

SUBAREAS

| | |
|-----------------------|--------------------------------------|
| Allowed in Subarea(s) | All Subareas except WC-1-GL and WC-3 |
|-----------------------|--------------------------------------|

2.4.2 Buildings and Lots

BUILDING TYPE: Single-Family Attached



DESCRIPTION A small free-standing building that may have a detached or attached accessory garage structure. The detached building type shall be oriented to the street with an articulated primary entry.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

| | |
|--|----------------------|
| GROUND FLOOR LIMITATIONS | Residential |
| BUILDING HEIGHT | |
| Primary Building Height (maximum) | 3 stories / 35' |
| Ground Floor Height (feet) | Not applicable |
| Upper Floor Height (feet) | Not applicable |
| Finished Floor Elevation (above grade at front façade) | 18" minimum |
| Main Roof Pitch | 4:12 min / 12:12 max |
| Flat Roofs Permitted | No |

TRANSPARENCY AND ARTICULATION

| | |
|---|----------------|
| Ground Floor Transparency (% of façade) | Not applicable |
| Upper Floor Transparency (% of façade) | Not applicable |
| Street Frontage Wall Length Without Offset (feet) | Not applicable |
| Street Frontage Wall Offset – length/depth (feet) | Not applicable |
| BUILDING ENTRANCES | |
| Street Facing Entry Required | Yes |
| Entrance Spacing (maximum feet) | Not applicable |

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Vehicular access shall be provided at the side yard or rear of the site

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street along with the building entry. Semi-private outdoor space shall be oriented to the street if provided at the frontyard

TREATMENT OF GROUND FLOOR

None

SCALE AND PROPORTION

Architectural elements shall be employed to reduce overall scale of continuous blocks and to articulate a residential scale

VERTICAL AND HORIZONTAL EMPHASIS

None

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern of punched openings. Windows and doors shall be scaled to reinforce a residential character

ROOF FORM

Main roof shall be pitched. Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

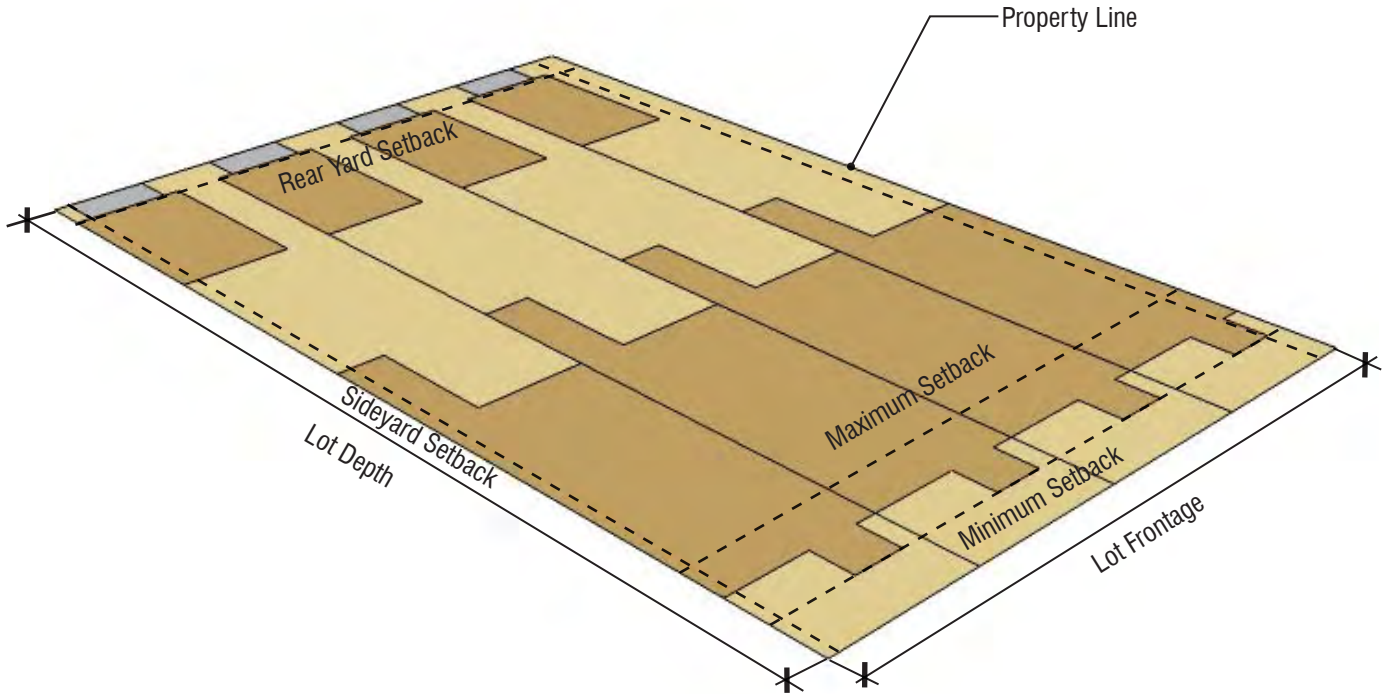
Balcony, bay window, porches, stoops, stairs

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Single-Family Attached



LOT STANDARDS APPLY AS SHOWN BELOW UNLESS AFFECTED BY A PERIMETER BUFFER AS SHOWN IN SECTION 2.2.2 LAND USE PLAN)

| | |
|------------------------|-----------------------|
| Lot Frontage (feet) | 18 minimum/30 maximum |
| Lot Depth (feet) | 45 minimum |
| Lot Area (square feet) | 810 minimum |

BUILDING PLACEMENT ON LOT

| | |
|--|----------------------|
| Front Yard Setback (feet) | 5 minimum/25 maximum |
| Side Yard Setback (feet) | 0 or 5 at corners |
| Rear Yard Setback (feet) | 5 |
| Side-Street Yard (feet on corner lots) | 20 minimum |
| Frontage Occupancy (%) | 30% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|--|
| Front Parking Setback from primary façade (feet) | Not applicable; parking in rear of lot |
| Side and Rear Parking Setback (feet) | 5 minimum |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|----------------|
| Front Lot Setback (feet) | Not applicable |
|--------------------------|----------------|

SUBAREAS

| | |
|-----------------------|---|
| Allowed in Subarea(s) | WC-1, WC-2, WC-4, WC-6, WC-8, WC-9, WC-10, WC-11, WC-12 |
|-----------------------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Two-Family



DESCRIPTION A small free-standing residential building with two dwelling units that may have a detached or attached accessory garage. The primary building type shall be oriented to the street with an articulated primary entry.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

| | |
|--|----------------------|
| GROUND FLOOR LIMITATIONS | Residential |
| BUILDING HEIGHT | |
| Primary Building Height (maximum) | 3 stories / 35' |
| Ground Floor Height (feet) | Not applicable |
| Upper Floor Height (feet) | Not applicable |
| Finished Floor Elevation (above grade at front façade) | 18" minimum |
| Main Roof Pitch | 4:12 min / 12:12 max |
| Flat Roofs Permitted | No |

TRANSPARENCY AND ARTICULATION

| | |
|---|----------------|
| Ground Floor Transparency (% of façade) | Not applicable |
| Upper Floor Transparency (% of façade) | Not applicable |
| Street Frontage Wall Length Without Offset (feet) | Not applicable |
| Street Frontage Wall Offset – length/depth (feet) | Not applicable |

BUILDING ENTRANCES

| | |
|---------------------------------|----------------|
| Street Facing Entry Required | Yes |
| Entrance Spacing (maximum feet) | Not applicable |

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be configured to appear as a single family residence

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern of punched openings. Windows and doors shall be scaled to reinforce a residential and pedestrian character

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street including the building entry

ROOF FORM

Main roof shall be pitched. Roof style and pitch may vary

TREATMENT OF GROUND FLOOR

None

ALLOWABLE SIGNAGE TYPES

Address signs

SCALE AND PROPORTION

Massing of building shall be consistent with neighboring homes and existing development. Buildings shall have a pedestrian scale in building height and bulk

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting and pedestrian lighting

VERTICAL AND HORIZONTAL EMPHASIS

None

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

Balcony, bay window, porches, stoops, stairs

LOCATION AND ARTICULATION OF ENTRY

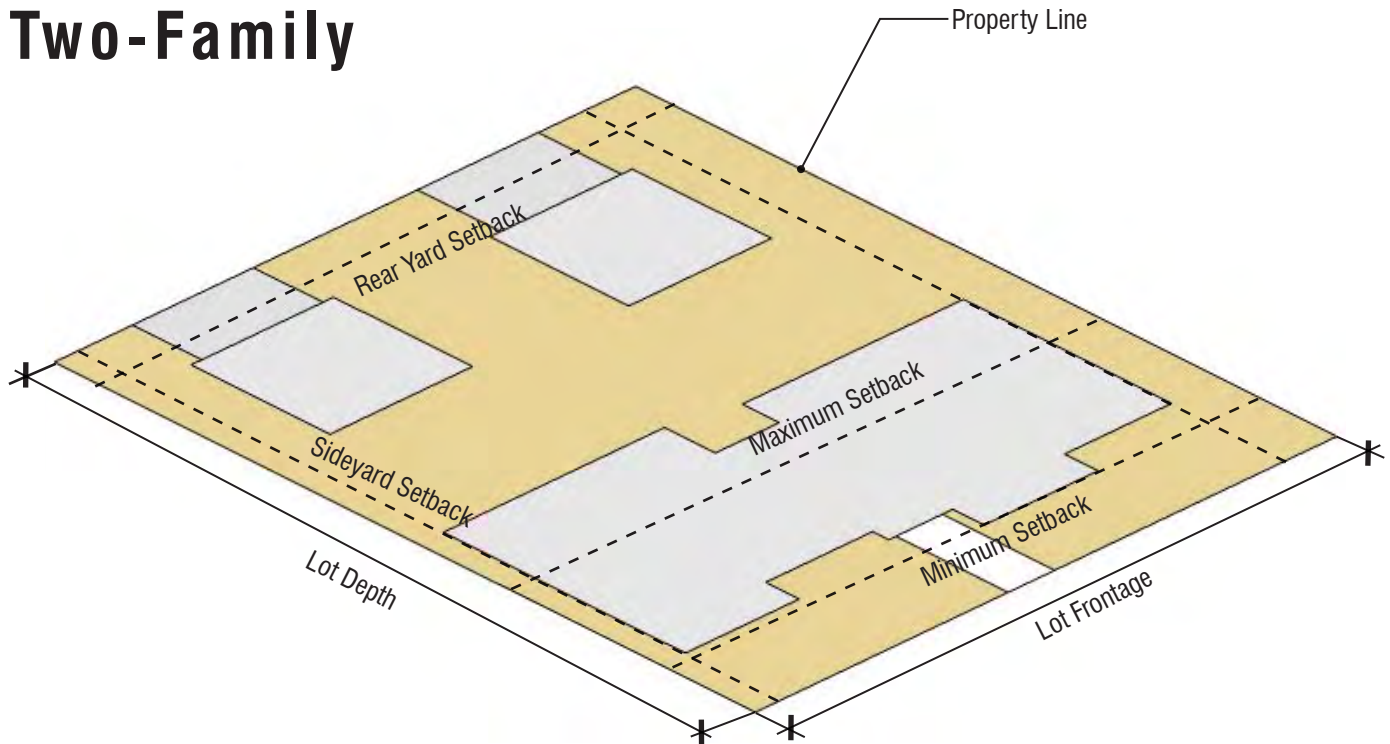
Building entry shall be oriented to the principal street

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Two-Family



LOT STANDARDS APPLY AS SHOWN BELOW UNLESS AFFECTED BY A PERIMETER BUFFER AS SHOWN IN SECTION 2.2.2 LAND USE PLAN)

| | |
|------------------------|---------------|
| Lot Frontage (feet) | 40 minimum |
| Lot Depth (feet) | 80 minimum |
| Lot Area (square feet) | 3,200 minimum |

BUILDING PLACEMENT ON LOT

| | |
|--|-----------------------|
| Front Yard Setback (feet) | 10 minimum/25 maximum |
| Side Yard Setback (feet) | 5 minimum |
| Rear Yard Setback (feet) | 5 minimum |
| Side-Street Yard (feet on corner lots) | 20 minimum |
| Frontage Occupancy (%) | 30% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|------------|
| Front Parking Setback from primary façade (feet) | 20 minimum |
| Side and Rear Parking Setback (feet) | 5 minimum |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|----------------|
| Front Lot Setback (feet) | Not applicable |
|--------------------------|----------------|

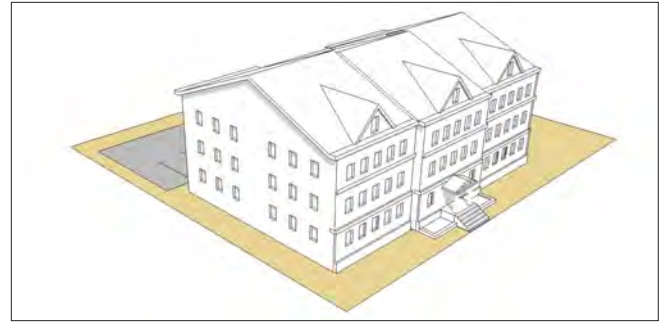
SUBAREAS

| | |
|-----------------------|---|
| Allowed in Subarea(s) | WC-1, WC-2, WC-4, WC-6, WC-8, WC-9, WC-10, WC-11, WC-12 |
|-----------------------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Multi-family

DESCRIPTION A free-standing building that may have a detached or attached accessory garage structure. The detached building type shall be oriented to the street with an articulated primary entry.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

| | |
|--|---|
| GROUND FLOOR LIMITATIONS | Residential |
| BUILDING HEIGHT | |
| Primary Building Height (maximum) | 3 stories / 35' 4 stories/50' in WC-1, WC-2 and WC-12 |
| Ground Floor Height (feet) | Not applicable |
| Upper Floor Height (feet) | Not applicable |
| Finished Floor Elevation (above grade at front façade) | Not applicable |
| Main Roof Pitch | 4:12 min / 12:12 max |
| Flat Roofs Permitted | Yes |

TRANSPARENCY AND ARTICULATION

| | |
|---|----------------|
| Ground Floor Transparency (% of façade) | Not applicable |
| Upper Floor Transparency (% of façade) | Not applicable |
| Street Frontage Wall Length Without Offset (feet) | 100 maximum |
| Street Frontage Wall Offset – length/depth (feet) | 4/8 minimum |

BUILDING ENTRANCES

| | |
|---------------------------------|-----|
| Street Facing Entry Required | Yes |
| Entrance Spacing (maximum feet) | 100 |

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to reduce the overall scale of multi-family dwellings

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to de-emphasize the vertical nature of multi-family buildings

LOCATION AND ARTICULATION OF ENTRY

Building shall clearly delineate primary entrance; all entries shall be covered

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Main roof shall be pitched. Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, site signage

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

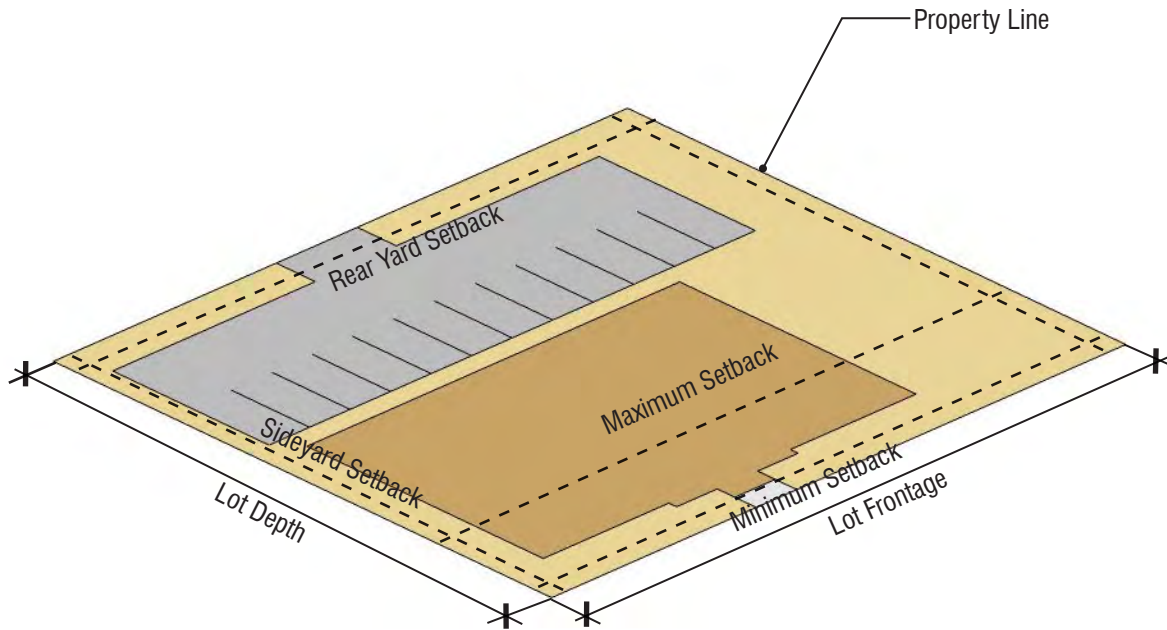
Porches, stoops, stairs, terraces, bay windows, balconies, awnings, signage, lighting, gallery, forecourts

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Multi-family



LOT STANDARDS

| | |
|------------------------|---------------|
| Lot Frontage (feet) | 40 minimum |
| Lot Depth (feet) | 80 minimum |
| Lot Area (square feet) | 3,200 minimum |

BUILDING PLACEMENT ON LOT

| | |
|--|----------------------|
| Front Yard Setback (feet) | 5 minimum/25 maximum |
| Side Yard Setback (feet) | 5 minimum |
| Rear Yard Setback (feet) | 5 minimum |
| Side-Street Yard (feet on corner lots) | 5 minimum |
| Frontage Occupancy (%) | 30% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|--|
| Front Parking Setback from primary façade (feet) | Not applicable; parking in rear of lot |
| Side and Rear Parking Setback (feet) | 5 minimum |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|------------|
| Front Lot Setback (feet) | 80 maximum |
|--------------------------|------------|

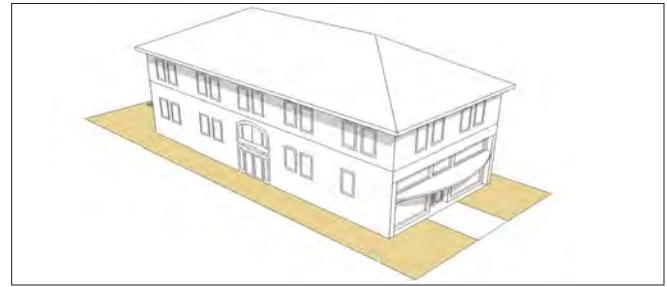
SUBAREAS

| | |
|-----------------------|---|
| Allowed in Subarea(s) | WC-1, WC-2, WC-8, WC-9, WC-10, WC-11, WC-12 |
|-----------------------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Small Building

DESCRIPTION A small scale building designed to accommodate multiple uses including residential or office upper floors with retail or service ground floor uses. Flex Use is limited to 5,000 square feet per use, per unit. Ground floor uses are intended to address the street with entries, and storefronts. Small Buildings may be located on all Block Types. A Small Building has a maximum of 10,000 square feet per floor.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS Retail, service, commercial

BUILDING HEIGHT

Primary Building Height (maximum) 1 story (16' minimum) / 3 stories (35' maximum)

Ground Floor Height (feet) 10 residential / 12 other uses minimum

Upper Floor Height (feet) 9 minimum

Finished Floor Elevation (above grade at front façade) Not applicable

Main Roof Pitch 12:12 maximum

Flat Roofs Permitted Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) 30% minimum

Upper Floor Transparency (% of façade) 20% minimum

Street Frontage Wall Length Without Offset (feet) 50 maximum

Street Frontage Wall Offset – length/depth (feet) 5/1 minimum

BUILDING ENTRANCES

Street Facing Entry Required Yes

Entrance Spacing (maximum feet) 75

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall provide an active façade, including transparent windows, that contributes to the public realm

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to reduce the overall scale of building

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Building shall clearly delineate separate entries for separate uses

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, Directional, site, building-attached, building-awning, and building-window

ALLOWABLE LIGHTING TYPES

Building lighting, storefront lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

For residential uses: porches, stoops, stairs, terraces, bay windows, balconies, awnings, signage, lighting, forecourt

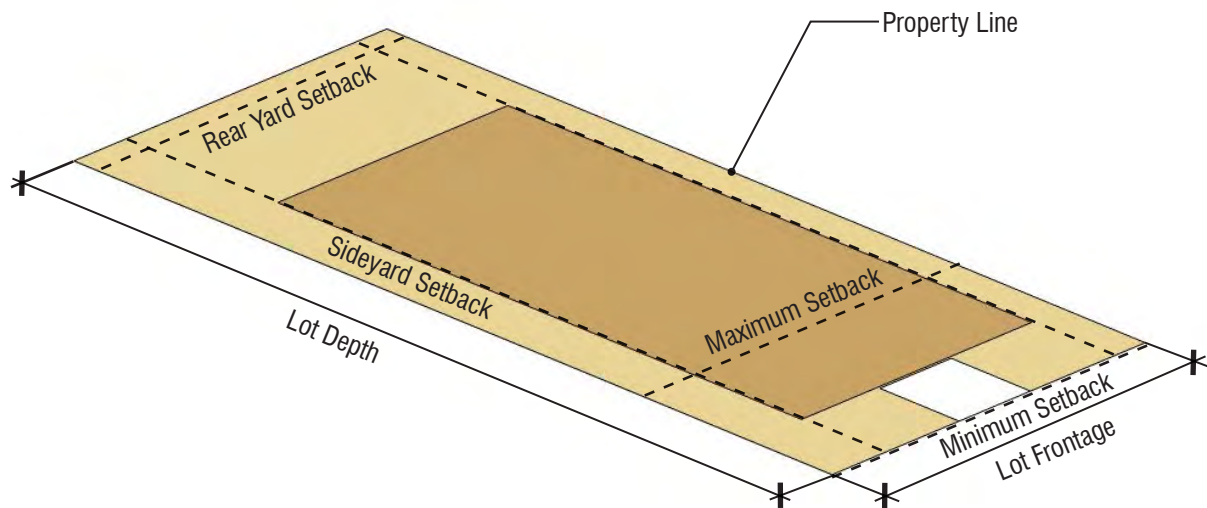
ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings signage, lighting, sidewalk dining, displays

2.4.2 Buildings and Lots

LOT TYPE:

Small Building

**LOT STANDARDS**

| | |
|------------------------|-------------------------------|
| Lot Frontage (feet) | 50 minimum / 75 maximum |
| Lot Depth (feet) | Not applicable |
| Lot Area (square feet) | 5,000 minimum / 7,500 maximum |

BUILDING PLACEMENT ON LOT

| | |
|--|------------------------|
| Front Yard Setback (feet) | 0 minimum / 25 maximum |
| Side Yard Setback (feet) | 0 minimum / 15 maximum |
| Rear Yard Setback (feet) | 5 feet minimum |
| Side-Street Yard (feet on corner lots) | 0 minimum / 15 maximum |
| Frontage Occupancy (%) | 50% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|------------------------|
| Front Parking Setback from primary façade (feet) | 8 minimum / no maximum |
| Side and Rear Parking Setback (feet) | 0 feet |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|----------------|
| Front Lot Setback (feet) | Not applicable |
|--------------------------|----------------|

SUBAREAS

| | |
|-----------------------|---|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-4, WC-6, WC-8, WC-10, WC-12 |
|-----------------------|---|

ADDITIONAL REGULATIONS

| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Medium Building



DESCRIPTION A medium scale building designed to accommodate multiple uses including residential, or office upper floors with retail, mixed use or service ground floor uses. Flex Use is limited to 5,000 square feet per use, per unit. Ground floor uses are intended to address the street with entries, and storefronts. A Medium Building has a maximum of 25,000 square feet per floor.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS Retail, service, mixed use

BUILDING HEIGHT

Primary Building Height (maximum) 1 story (16' minimum) / 4 stories (50' maximum)

Ground Floor Height (feet) 10 residential / 12 other uses minimum

Upper Floor Height (feet) 9 minimum

Finished Floor Elevation (above grade at front façade) Not applicable

Main Roof Pitch 12:12 maximum

Flat Roofs Permitted Yes, at 2 stories or more

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) 30% minimum

Upper Floor Transparency (% of façade) 20% minimum

Street Frontage Wall Length Without Offset (feet) 50 maximum

Street Frontage Wall Offset – length/depth (feet) 5/1 minimum

BUILDING ENTRANCES

Street Facing Entry Required Yes

Entrance Spacing (maximum feet) 75

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall provide an active façade, including transparent windows, that contributes to the public realm

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to reduce the overall scale of building

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Building shall clearly delineate separate entries for separate uses

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached, building-awning, and building-window

ALLOWABLE LIGHTING TYPES

Building lighting, storefront lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

For residential uses: Porches, stoops, stairs, terraces, bay windows, balconies, awnings, signage, lighting, forecourt

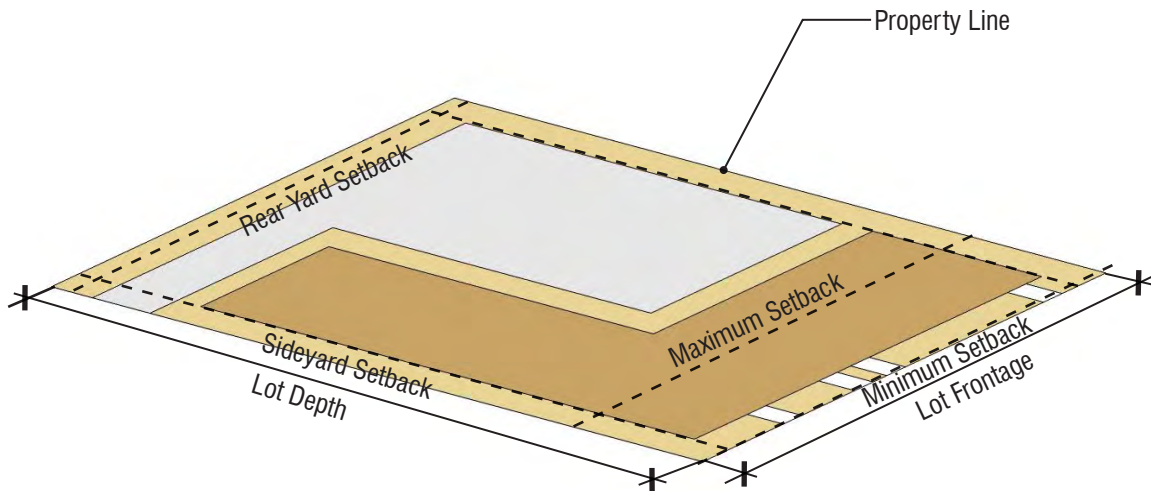
ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings signage, lighting, sidewalk dining, displays

2.4.2 Buildings and Lots

LOT TYPE:

Medium Building

**LOT STANDARDS**

| | |
|------------------------|--------------------------------|
| Lot Frontage (feet) | 75 minimum / 150 maximum |
| Lot Depth (feet) | 100 minimum / 250 maximum |
| Lot Area (square feet) | 7,500 minimum / 37,500 maximum |

BUILDING PLACEMENT ON LOT

| | |
|--|------------------------|
| Front Yard Setback (feet) | 0 minimum / 25 maximum |
| Side Yard Setback (feet) | 0 minimum / 15 maximum |
| Rear Yard Setback (feet) | 5 feet minimum |
| Side-Street Yard (feet on corner lots) | 0 minimum / 15 maximum |
| Frontage Occupancy (%) | 60% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|------------------------|
| Front Parking Setback from primary façade (feet) | 8 minimum / no maximum |
| Side and Rear Parking Setback (feet) | 0 feet |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|----------------|
| Front Lot Setback (feet) | Not applicable |
|--------------------------|----------------|

SUBAREAS

| | |
|-----------------------|-----------------------------------|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-10, WC-12 |
|-----------------------|-----------------------------------|

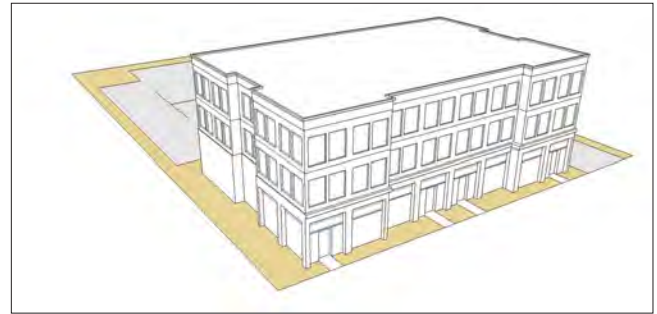
ADDITIONAL REGULATIONS

| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Large Building

DESCRIPTION A large scale building designed to accommodate multiple uses including residential, or office upper floors with retail, mixed use or service ground floor uses. Flex Use is limited to 5,000 square feet per use, per unit. Ground Floor uses are intended to address the street with entries, and storefronts. A Large Building has a minimum of 25,000 square feet per floor. In WC-1 only, a Large Building may be an office building with ancillary uses on the ground floor.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

| | |
|-----------------------------------|------------|
| GROUND FLOOR LIMITATIONS | None |
| BUILDING HEIGHT | |
| Primary Building Height (maximum) | 50 maximum |

| | |
|----------------------------|--|
| Ground Floor Height (feet) | 10 residential / 14 other uses minimum |
|----------------------------|--|

| | |
|--|-----|
| Upper Floor Height (feet) | - |
| Finished Floor Elevation (above grade at front façade) | - |
| Main Roof Pitch | - |
| Flat Roofs Permitted | Yes |

TRANSPARENCY AND ARTICULATION

| | |
|---|---|
| Ground Floor Transparency (% of façade) | 30% minimum |
| Upper Floor Transparency (% of façade) | 20% minimum |
| Street Frontage Wall Length Without Offset (feet) | 60 maximum articulation at corners; 120 mid-block |
| Street Frontage Wall Offset – length/depth (feet) | 10/2 minimum, 4-/20 maximum |

BUILDING ENTRANCES

| | |
|---------------------------------|------------|
| Street Facing Entry Required | Yes |
| Entrance Spacing (maximum feet) | 80 maximum |

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall provide an active façade, including transparent windows, that contributes to the public realm

SCALE AND PROPORTION

Architectural features and treatments shall be used to reduce the overall scale and mass of the building. Façades visible from a street should be articulated to reduce the mass of the building. Ground floor façades that face a public street shall have galleries, arcades, display windows, entry areas, and/or awnings, or other such features along no less than 60% of their horizontal length

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached, building-awning, and building-window

ALLOWABLE LIGHTING TYPES

Building lighting, storefront lighting, site lighting, parking lighting and pedestrian lighting

2.4.2 Buildings and Lots

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Building shall clearly delineate separate entries for separate uses

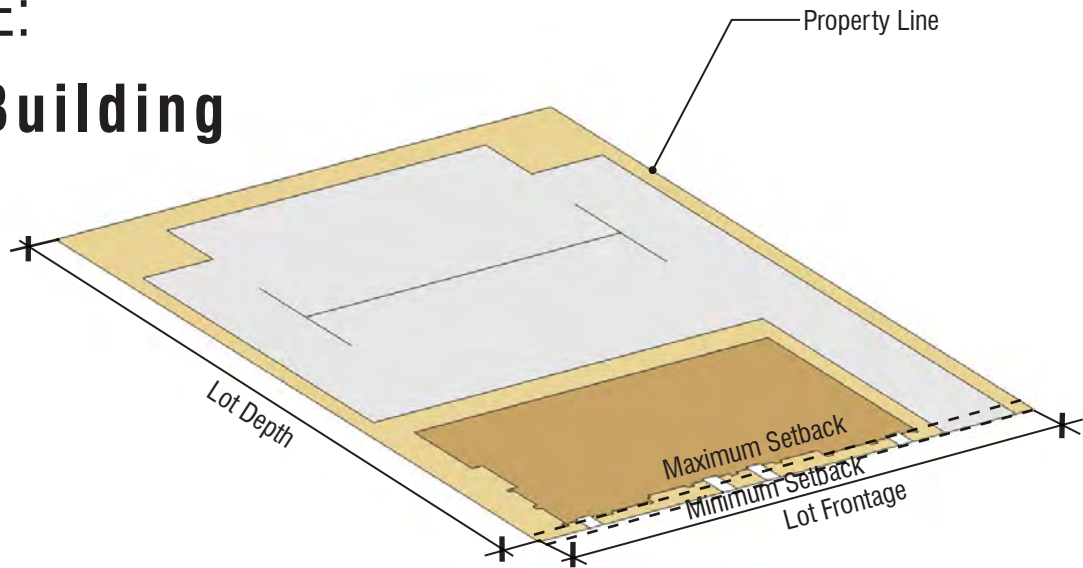
ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

For residential uses: Porches, stoops, stairs, terraces, bay windows, balconies, awnings, signage, lighting, forecourt

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings signage, lighting, sidewalk dining, displays

**LOT TYPE:
Large Building**



LOT STANDARDS

| | |
|------------------------|--------------------------|
| Lot Frontage (feet) | 150 minimum / no maximum |
| Lot Depth (feet) | Not applicable |
| Lot Area (square feet) | Not applicable |

BUILDING PLACEMENT ON LOT

| | |
|--|--|
| Front Yard Setback for primary building (feet) | 0 minimum / 2 maximum; 20% of façade may be set back 12 feet |
| Side Yard Setback (feet) | 0 |
| Rear Yard Setback (feet) | 0 |
| Side-Street Yard (feet on corner lots) | 0 |
| Frontage Occupancy (%) | 50% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|--|
| Front Parking Setback from primary façade (feet) | 6 minimum / no maximum |
| Side and Rear Parking Setback (feet) | 0; parking lots may be shared with adjoining buildings |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|------------|
| Front Lot Setback (feet) | 80 maximum |
|--------------------------|------------|

SUBAREAS

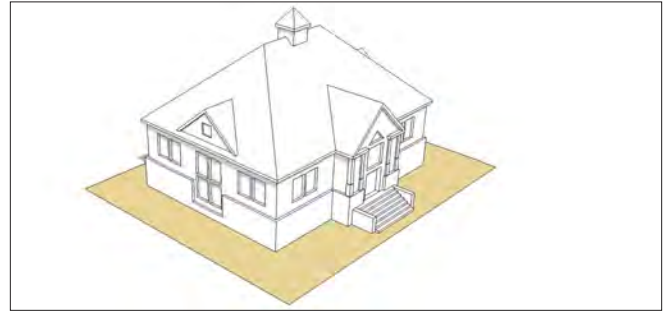
| | |
|-----------------------|----------------------------|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-12 |
|-----------------------|----------------------------|

ADDITIONAL REGULATIONS

| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Civic Building



DESCRIPTION A free-standing building that accommodates civic uses in Neighborhood or Village Center blocks. The building should be sited so as to give a sense of prominence in relation to the surrounding areas.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS Civic

BUILDING HEIGHT

Primary Building Height (maximum) 50 maximum in Subareas WC-1-GL, WC-1, WC-2 and WC-12; 35 maximum in all other Subareas where allowable

Ground Floor Height (feet) 12 minimum

Upper Floor Height (feet) 9 minimum

Finished Floor Elevation (above grade at front façade) Not applicable

Main Roof Pitch Not applicable

Flat Roofs Permitted Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) Not applicable

Upper Floor Transparency (% of façade) Not applicable

Street Frontage Wall Length Without Offset (feet) Not applicable

Street Frontage Wall Offset – length/depth (feet) Not applicable

BUILDING ENTRANCES

Street Facing Entry Required Yes

Entrance Spacing (maximum feet) Not applicable

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street

TREATMENT OF GROUND FLOOR

None

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to emphasize the importance of civic and cultural uses

VERTICAL AND HORIZONTAL EMPHASIS

None

LOCATION AND ARTICULATION OF ENTRY

Main entry shall be clearly articulated and be part of a primary building feature

PLACEMENT OF WINDOWS AND DOORS

None

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

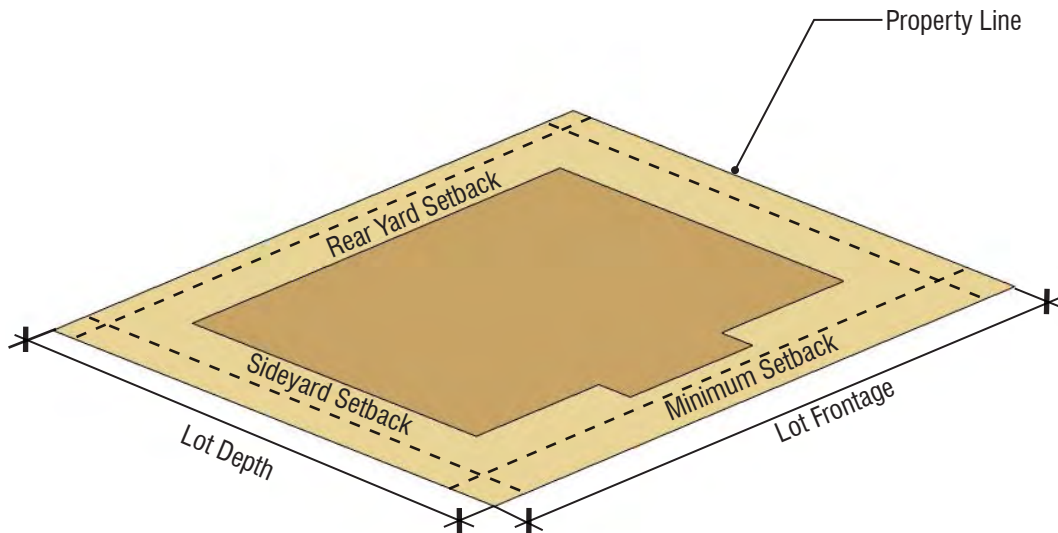
Porches, stoops, stairs, awnings, signage, lighting, forecourt, balcony, terrace, gallery

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Civic Building



LOT STANDARDS

| | |
|------------------------|----------------|
| Lot Frontage (feet) | Not applicable |
| Lot Depth (feet) | Not applicable |
| Lot Area (square feet) | Not applicable |

BUILDING PLACEMENT ON LOT

| | |
|--|-------------------------|
| Front Yard Setback (feet) | 10 minimum / no maximum |
| Side Yard Setback (feet) | 10 minimum / no maximum |
| Rear Yard Setback (feet) | 5 minimum / no maximum |
| Side-Street Yard (feet on corner lots) | - |
| Frontage Occupancy (%) | - |

PARKING PLACEMENT ON LOT

| | |
|--|--|
| Front Parking Setback from primary façade (feet) | 6 minimum / no maximum |
| Side and Rear Parking Setback (feet) | 0; parking lots may be shared with adjoining buildings |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|------------|
| Front Lot Setback (feet) | 80 maximum |
|--------------------------|------------|

SUBAREAS

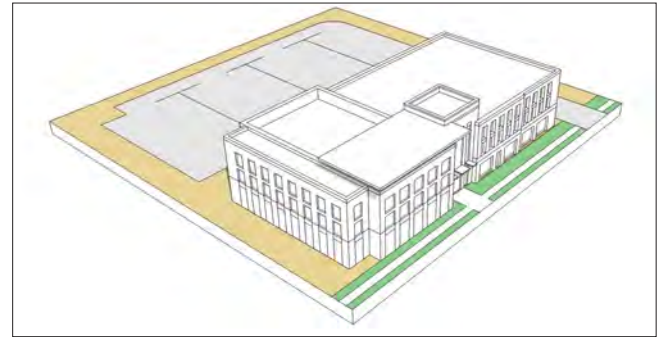
| | |
|-----------------------|--|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-3, WC-6, WC-8, WC-9, WC-10, WC-11, WC-12 |
|-----------------------|--|

ADDITIONAL REGULATIONS

| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Dedicated Office



DESCRIPTION A large floor plate commercial building designed for office use. Ancillary uses such as a bank, cafeteria, restaurant, dry cleaner, or retail shop may be added to the ground floor. Building design mitigates overall building scale and minimizes large blank walls and façades from the street. Site design integrates parking into well landscaped and screened areas.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

| | |
|--|----------------|
| GROUND FLOOR LIMITATIONS | Commercial |
| BUILDING HEIGHT | |
| Primary Building Height (maximum) | 50 maximum |
| Ground Floor Height (feet) | Not applicable |
| Upper Floor Height (feet) | Not applicable |
| Finished Floor Elevation (above grade at front façade) | Not applicable |
| Main Roof Pitch | 8:12 maximum |
| Flat Roofs Permitted | Yes |

TRANSPARENCY AND ARTICULATION

| | |
|---|-------------|
| Ground Floor Transparency (% of façade) | 30% minimum |
| Upper Floor Transparency (% of façade) | 20% minimum |
| Street Frontage Wall Length Without Offset (feet) | 75 |
| Street Frontage Wall Offset – length/depth (feet) | 10/5 |

BUILDING ENTRANCES

| | |
|---------------------------------|-----|
| Street Facing Entry Required | Yes |
| Entrance Spacing (maximum feet) | 100 |

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street and configured to provide active entries and lobbies at the street frontage

RELATIONSHIP TO STREET

Primary building façade and building entry shall be oriented to address the street

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to reduce the overall scale of building

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached, and building-window

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

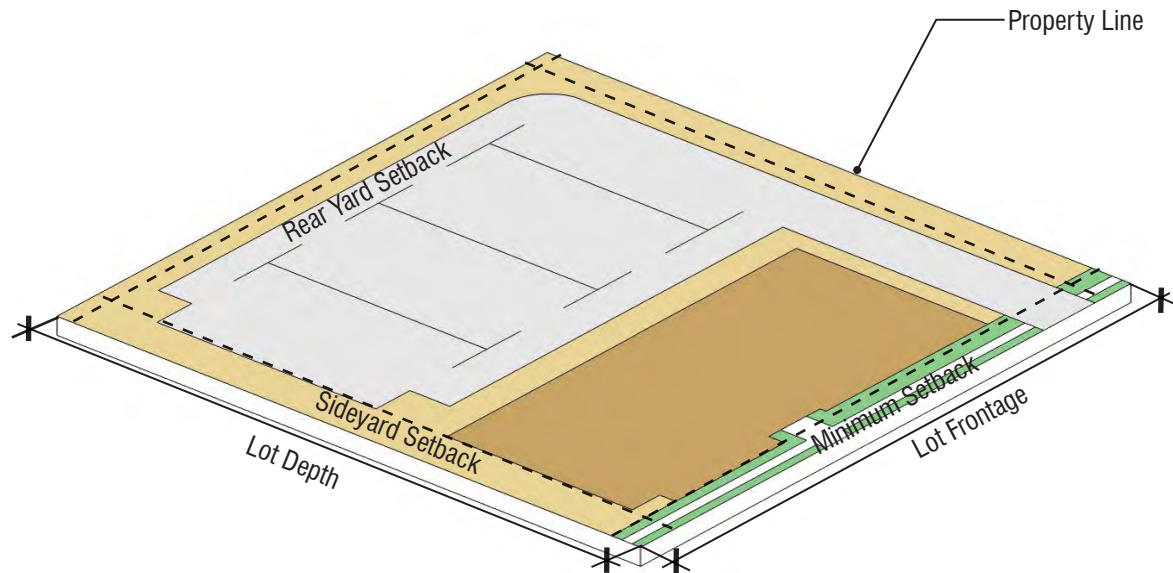
Forecourt, balcony, terrace

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings, signage, lighting

2.4.2 Buildings and Lots

LOT TYPE: Dedicated Office

**LOT STANDARDS**

| | |
|------------------------|--------------------------|
| Lot Frontage (feet) | 50 minimum / 350 maximum |
| Lot Depth (feet) | Not applicable |
| Lot Area (square feet) | Not applicable |

BUILDING PLACEMENT ON LOT

| | |
|--|-------------------------|
| Front Yard Setback (feet) | 10 minimum / no maximum |
| Side Yard Setback (feet) | 10 minimum / no maximum |
| Rear Yard Setback (feet) | 5 minimum / no maximum |
| Side-Street Yard (feet on corner lots) | 10 minimum / no maximum |
| Frontage Occupancy (%) | 40% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|--|
| Front Parking Setback from primary façade (feet) | 6 minimum / no maximum |
| Side and Rear Parking Setback (feet) | 0; parking lots may be shared with adjoining buildings |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|------------|
| Front Lot Setback (feet) | 80 maximum |
|--------------------------|------------|

SUBAREAS

| | |
|-----------------------|-------------|
| Allowed in Subarea(s) | WC-2, WC-12 |
|-----------------------|-------------|

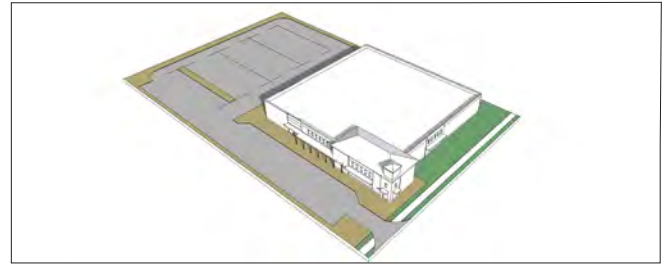
ADDITIONAL REGULATIONS

| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Large Format Retail

DESCRIPTION Large footprint retail building designed to provide interior open retail space. Building design mitigates overall building scale and minimizes large blank walls and façades from the street. Site design integrates parking into well landscaped and screened areas. This type may include other uses as a mixed-use building.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

| | |
|--|----------------|
| GROUND FLOOR LIMITATIONS | Retail |
| BUILDING HEIGHT | |
| Primary Building Height (maximum) | 50 maximum |
| Ground Floor Height (feet) | 16 minimum |
| Upper Floor Height (feet) | Not applicable |
| Finished Floor Elevation (above grade at front façade) | Not applicable |
| Main Roof Pitch | 8:12 maximum |
| Flat Roofs Permitted | Yes |

TRANSPARENCY AND ARTICULATION

| | |
|---|----------------|
| Ground Floor Transparency (% of façade) | 30% minimum |
| Upper Floor Transparency (% of façade) | Not applicable |
| Street Frontage Wall Length Without Offset (feet) | 75 |
| Street Frontage Wall Offset – length/depth (feet) | 10/5 |

BUILDING ENTRANCES

| | |
|---------------------------------|-----|
| Street Facing Entry Required | Yes |
| Entrance Spacing (maximum feet) | 100 |

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street and configured to provide active entries and lobbies at the street frontage

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Architectural features and treatments shall be used to reduce the overall scale and mass of the building. Façades visible from a street should be articulated to reduce the mass of the building. Ground floor façades that face a public street shall have galleries, arcades, display windows, entry areas, and/or awnings, or other such features along no less than 60% of their horizontal length

VERTICAL AND HORIZONTAL EMPHASIS

Vertical building elements shall be used to reduce horizontal building masses

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be used to provide a regular rhythm on façades and to orient the structure to a pedestrian scale. No uninterrupted façade of more than 50 feet shall occur

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached

ALLOWABLE LIGHTING TYPES

Site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

Porch, gallery, terrace, balcony, forecourt

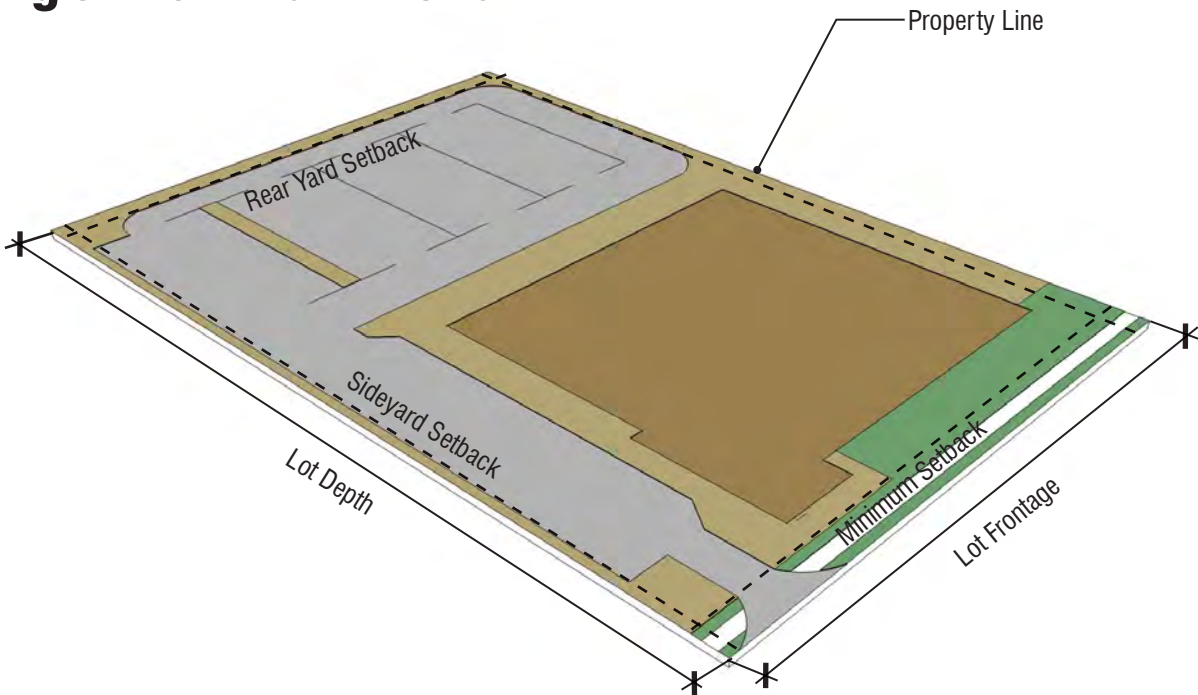
ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings signage, lighting

2.4.2 Buildings and Lots

LOT TYPE:

Large Format Retail



LOT STANDARDS

| | |
|------------------------|----------------|
| Lot Frontage (feet) | 150 minimum |
| Lot Depth (feet) | 150 minimum |
| Lot Area (square feet) | 20,000 minimum |

BUILDING PLACEMENT ON LOT

| | |
|--|-------------------------|
| Front Yard Setback (feet) | 10 minimum / no maximum |
| Side Yard Setback (feet) | 10 minimum / no maximum |
| Rear Yard Setback (feet) | 5 minimum |
| Side-Street Yard (feet on corner lots) | 20 minimum |
| Frontage Occupancy (%) | 30% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|--|
| Front Parking Setback from primary façade (feet) | 10' from face of building |
| Side and Rear Parking Setback (feet) | 0; parking lots may be shared with adjoining buildings |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|------------|
| Front Lot Setback (feet) | 80 maximum |
|--------------------------|------------|

SUBAREAS

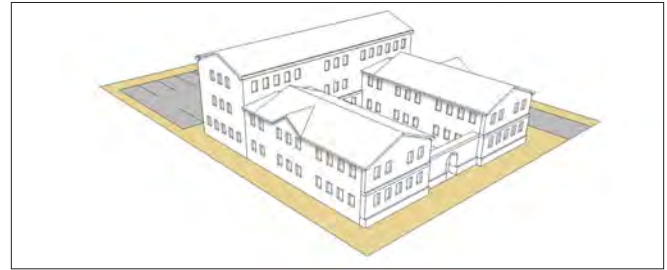
| | |
|-----------------------|----------------------------|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-12 |
|-----------------------|----------------------------|

ADDITIONAL REGULATIONS

| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Institutional Building



DESCRIPTION Institutional building for assisted living, nursing home, elderly housing and hospital. Building design should be compatible with the intent of the Subarea and should minimize large blank walls and façades from the street. Site design integrates parking into well landscaped and screened areas. In WC-12 only, Institutional Lots may be combined to accommodate a hospital use as allowed by the Allowable Densities Table.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS None

BUILDING HEIGHT
 Primary Building Height (maximum) 35 maximum in WC-4, WC-6 and WC-8 / 50 maximum in WC-1, WC-2, WC-12

Ground Floor Height (feet) -
 Upper Floor Height (feet) -
 Finished Floor Elevation (above grade at front façade) -
 Main Roof Pitch -
 Flat Roofs Permitted Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) None
 Upper Floor Transparency (% of façade) None
 Street Frontage Wall Length Without Offset (feet) 60
 Street Frontage Wall Offset – length/depth (feet) 10/5

BUILDING ENTRANCES

Street Facing Entry Required Yes
 Entrance Spacing (maximum feet) 80

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street and configured to provide active entries and lobbies at the street frontage

RELATIONSHIP TO STREET

Primary building façade and building entry shall be oriented to address the street

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to reduce the overall scale of building

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached, and building-window

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

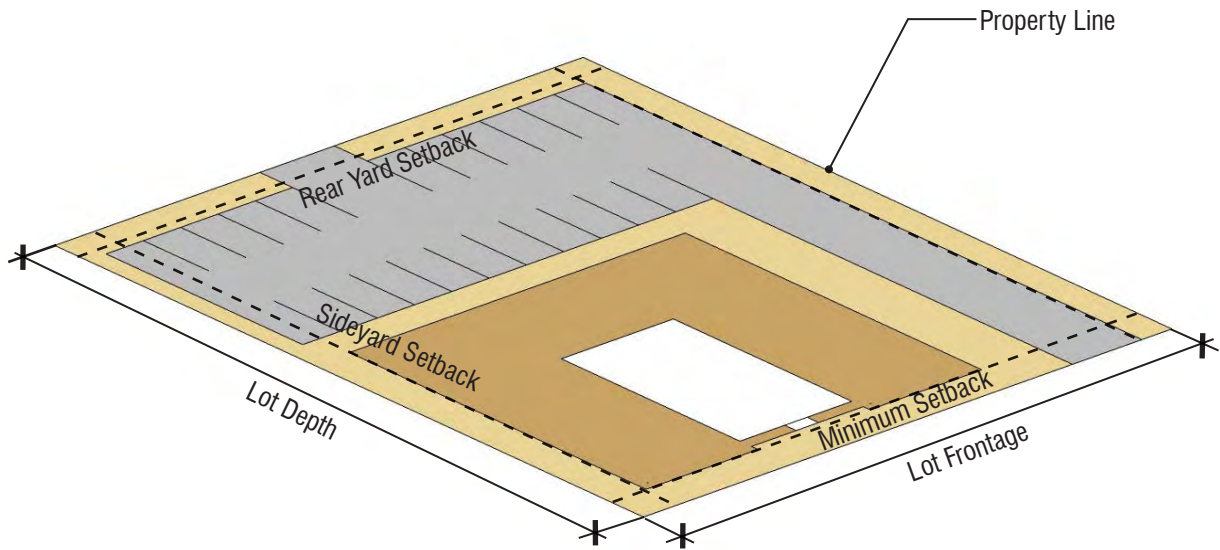
Porch, gallery, terrace, balcony, forecourt, stoop

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings, signage, lighting

2.4.2 Buildings and Lots

LOT TYPE: Institutional Building



LOT STANDARDS

| | |
|------------------------|---------------|
| Lot Frontage (feet) | 100 maximum |
| Lot Depth (feet) | 300 maximum |
| Lot Area (square feet) | 5,000 minimum |

BUILDING PLACEMENT ON LOT

| | |
|--|------------|
| Front Yard Setback (feet) | 10 minimum |
| Side Yard Setback (feet) | 10 minimum |
| Rear Yard Setback (feet) | 5 minimum |
| Side-Street Yard (feet on corner lots) | - |
| Frontage Occupancy (%) | - |

PARKING PLACEMENT ON LOT

| | |
|--|--|
| Front Parking Setback from primary façade (feet) | 20 minimum |
| Side and Rear Parking Setback (feet) | 0; parking lots may be shared with adjoining buildings |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|------------|
| Front Lot Setback (feet) | 80 maximum |
|--------------------------|------------|

SUBAREAS

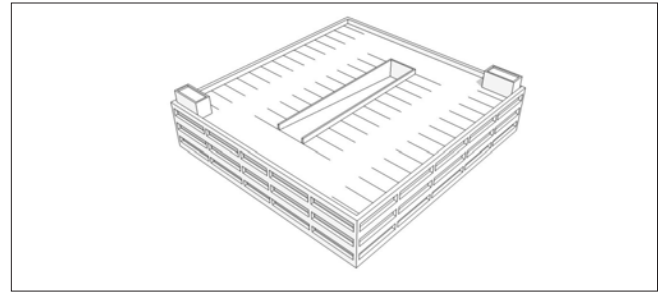
| | |
|-----------------------|--------------------------------------|
| Allowed in Subarea(s) | WC-1, WC-2, WC-6, WC-8, WC-10, WC-12 |
|-----------------------|--------------------------------------|

ADDITIONAL REGULATIONS

| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.2 Buildings and Lots

BUILDING TYPE: Parking Structure



DESCRIPTION Parking structures provide off-site parking within the block and can be combined with village center blocks and Small, Medium and Large Building Types connected to the Parking Structure to create integrated development. Buildings should have frontage on the primary street, screening the Parking Structure. Screening on secondary streets can also be accomplished with landscaping.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

| | |
|--|----------------|
| GROUND FLOOR LIMITATIONS | None |
| BUILDING HEIGHT | |
| Primary Building Height (maximum) | 50 maximum |
| Ground Floor Height (feet) | Not applicable |
| Upper Floor Height (feet) | - |
| Finished Floor Elevation (above grade at front façade) | - |
| Main Roof Pitch | - |
| Flat Roofs Permitted | Yes |

TRANSPARENCY AND ARTICULATION

| | |
|---|----------------|
| Ground Floor Transparency (% of façade) | None |
| Upper Floor Transparency (% of façade) | None |
| Street Frontage Wall Length Without Offset (feet) | Not applicable |
| Street Frontage Wall Offset – length/depth (feet) | Not applicable |

BUILDING ENTRANCES

| | |
|---------------------------------|----------------|
| Street Facing Entry Required | No |
| Entrance Spacing (maximum feet) | Not applicable |

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Parking structure shall be placed at the interior of blocks or shall be articulated to integrate with surrounding façades

RELATIONSHIP TO STREET

Parking structure shall provide convenient vehicle and pedestrian access to the street

TREATMENT OF GROUND FLOOR

The ground floor shall be articulated to integrate with surrounding façades

SCALE AND PROPORTION

Architectural elements shall be used to reduce the overall scale and massing of the parking structure

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Main pedestrian and vehicle entries shall be separated, oriented to the street, and clearly marked

PLACEMENT OF WINDOWS AND DOORS

None

ROOF FORM

Not applicable

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached

ALLOWABLE LIGHTING TYPES

Site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

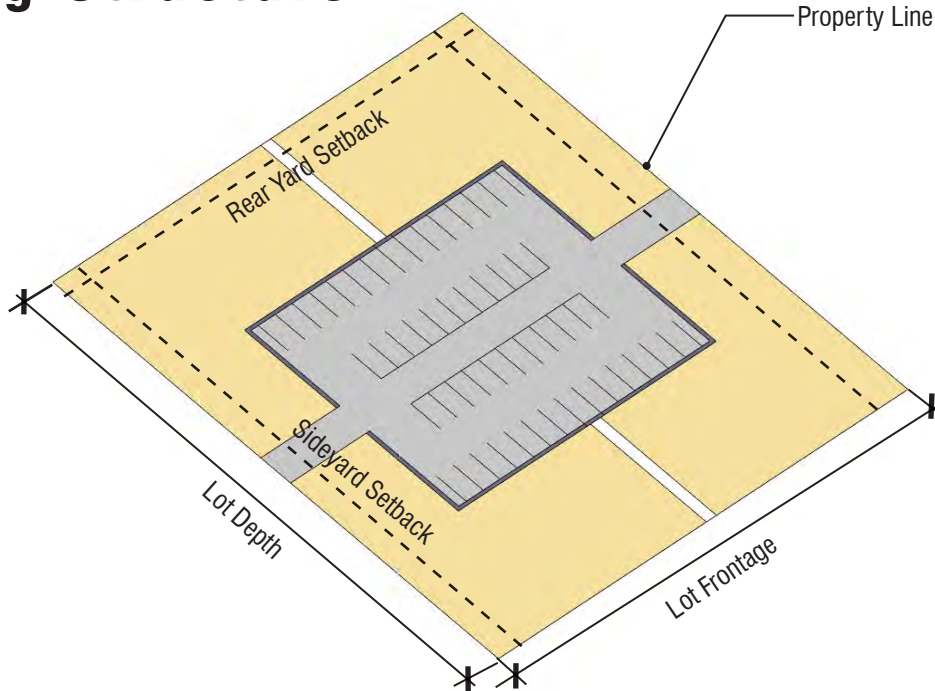
None

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Parking Structure



LOT STANDARDS

| | |
|------------------------|----------------|
| Lot Frontage (feet) | Not applicable |
| Lot Depth (feet) | Not applicable |
| Lot Area (square feet) | Not applicable |

BUILDING PLACEMENT ON LOT

| | |
|--|---------------------------------|
| Front Yard Setback (feet) | Not applicable, internal to lot |
| Side Yard Setback (feet) | 10 minimum |
| Rear Yard Setback (feet) | 5 minimum |
| Side-Street Yard (feet on corner lots) | - |
| Frontage Occupancy (%) | - |

PARKING PLACEMENT ON LOT

| | |
|--|----------------|
| Front Parking Setback from primary façade (feet) | Not applicable |
| Side and Rear Parking Setback (feet) | Not applicable |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|----------------|
| Front Lot Setback (feet) | Not applicable |
|--------------------------|----------------|

SUBAREAS

| | |
|-----------------------|----------------------------|
| Allowed in Subarea(s) | WC-1-GL, WC-1, WC-2, WC-12 |
|-----------------------|----------------------------|

ADDITIONAL REGULATIONS

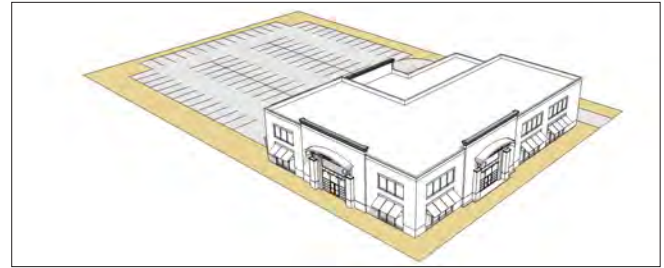
| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.2 Buildings and Lots

BUILDING TYPE:

Flex

DESCRIPTION A large footprint commercial use, mixed-use or Flex Use building and lot that integrate flexible and potentially large scale building space into a landscaped lot with parking. Lots may be combined to form larger lots that may share building or parking uses.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS Commercial, Light Industrial

BUILDING HEIGHT

Primary Building Height (maximum) 50 maximum

Ground Floor Height (feet) Not applicable

Upper Floor Height (feet) Not applicable

Finished Floor Elevation (above grade at front façade) Not applicable

Main Roof Pitch 8:12 maximum

Flat Roofs Permitted Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) 30% minimum

Upper Floor Transparency (% of façade) Not applicable

Street Frontage Wall Length Without Offset (feet) 100

Street Frontage Wall Offset – length/depth (feet) 20/5 minimum

BUILDING ENTRANCES

Street Facing Entry Required Yes

Entrance Spacing (maximum feet) 150

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street and configured to provide active entries and lobbies at the street frontage

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Architectural features and treatments shall be used to reduce the overall scale and mass of the building. Façades visible from a street should be articulated to reduce the mass of the building. Ground floor façades that face a public street shall have galleries, arcades, display windows, entry areas, and/or awnings, or other such features along no less than 60% of their horizontal length

VERTICAL AND HORIZONTAL EMPHASIS

Vertical building elements shall be used to reduce horizontal building masses

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be used to provide a regular rhythm on façades and to orient the structure to a pedestrian scale. No uninterrupted façade of more than 50' shall occur

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached

ALLOWABLE LIGHTING TYPES

Site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

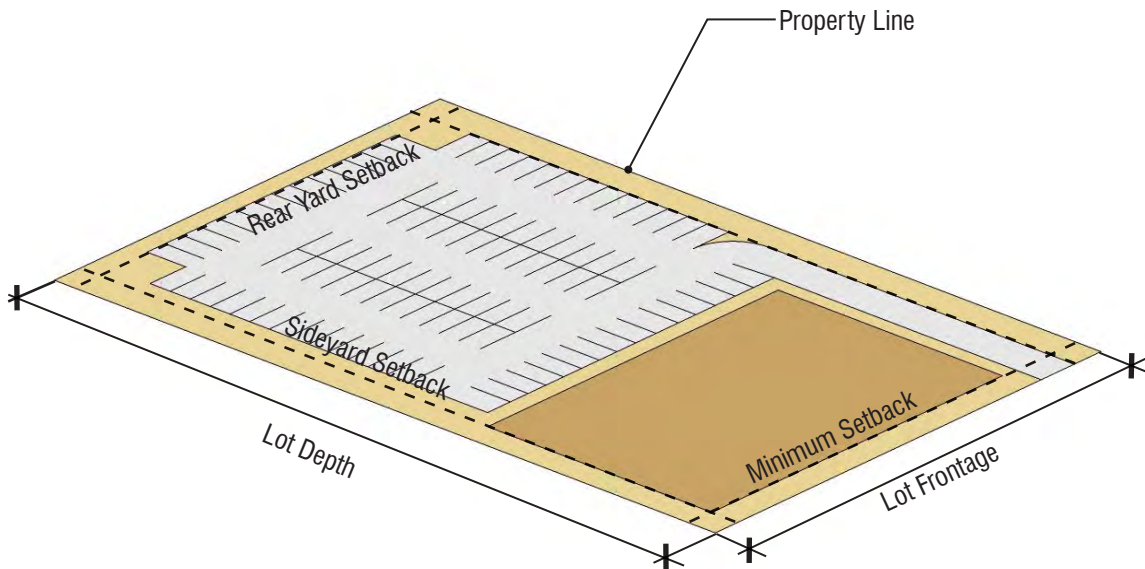
Gallery, awnings signage, lighting, terrace, balcony, forecourt

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Flex



LOT STANDARDS

| | |
|------------------------|----------------|
| Lot Frontage (feet) | 150 minimum |
| Lot Depth (feet) | 150 minimum |
| Lot Area (square feet) | 20,000 minimum |

BUILDING PLACEMENT ON LOT

| | |
|--|-------------------------|
| Front Yard Setback (feet) | 10 minimum / no maximum |
| Side Yard Setback (feet) | 10 minimum / no maximum |
| Rear Yard Setback (feet) | 5 minimum |
| Side-Street Yard (feet on corner lots) | 20 minimum |
| Frontage Occupancy (%) | 30% minimum |

PARKING PLACEMENT ON LOT

| | |
|--|--|
| Front Parking Setback from primary façade (feet) | 10' from face of building |
| Side and Rear Parking Setback (feet) | 0; parking lots may be shared with adjoining buildings |

PARKING PLACEMENT ON SIDE STREETS

| | |
|--------------------------|------------|
| Front Lot Setback (feet) | 80 maximum |
|--------------------------|------------|

SUBAREAS

| | |
|-----------------------|---------------------------|
| Allowed in Subarea(s) | WC-1-GL, WC-1 WC-2, WC-12 |
|-----------------------|---------------------------|

ADDITIONAL REGULATIONS

| | |
|-------|---|
| Other | Streets, Landscaping, Lighting, Signage, Stormwater |
|-------|---|

2.4.3 Encroachment and Shared Use Agreements

Encroachments

The building façade must be located at or behind the setback within build-to zone as required by the Lot Types. Some building elements are permitted to encroach into the setback and into the front yard and side yards facing streets. Other building element encroachments are permitted within the public or private right-of-way.

- A. Encroachments into front setbacks and side setbacks facing streets – The following building elements are permitted to encroach into the setback but not beyond the lot line into the right-of-way where permitted by a specific Building Type:

| | |
|----------------------------------|---|
| 1. Porches | No restrictions |
| 2. Stoops | No restrictions |
| 3. Stairs or ramps and handrails | No restrictions |
| 4. Awnings and canopies | No restrictions with a vertical clearance of seven (7) feet minimum |
| 5. Porte Cocheres | No restrictions |
| 6. Terraces | Eight (8) feet maximum |
| 7. Bay windows | Five (5) feet maximum |
| 8. Balconies | Five (5) feet maximum |
| 9. Signage | Six (6) feet maximum |
| 10. Lighting | Three (3) feet maximum, unless lighting is for an allowed sign under 9. Signage directly above. |
| 11. Driveways | For restrictions, see <i>Section 2.3.3 Transportation Network</i> |
| 12. Drop-offs | Allowable for the following uses: Dedicated Office Buildings, Hotels, Civic and Institutional |

- B. Encroachments into right-of-way – The following building elements are permitted to encroach into the right-of-way where permitted by a specific Building Type:

| | |
|-------------------------|--|
| 1. Awnings and canopies | Eight (8) feet maximum with a vertical clearance of seven (7) feet minimum |
| 2. Galleries | Eight (8) feet maximum with a vertical clearance of seven (7) feet minimum, two (2) feet minimum behind the curb, private streets only |
| 3. Signage | Six (6) feet maximum |

2.4.3 Encroachments and Shared Use Agreements

- | | |
|--------------|---|
| 4. Lighting | Three (3) feet maximum, unless lighting is for an allowed sign under 3. Signage directly above. |
| 5. Driveways | For restrictions, see <i>Section 2.3.3 Transportation Network</i> |

C. Other encroachments – Other encroachments into front and side yards, where permitted by the building code, to include eaves, downspouts, gutters and other similar building elements.

Encroachment and Shared Use Agreements

Shared Use and Encroachment Agreements are allowed in order to minimize land disturbance and the impact to street sections, to encourage joint trenching agreements, and to maximize the beneficial use of easements for Shared Use Paths or Recreation Trails. Such agreements are allowed with approval from the Planning Board during the PUD Subdivision and/or PUD Site Plan submittal process.

Any encroachment into a Town of Londonderry right-of-way or easement requires an encroachment and maintenance agreement with the Town before such encroachment is made.

2.4.4 PUD Site Plan Signage

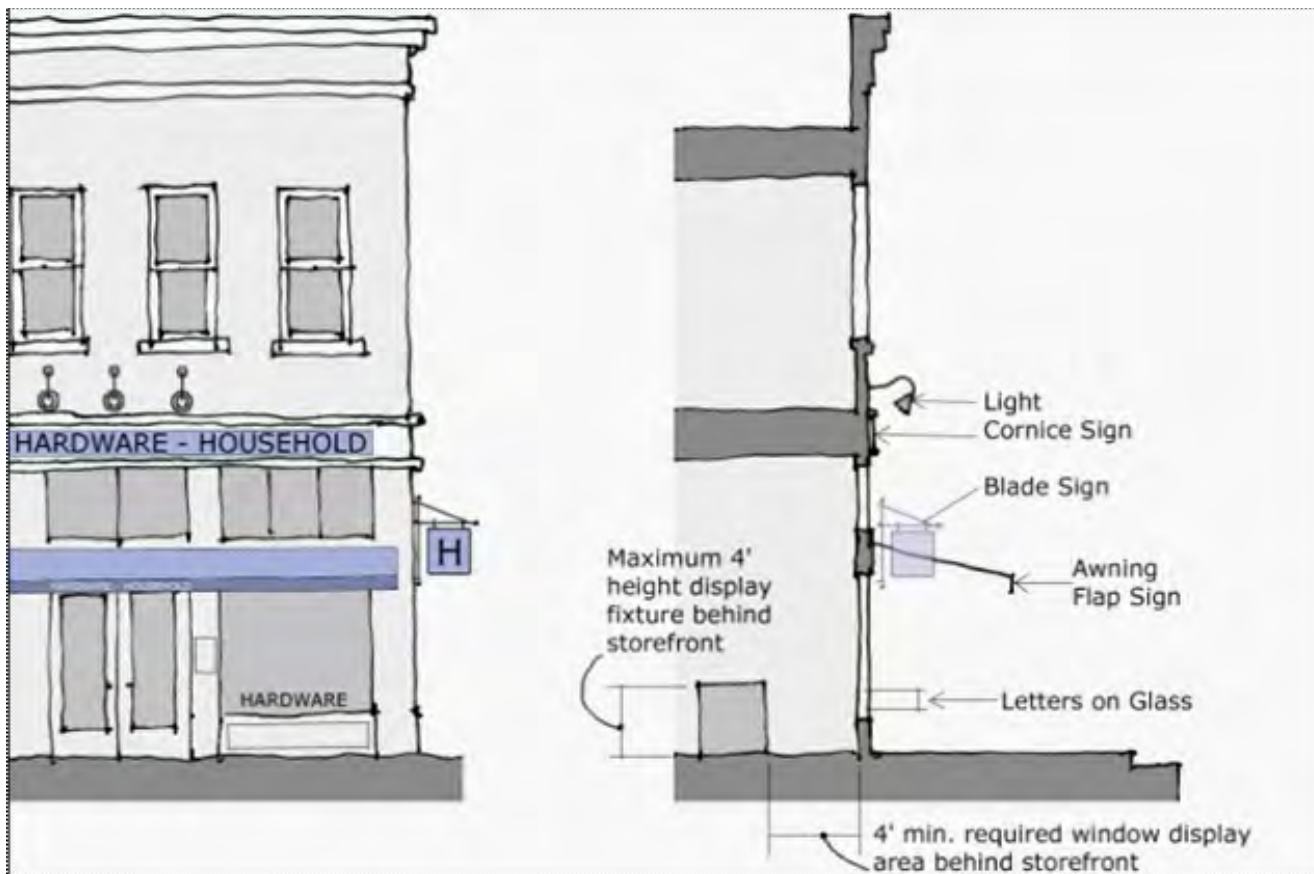
The following parameters pertain to the installation, use, maintenance, and lighting standards of signs for PUD Site Plan submittals or signage submittals within and relating to business and mixed-use areas within the Subareas. Signage for residential uses will be subject to *Section 3.11* of the *Londonderry Zoning Ordinance*, unless such Section conflicts with the PUD Master Plan, in which case the PUD Master Plan provisions shall prevail.

Permitted PUD Site Plan Signs

- A. Without limitation of the later provisions of this Section, the following signs are allowable at the PUD Site Plan level:
1. Construction signs;
 2. Parking signs;
 3. Portable valet signs;
 4. Rear entry sign except along alleys;
 5. Flags;
 6. Temporary portable signs in the public right-of-way;
 7. Temporary sign advertising nonresidential property;
 8. Temporary special event signs;
 9. Wall signs;
 10. Blade signs;
 11. Canopy and awning signs;
 12. Pole mounted & directory signs;
 13. A-frame signs;
 14. Window signs;
 15. Address signs;
 16. Building signs;
 17. Monument signs;
 18. Churches, schools, community centers, and health care facilities, buildings housing government functions, and municipal, county and state utilities signs;
 19. Multi-family residential signs;
 20. Temporary signs for churches and private schools;
 21. Traffic/direction signs on grounds of public, charitable or religious institutions;
 22. Signs not requiring a permit in accordance with *Section 3.11.5* of the *Londonderry Zoning Ordinance*.
- B. Approval Authority – The Zoning Administrator shall review all signage requiring a permit for conformity to this PUD Master Plan.

2.4.4 PUD Site Plan Signage

- C. Appeals – The petitioner shall be entitled to appeal staff denials to the Planning Board.
- D. Measurement and calculation of area – The calculation shall be in accordance with *Section 3.11.5* of the *Londonderry Zoning Ordinance* except that sign height may exceed ten (10) feet as indicated below.
- E. Submittal Requirements – All signage applications shall include the following:
1. Written approval from the Developer for proposed signage;
 2. If the applicant is not the owner of the property on which the sign(s) is to be erected, written permission from the owner or authorized representative stating that the sign(s) may be erected on the parcel;
 3. The street name and street number of the building to which the sign(s) is to be erected;
 4. A site or plat plan of the property showing accurate placement of the permanent sign(s);
 5. Front, side and top elevations with sign(s) drawn to scale;
 6. Color and material descriptions;
 7. Illumination method for the sign(s) (if any);
 8. Landscaping for the sign (if any).



Example of building signage locations

2.4.4 PUD Site Plan Signage

General Sign Guidelines

All exterior business signs located within the PUD shall conform to the following guidelines:

- A. Signs are used to
 1. Establish identities of tenants;
 2. Contribute to a lively visual atmosphere;
 3. Give clear functional information.
- B. Graphic signs should be simple and clear and reinforce the character of the subject business and overall character of the PUD;
- C. Materials, colors, placement and shape must be clearly differentiated from public safety, traffic and regulatory signage;
- D. Repetitive use of words or symbols as a sign element is not permitted except if part of the business name or except for a single band of letters inside a glass storefront.;
- E. No sign, except halo where allowed, may be internally lit, except in Subareas WC-1, WC-2, WC-1-GL, WC-8, WC-10 and WC-12;
- F. No sign or related outdoor lighting fixture shall be so placed as to focus light directly into any oncoming traffic or any street;
- G. All illuminated detached signs shall be so illuminated by an underground electrical service;
- H. Animated, moving, flashing and noise making signs are prohibited;
- I. Changeable electronic message board signs are prohibited;
- J. Signs painted directly on the building exterior walls shall not be permitted;
- K. Iridescent materials or signs, flashing lights, and Day-Glo colors are not permitted;
- L. Service doors to tenant spaces will have only standard identification, i.e., tenant's name address and number, hours of delivery and operation;
- M. Sign maintenance is the sole responsibility of the sign owner. Signs must be kept in good repair;
- N. Signs must also be maintained in a safe condition and pose no safety hazard and must meet all electrical and structural codes;
- O. Signage may not extend beyond the roof line, except in the WC-1-GL, WC-1, WC-2 and WC-12 Sub-areas.

Design, Construction and Maintenance

- A. Signs or their supports shall not be placed in such position or manner as to obstruct or interfere, either physically or visually, with any fire alarm, police alarm, traffic signal or sign, or any devices maintained

2.4.4 PUD Site Plan Signage

by or under public authority or with vehicular or pedestrian ingress or egress to or from any public or private right-of-way, roadway, driveway, or sidewalk.

- B. All signs shall be constructed and secured so as to ensure the safety of pedestrians.
- C. The material and construction of any sign shall be in accordance with the requirements above and the Building Code.
- D. Permanent signs shall be made of durable materials.
- E. All signs shall be maintained in a safe condition and, together with their structural elements, shall be kept in good repair to the satisfaction of the Zoning Administrator.

Business Sign Design Standards

The following signs shall be permitted for all businesses within the Woodmont Commons PUD.

- A. Wall Signage – Wall Signs may be permanently installed parallel to the store front for ground floor businesses as follows:
 1. Wall signs should align with major architectural elements, such as doors and windows. Ornamental elements, such as moldings, pilasters, arches, clerestory windows, roof eaves, or cornice lines should be used as a frame.
 2. All businesses are permitted one Wall Sign on the façade, unless the building is on a corner lot, in which case one sign on each street is allowed;
 3. Wall signs should not extend above the cornice line or into or above roof areas, unless they function as an integral part of the façade or roof design or are a faithful accent to existing architectural details or forms.;
 4. Businesses shall be permitted one (1) square-foot of signage per one (1) lineal-foot of street frontage, except in the WC-1-GL, WC-2 and WC-12 Subareas where the formula is twice that;
 5. Letters may also be printed or etched on the same surface as the background, which is

Example of wall sign



2.4.4 PUD Site Plan Signage

then affixed to the wall;

6. Band signs may be illuminated with halo lighting or other indirect light sources and may be internally illuminated in Subareas listed above.
- B. Blade Signs – Blade signs may be permanently installed perpendicular to the store front for ground floor and upper level businesses as follows:
1. Blade signs shall be designed to complement the business activities or the adjacent building;
 2. One blade sign shall be permitted on each façade having frontage. No more than two blade signs shall be permitted on any individual business;
 3. Blade signs shall only display the business name, business type or logo. Third party advertising is not permitted;
 4. Ground level blade signs shall not exceed twelve (12) square feet per side;
 5. Upper level blade signs shall not exceed six (6) square feet per side;
 6. All signs shall clear eight (8) feet above the adjacent sidewalk;
 7. Blade signs shall not project vertically above the roof line or parapet wall of a building;
 8. All blade signs shall be horizontally spaced fifteen (15) feet minimum from the nearest blade sign;
 9. Blade signs shall not project more than six (6) feet from a building wall;
 10. Blade signs may be illuminated with halo lighting or externally illuminated;
 11. Mounting hardware, such as supports and brackets, shall complement the design of the sign, the building or both.

Examples of blade signs



Example of canopy sign



2.4.4 PUD Site Plan Signage

- C. Canopy and Awning Signs – Canopy design shall be in keeping with the design of the building.
1. Awnings shall be sloped with a vertical flap at the bottom. Rounded, curved or other shapes are not permitted;
 2. Canopy signs shall not exceed two-and-a-half (2 1/2) feet in height and the combination of wall and canopy signs shall not exceed the maximum area permitted in Wall Signs above. Canopy signs may extend above the roof line of the canopy but shall not extend above the roof line of the building;
 3. Awning signs are limited to two (2) surfaces and shall not exceed twenty (20) percent of the total awning area. Signs on awnings may be on the sloped portion or on the flap;
 4. Awnings may have open 'sides' or 'ends;'
 5. Awnings may be either fixed or operable;
 6. Color combinations for awning or canopy signs are limited to three colors and should be simple. Lettering color and background color should contrast for legibility. Subtle bands of color are appropriate for awnings; more complex patterns or textures should generally not be permitted;
 7. Canopy signs may be illuminated with halo lighting;
 8. Awnings shall not be internally lit but may be illuminated with indirect light sources;
 9. Awnings shall be made of canvas or other coated fiber textiles similar in durability and appearance. Permanent awnings as architectural features are also permitted;
 10. Plastic awnings are prohibited;
 11. Signs may be attached, painted, or printed upon a canopy or awning.

Examples of awning sign



Example of pole mounted sign



2.4.4 PUD Site Plan Signage

- D. Pole Mounted & Freestanding Directory Signs – Pole-mounted and/or other forms of freestanding signs shall be permitted in village centers as follows:
1. Eight (8) square feet maximum per side for pole-mounted signs and eighteen (18) square feet per side for directory signs, not including decorative stanchions;
 2. Business logos are permitted;
 3. Signs shall be limited to eight (8) feet height, not including decorative stanchions;
 4. Project signs not are permitted within the public right-of-way;
 5. Directory signs may be externally illuminated;
 6. Pole signs shall be spaced at least twenty (20) feet apart;
 7. All such signs shall be constructed and secured so as to ensure the safety of pedestrians;
 8. Signs shall be constructed of durable materials that complement the design of the adjacent buildings;
 9. Directory signs or kiosks for private arcades or buildings should be on private property, located in publicly accessible courts, accessways, or passages. Only one directory sign or kiosk is allowed per building entrance.
- E. A-Frame Signs – A-Frame Signs in accordance with this Section shall be exempt from Temporary portable signs such as menu boards for restaurants, and special promotional signs for stores shall be permitted in the public right-of-way and between the building and street curb subject to the following:
1. Maximum area of one side shall be seven (7) square feet per side for a total of fourteen (14) square feet;

Example of directory sign



Example of A-frame sign



2.4.4 PUD Site Plan Signage

2. Maximum sign height shall be forty-two (42) inches;
 3. Maximum letter height shall be eight (8) inches;
 4. One sign is permitted per business, unless the building is on a corner lot and has multiple entrances;
 5. Signs shall not be illuminated;
 6. Signs must be placed immediately in front of the business, within the width of store frontage and not, for example, at the street corner in front of other businesses. For businesses located in an arcade or plaza, a portable sign may be placed at the street entrance to the arcade or plaza;
 7. A-frame signs shall be spaced at least twenty (20) feet apart;
 8. The sign shall be at least one (1) foot from the street curb and placed so that it does not unduly retard pedestrian traffic flow;
 9. All such signs shall be constructed and secured so as to ensure the safety of pedestrians;
 10. Signs shall be maintained in good condition and removed each day prior to close of business;
 11. Signs shall be constructed of durable materials that complement the design of the adjacent building;
 12. Signs shall be stored indoors after hours of operation.
- F. Window Signs – Window signs shall be permitted in all business storefronts as follows:
1. Letters and logos shall be silk-screened or of a custom vinyl appliqué. Letters and logos shall be affixed to back of glass and up to six (6) inches maximum height;
 2. One window sign per window is permitted;

Example of window sign



Example of an address sign



2.4.4 PUD Site Plan Signage

3. One suspended sign, hung a minimum thirty-six (36) inches behind glass display window is permitted at each display window within the storefront. Flat signs, clear glass, and/or acrylic are encouraged. Maximum height for a horizontal sign is twelve (12) inches and maximum size for a logo object sign is eighteen (18) inches by eighteen (18) inches;
 4. Window signs may list information including services and/or products sold on the premises, phone numbers, and operating hours;
 5. Temporary Promotional signs are permitted in each business for up to ten (10) days per month and shall not exceed twenty-five (25) percent of the total storefront window area.
- G. Address Signs – Address signs containing the numerical street address are permitted near the principal entrance of all businesses or near the mailbox if applicable.
1. Address sign numerals shall be of uniform size and may be between four (4) inches and six (6) inches in height;
 2. Address signs shall be easily visible by using colors or materials that contrast with their background.
- H. Construction Signs – Construction signs are permitted as follows:
1. One (1) sign not exceeding forty-eight (48) square feet on each site providing construction information, including but not limited to the architect(s), owner(s) and/or contractor(s). Construction signs are permitted within the site or off-premises within 800 feet of the site of the ongoing construction. Any off-premises construction sign shall remain with the project. No more than four (4) off-premises signs shall be permitted for any single construction project (i.e. one building or housing subdivision). Construc-

Example of building sign



2.4.4 PUD Site Plan Signage

tion signs are to be maintained during actual construction and promptly removed within seven (7) days after issuance of all certificates of occupancies.

Highway Corridor Signage

In Subareas WC-2 and WC-12, permitted monument and building signage shall be as follows:

- A. **Building Signage** – Building signage shall conform to the following standards:
 1. Total sign area shall be limited to two (2) square feet per one (1) linear foot of building length, but shall not exceed 350 total square feet per building;
 2. Building signs shall be independent letters pinned to the building elevation, which may be internally illuminated;
 3. Corporate logos and colors shall be permitted;
 4. Ground mounted signs are permitted;
 5. There shall be no height limit for building signage.
- B. **Monument Signage** – The PUD shall be permitted a total of 8 freestanding monument identification features as follows:
 1. Monument signs shall be located along the east side of I-93, the west side of I-93, along the median of the boulevards, or along Garden Lane, on property within or outside of the PUD;
 2. Each sign shall be limited to a maximum of 350 square feet per side or 700 square feet total, plus architectural allowances such as walls and columns;
 3. The architectural allowances of thirty-six (36) inches on the top of the sign and forty-eight (48) inches on each side of the sign shall not count toward the maximum allowed sign area, and may exceed the maximum height

Example of monument sign



by thirty-six (36) inches. The architectural allowance shall not include any portion of text, logo, or other advertising material;

4. Maximum sign maximum height shall be thirty-five (35) feet per sign as measured from the adjacent ground to the top of the sign;
5. Signs shall be halo lit, externally or internally illuminated with cutout letters.
6. All monument signs shall be spaced 200 feet minimum distance from each other;
7. The ground area between the support base for monument signs shall be planted with appropriate landscaping materials with the intent of creating an aesthetically pleasing and visually compatible landscaped area around the sign.

2.4.5 PUD Site Plan Lighting

Description

These standards are designed to reduce the impacts of glare, light spill onto adjoining properties and unnecessary lighting and to promote safety, security and energy conservation.

Purpose

The purpose of these PUD Site Plan exterior lighting standards is to create a well-balanced, integrated lighting plan for private properties that enhances vehicular and pedestrian visibility while minimizing lighting glare and contrast. Exterior lighting is intended to light building architecture and site areas so as to accentuate design features and promote security in an attractive and understated manner that minimizes off-site impacts. Lighting should emphasize both public and private features and destinations by using a minimum amount of light to meet these objectives. It should provide needed illumination of the site and, at the same time, prevent undesired off-site glare.

Applicability

Outdoor lighting standards are to be addressed at the time of PUD Site Plan submittals. They apply to the installation of new outdoor lighting fixtures or the replacement of any existing outdoor light fixtures located within the public right-of-way for the Woodmont Commons PUD.

Standards

All outdoor lighting shall be subject to *Section 3.13 Outdoor Lighting Standards of the Londonderry Site Plan Regulations* except as amended herein:

- A. Internally lit signs are acceptable provided that they meet the requirements of *Section 2.4.4 PUD Site Plan Signage*;
- B. Significant public or civic buildings may be illuminated with up-lighting;

- C. Adjacent to residential property at the perimeter of the Woodmont Commons PUD, no bulb or lamp will be visible at the property line at ground level or above unless to illuminate a path or walkway;
- D. All non-essential lighting is required to be turned off after business hours except in mixed-use areas and for monument signs along I-93, leaving only the necessary lighting for security. In mixed-use areas, storefront display lighting, parking lot lighting and sign lighting may remain on since they contribute to the illumination of walkways and the safety and security of the residents. (“Non-essential” can apply to: display, aesthetic, parking and sign lighting).

Outdoor lighting used to illuminate flags, monuments, signs, public or civic buildings, landscapes and other features must use cut-off or directionally shielded light fixtures that are aimed and controlled so that the light is substantially confined to the object to be illuminated. Such lighting must be shielded to prevent glare for drivers or light trespass beyond the Woodmont Commons PUD Boundary.

Parking lot lighting shall be designed to cast light downward. Cut-off and shielded fixtures shall be used to reduce the impacts of glare on adjacent uses. The maximum mounting height of a lighting fixture above the ground shall be twenty (20) feet in residential areas and twenty-five (25) feet in mixed-use and nonresidential areas. In Subarea WC-1-GL and in Subarea WC-12 where the fixtures are located more than one hundred feet from the Woodmont Commons PUD Boundary maximum mounting height of a lighting fixture above the ground shall be thirty-five (35) feet. The maximum height of parking lot lights on the top level of parking decks shall be twenty (20) feet. Internal lights in open sided parking decks shall be full cut-off lighting fixtures.

Light sources should generally be metal halide or LED. Low wattage high-pressure sodium may be desirable in some residential settings where glare may be an issue.

Building lighting, other than decorative fixtures at entrances and at architectural features, and walkway lighting shall be full cut-off lighting fixtures.

2.4.6 PUD SITE PLAN Landscape

Description

PUD Site Plan Standards for landscaping are designed to create an attractive setting for the Woodmont Commons PUD.

Purpose

Landscape Standards enhance the quality of the environment, provide shade for pedestrians, reduce heat island effects, screen parking and utilities and soften building and structure edges.

Applicability

These landscape standards apply to landscaping for PUD Site Plans within private lots in the Woodmont Commons PUD.

Standards

PUD Site Plan landscaping shall be in accordance with the following:

- A. The Woodmont Commons PUD is designed as an integrated mixed-use walkable community. Screening shall not be required between uses, to soften the visual impact of building, or in any front setback within the boundary of the Woodmont Commons PUD;
- B. Preservation of existing vegetation may include but not be limited to areas where access or utilities are not required, Conserved Green Space and along major storm drainage features where access or utilities are not required. Vegetation may be removed to permit public access trails and structures;
- C. Screening along public rights-of-way within the Woodmont Commons PUD shall only be required to be three (3) feet in height at parking areas to permit increased visibility and security within the parking areas in front of or beside

buildings. Screening may be accomplished with Landscaping, walls or a combination thereof;

- D. No landscaped screening is required for parking lots within the interior of blocks and parking decks located behind buildings;
- E. Other parking lots may be landscaped in compliance with the following guidelines:
 1. Each parking lot shall provide landscaping within and/or around the parking lot at a minimum ratio of five percent (5%) of the gross area of the parking lot. Landscaping shall be evenly dispersed throughout each parking lot. Orchard-style planting (placement of trees in uniformly-spaced rows) is encouraged for larger parking areas;
 2. Internal parking lot landscaping as required above, shall contain one (1) deciduous shade tree for every fifteen (15) parking spaces. Trees shall be distributed throughout the parking lot as evenly as possible. Trees shall be set back at least five (5) feet minimum from the face of the curb. Tree placement and parking lot lighting shall not conflict;
 3. Perimeter shade trees shall not be required where buildings or street trees are present;
 4. Where perimeter shade trees are required, they shall be provided around the perimeter of parking areas at a minimum ratio of one (1) tree per fifty (50) feet of parking lot perimeter, unless existing trees disrupt this ratio;
 5. Parking lot screening is not required except as indicated above or where the parking lot abuts the Woodmont Commons PUD boundary.

Screening shall be required along the PUD Perimeter Buffer where non-residential uses or residential uses of higher density in the Woodmont Commons PUD back up to residential uses outside the Woodmont Commons PUD boundary. Existing landscaping,

2.4.6 PUD Site Plan Landscape

topography and other natural or cultural features may count towards the required screening. In other circumstances, where uses on both sides are the same or are compatible, then the landscape treatment, site improvements and architecture may be planned to complement each other and enhance views to, from and across the sites.

The ground mounted signage landscaping area shall be a minimum of four (4) foot wide area surrounding each monument or pedestal sign base. All sign bases shall be planted with shrubs and groundcover with a minimum height of eighteen (18) inches at time of planting.

2.4.7 PUD Site Plan Stormwater Management

Description

The PUD Site Plan Standards for stormwater are provided to allow the design and implementation of stormwater management systems consistent with the design principles of the Woodmont Commons PUD.

Purpose

These stormwater standards protect local watersheds and are consistent with low impact design. Compact development, such as the Woodmont Commons PUD, is beneficial to the management of stormwater runoff by reducing the amount of impervious surfaces.

Applicability

The PUD Master Plan will abide by *Sections 3.05 and 3.06* of the *Londonderry Site Plan Regulations* regarding the design and implementation of storm water management systems except that vegetative swales and rain gardens intended to assist in the treatment of storm water run-off will be allowed by right and subject to design review by staff.

Chloride Management

Beaver Brook has been declared an impacted watershed in need of corrective action due to Chloride loading, most notably resulting from winter maintenance activities. *Section 3.3 Chloride Management* discusses the mitigation measures for chloride management in this watershed.

2.5 ADMINISTRATION

2.5.1 Procedures

The PUD Master Plan serves as the land use regulations for the properties located within the PUD Boundary as outlined in *Section 2.1 Introduction*.

The process for administering the provisions of this PUD Master Plan is as follows:

- A. The Developer, or their assignee, shall be responsible for the coordination of the Land Use Plan and the amount of authorized development within the Woodmont Commons PUD. As such the Developer may be the developer of all or portions of the project, or shall be the first point of contact for all other applicants intending to develop within the PUD Master Plan.
- B. The review and approval of PUD Subdivision and PUD Site Plan submittals within the Woodmont Commons PUD shall be administered by the Town of Londonderry in accordance with the PUD Master Plan and the applicable portions of the *Londonderry Zoning Ordinance, Subdivision Regulations and Site Plan Regulations*. In addition, the following provisions shall apply:
 1. All projects shall be subject to review and acceptance by the Developer prior to submittal to the Town of Londonderry. This acceptance is a pre-requisite for review of any submittal by the Planning Department and all submittals shall be accompanied by written acceptance from the Developer;
 2. All approved projects shall count against the total amount of authorized development for the Woodmont Commons PUD. The Developer shall track and record the amount of development completed to date, the amount of development under review or construction, the amount of development previously approved but not under construction, and the proposed amount of development in

the current application as compared to the authorized development amounts for each Subarea and for the total development as listed in the Allowable Densities Table in *Section 2.2 Land Use Regulations*. Said accounting shall be reported to the Planning Department upon request, but at least annually.

- C. PUD Subdivision Application Procedures and Approval Process:
 1. An applicant preparing to create a PUD Subdivision shall follow the process outlined in the *Londonderry Subdivision Regulations* except that the Initial Inquiry shall be mandatory;
 2. The Developer shall review and accept the application prior to the Formal Application Submission and submit an acceptance letter to the Planning Department.
 - D. PUD Site Plan Application Procedures and Approval Process
 1. An applicant preparing to create a PUD Site Plan shall follow the process outlined in the *Londonderry Site Plan Regulations* except that the Initial Inquiry shall be mandatory;
 2. The Developer shall review and accept the application prior to the Formal Application Submission and submit an acceptance letter to the Planning Department.
 - E. Return of Development Capacity
 1. Once approval has been granted, a developer may disclaim its permit and return the authorized development capacity to the Woodmont Commons PUD.
- A “minor modification and amendment” to the PUD Master Plan may, upon the request of the Developer,

2.5.2 Modifications and Amendments to the PUD Master Plan

be approved by the Planning Board without following the administrative process set forth in *Section 2.8.3* of the *Londonderry Zoning Ordinance*. Instead, any proposed “minor modification and amendment” shall be reviewed by the Planning Board for good cause shown and consistency with the spirit and intent of the PUD Master Plan and the *Londonderry Zoning Ordinance*.

be considered in accordance with *Section 2.8.3.9* of the *Londonderry Zoning Ordinance*. Specifically, the Developer “may apply to amend all or a portion of an approved PUD following the same process applicable to the establishment of a PUD”, including compliance with *Section 2.8.3* of the *Londonderry Zoning Ordinance*.

A “minor modification or amendment” shall mean any and all proposed changes to the PUD Master Plan that does NOT:

1. Add or delete more than 10 acres of real property to the PUD , provided any such additional real property is contiguous to the PUD and is to be regulated by the same PUD Regulations and Standards applicable to the immediately adjacent and contiguous subarea;
2. Alter the “Allowable Uses Table”, or the uses designated as allowable for each Subarea, stated in *Section 2.2.3 Land Use Standards*;
3. Alter the “Land Use Density Table” stated in *Section 2.2.3 Land Use Standards*;
4. Alter the “Waivers/Modifications/Compliance with Specified Development Standards” stated in *Section 2.1.4 Applicability*;
5. Alter the “Block Types - Size and Dimension” criterion stated in *Section 2.3.2 Block Types*; or,
6. Alter the “Building Types – Building Height, Primary Building Height (maximum)” criterion stated in *Section 2.4.2 Building and Lots*.

Proposed amendments to the PUD Master Plan that do not qualify as “Minor Modifications and Amendments”, as the same are defined above, shall

2.6 FORMS AND RECORDS

There are two required forms for this PUD Master Plan – a submittal form in the form of an Acceptance Letter and a Tracking Form that is used to track all projects, their status relative to submittal, approval and rescission and the amount of development capacity used.

Once approval has been granted, a developer may disclaim its permit and return the approved development capacity to the Woodmont Commons PUD. Such rescission will be tracked using the tracking form and reported to the Town on an annual basis as described below.

An example of each form can be found in *Section 4.7 Sample Forms*.

The Tracking Form shall also be provided to Town Staff on an annual basis confirming the amount of development maximums that has been used to date and any changes over the past year.

2.6.1 Submittal Form

An Acceptance Letter from the Developer must accompany any submittal for PUD Subdivision or PUD Site Plan review. The form must be signed by the Developer, confirming compliance with the PUD Master Plan prior to submittal to the Town. An example of this form can be found in *Section 4.7.1 Sample Submittal Form*. This form may be modified with notification to the Planning Board and Town of Londonderry staff.

2.6.2 Tracking Forms

A Tracking Form must be submitted with each application for PUD Site Plan review. This form must be signed by the Developer and will confirm that the project is consistent with the allowable densities and Open Space minimums under *Section 2.2 Land Use Regulations*. An example of this form can be found in *Section 4.7.2 Sample Tracking Form*. This form may be modified with notification to the Planning Board and Town of Londonderry staff.



3.0 Mitigation and Improvement Requirements

WOODMONT COMMONS

PLANNED UNIT DEVELOPMENT MASTER PLAN

JULY 2013

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3.1 TRANSPORTATION

The following measures may be considered by an applicant for PUD Subdivision and/or Site Plan approval and the Planning Board in order to mitigate the impacts of new vehicle trips to the surrounding roadway network within the study area:

- New roadway connections within the PUD to distribute traffic to locations with a higher level of reserve capacity;
 - Roadway widening to add through lanes or auxiliary turn lanes accessing side streets or major access driveways;
 - Installation of, or enhancements to, traffic signals to improve delays for side street movements or improve the flow of traffic between adjacent traffic signals;
 - Construction of sidewalk or bicycle facilities;
 - Traffic calming measures to reduce the speed of traffic in particular areas or influence motorists to use other more appropriate facilities;
 - Installation of signs and pavement markings to enhance pedestrian and bicycle safety;
 - Installation of lane use, directional, and way-finding signs to better define traffic operations or improve motorist identification of major destinations within the PUD, such as a parking facility.
- The overall level-of-service (LOS) for the intersection is degraded by two or more letter grades as a result of the development phase(s);
 - The overall LOS for the intersection is materially degraded to LOS E or F;
 - The volume-to-capacity (v/c) ratio for the overall intersection exceeds 1.00;
 - The LOS of any major movement through the intersection reaches LOS F or the v/c ratio for any major movement exceeds 1.00 and the resultant queues are expected to impact an upstream intersection;
 - The volume of traffic on that movement exceeds the thresholds for mitigation, such as through installation of a traffic signal or construction of turning lanes (Note: high delays for an unsignalized left-turn from a side street shall not require any mitigation if the volumes are not sufficient to warrant a specific traffic control change.)

The conceptual improvement graphics provided within the PUD Master Plan and its attachments may be used as a guide for future consideration, but does not prohibit the introduction of new or refined concepts that provide a comparable mitigated condition.

3.1.1 Thresholds for Physical Mitigation Measures

Alterations may be warranted at an intersection to mitigate the impacts of the project when one or more of the following criteria are satisfied under Build conditions:

The need for improvements at a particular location assumes that the traffic volumes at that location exceed thresholds to satisfy the warranting conditions of a specific mitigation measure, such as the installation of a traffic signal or roadway widening to provide additional lanes. Furthermore, the need for mitigation measures is dependent upon the physical constraints of the roadway or intersection, including, but not limited to, right-of-way limits and proximity to wetlands or historic resources.

If the applicant implements physical mitigation measures that are projected to mitigate the impacts of the project at an intersection (overall LOS and volume-to-capacity ratios), as identified for the entire PUD in the MTIA (or as a similarly scaled improvement), then the applicant shall not be required to perform supplemental improvements to the intersection. In this manner, the funds expended by the applicant to build intersection capacity as mitigation for the PUD will not be eroded by unanticipated background traffic growth.

3.1.1 Thresholds for Physical Mitigation Measures

In the event that an applicant builds a portion of the master planned traffic improvements during an introductory phase of the PUD, the Town shall grant a proportional level of trip generation for the PUD until such time as the balance of the improvements are constructed. As some of the improvements are located within State Highway right-of-way, the New Hampshire Department of Transportation will ultimately make the determination of the adequacy of mitigation within their jurisdiction.

3.2 UTILITIES

3.2.1 Mitigation Methods

The following utility improvement measures may be considered by the applicant for PUD Subdivision and/or Site Plan approval and the Planning Board in order to mitigate the impacts of additional water use or wastewater generation to the existing systems:

- Upgrading water lines for increased flow to the PUD;
- Upgrading water pump stations servicing the PUD or the adjacent area;
- Installation of water storage facilities for reserve water supply and improved water pressures;
- Upgrading sanitary sewer facilities to accommodate the additional wastewater flow through the PUD or to the adjacent system. The improvements will be limited to the lines between the various portions of the PUD and the Derry Wastewater Treatment Facility;
- Replacing or upgrading sewer pump stations to accommodate the design flows for each PUD Subdivision or to improve the efficiency of operations for existing flows;
- Introducing or improving flow monitoring devices in the existing system.

The specific methods required for each PUD Subdivision shall be determined in consultation with Town staff, the water supply licensee and other private utility companies.

3.2.2 Thresholds

The threshold for improvements is highly dependent upon the specific uses proposed in each phase of development as reflected in a PUD Subdivision and/or PUD Site Plan review. The applicant shall identify specific flows, conveyance paths and required mitigation in an infrastructure facilities report provided at the time of each major PUD Subdivision. The applicant will review the proposed improvements and any associated reserve capacity for the PUD with Town Engineering and DPW staff prior to submission to the Planning Board.

3.3 CHLORIDE MANAGEMENT

Beaver Brook has been declared an impacted watershed in need of corrective action due to Chloride loading, most notably resulting from winter maintenance activities. Londonderry has a Salt Reduction Plan (SRP) that provides a procedural framework for the Town of Londonderry to continuously strive to improve winter maintenance operations while effectively and efficiently using road salt during the snow and ice removal operations. Londonderry will provide winter maintenance to town roadways, parking lots and sidewalks in accordance with Londonderry's Winter Maintenance Snow and Ice Control Policy while striving to minimize adverse impacts to the environment by:

- Adhering to the procedures contained within the SRP;
- Committing to ongoing winter maintenance staff training and education;
- Reporting fiscal year salt usage to New Hampshire Department of Environmental Services (NHDES);
- Re-evaluating the effectiveness of the SRP as needed to incorporate new technologies or changes in procedures.

The town has identified four strategies to help meet the Total Maximum Demand Load allocation within the Beaver Brook Watershed. The current load is estimated at 854.47 tons/year imported to the watershed. The town's reduction goals are 6% for year one, 12% for year two, and 18% for year three to be achieved from the following strategies.

- Equipment upgrade pilot project;
- Improved calibration;
- Private sector outreach;
- Improved weather system monitoring.

NHDES will monitor the progress made by each community impacting the Beaver Brook watershed and may require modifications to each plan.

In recognition of the Beaver Brook Watershed chloride reduction plan for Londonderry, and that approximately 40% of the chloride impact is a result of winter operations on private roadways and parking lots, the Woodmont Commons PUD will be guided by the following actions:

- Private Maintenance Contracting Equipment Upgrades and Training – the Woodmont Commons PUD will serve as a model for private sector participation by committing to contract with snow removal and maintenance providers who have been trained and are knowledgeable of Best Management Practices (BMP) for snow removal with reduced salt application. These practices are published and are updated through the UNH – Technology Transfer (T2) program. All private contractors employed by Woodmont Commons PUD roadway maintenance will use only pre-approved methods for spreading abrasives on private roadways and parking lots. When salt is applied, it will be applied only in accordance with best management practices and will include pre-treatment and ground speed controlled spreaders as described in the NHDES *August 2011 Salt Reduction Implementation Plan for Beaver Brook Watershed*;
- Improved Weather System Monitoring – the Woodmont Commons PUD will coordinate weather information gathered by both the town and NHDOT for use by maintenance contractors. In doing so, contractors will be able to make more informed decisions as to when and to what extent chloride is applied to roadways, sidewalks and parking lots;
- Increased Mechanical Removal Capabilities – Woodmont Commons PUD will endeavor to use mechanical removal means on a more frequent basis using more manpower and equipment than traditionally used to shorten plowing intervals and reduce the need for salting to break up hard packed snow. Shortened maintenance routes with shorter service inter-

vals will be used to stay ahead of snowfall, thus reducing the need to sand and salt after snow and ice packing has already occurred;

- Public Awareness Campaign – Woodmont Commons PUD will inform all future developers, grantees, and tenants of the need to reduce salt application on roadways, parking lots and sidewalks.

The above described methods will be published in an operations manual with sample specifications used to retain private road snow removal contractors. The manual will serve as a model framework that could be adopted by other private developments for their roadways and parking lots as part of an overall outreach program. The manual, as with the Town's Salt Reduction Plan, is intended to be a dynamic document modified as required based on knowledge gained from past practices and technological advancement to reflect chloride best management practices.

3.4 RELATIONSHIP TO THE DEVELOPMENT AGREEMENT

For a discussion of relative financial responsibilities for mitigation measures pertaining to roadway and transportation improvements, wastewater collection and treatment facilities, potable water distribution lines and facilities, and recreational and community facilities, please refer to Sections VI and VII of the Woodmont Commons Development Agreement. A copy of this document is provided in *Section 4.5 Development Agreement*.



4.0 Supplemental Documents

WOODMONT COMMONS

PLANNED UNIT DEVELOPMENT MASTER PLAN

JULY 2013

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4.1 UPDATED ABUTTERS LIST

The following pages contain the abutters list as of December 31, 2012.

| Parcel ID | Owner(1) | Owner Address | City | State | Zip |
|-------------|---|-----------------------------|---------------|-------|------------|
| 007 065 1 | KELLEY ROBERT E + JEAN C | 87 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 007 065 2 | KELLEY ROBERT E + JEAN C | 87 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 007 065 3 | SMITH KELLEY RAE REVOC TRUST | PO BOX 1105 | LONDONDERRY | NH | 03053 |
| 007 067 0 | LEEMEN CORPORATION | 220 N MAIN ST | NATICK | MA | 01760 |
| 007 068 0 | CAROLINA REALTY PARTNERS INC | 625 AMHERST ST | NASHUA | NH | 03063 |
| 007 068 1 | NH SIX REALTY TRUST C/O HAJJAR MGMT CO, INC. CHARLES C. HAJJAR, TRUSTEE | 30 ADAMS ST | MILTON | MA | 02186 |
| 009 064 6 | KIM JONG S + YOUN MI | 62 PILLSBURY RD | LONDONDERRY | NH | 03053 |
| 010 001 0 | NEW HARBOR PROPERTIES INC. | 1F COMMONS DR SUITE 39 | LONDONDERRY | NH | 03053 |
| 010 001 1 | GILCREAST ROAD LLC | P.O. BOX 1330 | LONDONDERRY | NH | 03053 |
| 010 001C 0 | LONDONDERRY COMMONS | PO BOX 465 | LONDONDERRY | NH | 03053 |
| 010 001C 14 | CALVIN RICHARD A REVOC TRUST | 6 MOUNTAIN FARM RD | BOW | NH | 03304 |
| 010 001C 15 | Anschi, LLC | 1C Commons Drive, Suite 16 | LONDONDERRY | NH | 03053 |
| 010 001C 16 | Anschi, LLC | 1C Commons Drive, Suite 16 | LONDONDERRY | NH | 03053 |
| 010 001C 17 | 1C COMMONS DR UNIT 17 LLC | 1C COMMONS DR UNIT 17 | LONDONDERRY | NH | 03053 |
| 010 001C 18 | JTLB LLC | 1C COMMONS DR UNIT 18 | LONDONDERRY | NH | 03053 |
| 010 001C 19 | LOT 11-C-700 LLC | PO BOX 439 | LONDONDERRY | NH | 03053 |
| 010 001C 20 | LOT 11-C-700 LLC | PO BOX 439 | LONDONDERRY | NH | 03053 |
| 010 001C 21 | LOT 11-C-700 LLC | PO BOX 439 | LONDONDERRY | NH | 03053 |
| 010 001C 22 | LOT 11-C-700 LLC | PO BOX 439 | LONDONDERRY | NH | 03053 |
| 010 001C 23 | LOT 11-C-700 LLC | PO BOX 439 | LONDONDERRY | NH | 03053 |
| 010 001L 0 | LC LONDONDERRY LLC | 1E COMMONS DR UNIT 30 | LONDONDERRY | NH | 03053 |
| 010 001 2 | NEW HARBOR PROPERTIES INC. | 1F COMMONS DR SUITE 39 | LONDONDERRY | NH | 03053 |
| 010 002 0 | VACCO RICHARD P + MARJEANNE B | 78 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 003 0 | HAUSSLER JOHN A & MARY | PO BOX 912 | LONDONDERRY | NH | 03053-0912 |
| 010 005 0 | TRAVASSOS BRANDON A | 91 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 006 0 | HOGAN MATTHEW A & MARCELA SMITH-HOGAN | 93 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 007 0 | DAHL EDWARD F | 95 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 008 0 | MACASKILL WAYNE + DARLENE | 97 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 009 0 | KUKOWSKI JOHN E + CAROL A | 99 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 010 0 | EMERY EARL C + ALBERTA M | 101 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 011 0 | FOLEY RONALD T + EMILY C | 103 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 012 0 | WOODMONT ORCHARDS INCORPORATED | 80 NASHUA RD | LONDONDERRY | NH | 03053 |
| 010 013 0 | SUGAR PLUM HILL LLC | 100 ANDOVER BY-PASS STE 300 | NORTH ANDOVER | MA | 01845 |
| 010 013C 1A | ELLIOT MARK J | 107A GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 013C 1B | HEGARTY FRANK G + BERNADETTE | 107B GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 014 10 | AUGERI ROBERT + KIMBERLY | 49 PILLSBURY RD | LONDONDERRY | NH | 03053 |
| 010 014 6 | INSERRA JOYCE I & NANCY SALVUCCI | 57 PILLSBURY RD | LONDONDERRY | NH | 03053 |
| 010 014 7 | DALPHOND KATHLEEN M | 55 PILLSBURY RD | LONDONDERRY | NH | 03053 |

| Parcel ID | Owner(1) | Owner Address | City | State | Zip |
|-------------|---|-----------------------|-------------|-------|-------|
| 010 014 8 | DALRYMPLE PHILLIP + PRISCILLA | PO BOX 1045 | LONDONDERRY | NH | 03053 |
| 010 014 9 | SMITH CRAIG C + DORIS | 51 PILLSBURY RD | LONDONDERRY | NH | 03053 |
| 010 014A 0 | NEWELL JEFFREY D | 104 GILCREAST RD | LONDONDERRY | NH | 03053 |
| 010 016 10 | OBRIEN DAVID W & LORRAINE FAMILY TRUST | 4 GARY DR | LONDONDERRY | NH | 03053 |
| 010 016 11 | HICKS NATHANIEL E + JULIE L | 6 GARY DR | LONDONDERRY | NH | 03053 |
| 010 016 12 | HAUSER ALLEN E + PATRICIA E | PO BOX 208 | LONDONDERRY | NH | 03053 |
| 010 016 13 | TYRRELL JUDITH S | 10 GARY DR | LONDONDERRY | NH | 03053 |
| 010 016 14 | HOULE MARGO S | 12 GARY DR | LONDONDERRY | NH | 03053 |
| 010 016 15 | JOHNSON FREDERICK C & DIANE L. | 14 GARY DR | LONDONDERRY | NH | 03053 |
| 010 016 16 | KEVIN BAILEY | 16 GARY DR | LONDONDERRY | NH | 03053 |
| 010 016 17 | BRATHWAITE DEXTER P + BONNIE A | 18 KITT LN | LONDONDERRY | NH | 03053 |
| 010 016 18 | SCHMIDT ALAN D + DONNA M | 20 KITT LN | LONDONDERRY | NH | 03053 |
| 010 016 19 | CARTER ROBERT G + DEBORAH A | 22 KITT LANE | LONDONDERRY | NH | 03053 |
| 010 016 20 | TISHLER DONALD + LINDA & TISHLER, LINDSEY | 24 KITT LN | LONDONDERRY | NH | 03053 |
| 010 016 21 | NORTON ROBERT H + ALICE B | 26 KITT LN | LONDONDERRY | NH | 03053 |
| 010 016 22 | RICE DAVID W JR & JEAN | 28 KITT LN | LONDONDERRY | NH | 03053 |
| 010 016 23 | GALLO PETER F | 30 KITT LN | LONDONDERRY | NH | 03053 |
| 010 016 24 | JEFFREY M. BARNARD | 32 KITT LN | LONDONDERRY | NH | 03053 |
| 010 016 25 | AUGERI MATTHEW A + LEANNE | 34 KITT LN | LONDONDERRY | NH | 03053 |
| 010 016A 0 | SHATTO JACQUES M | 2 HARDY RD | LONDONDERRY | NH | 03053 |
| 010 018 0 | CTI TOWERS, INC. | 38 POND ST, SUITE 305 | FRANKLIN | MA | 02038 |
| 010 018 1 | BEAULIEU JOSEPH & SUSAN FRANZ | 51 HOVEY RD | LONDONDERRY | NH | 03053 |
| 010 018 2 | MAUCERI DAVID A + JULIANNE M | 1 DRAGONFLY WAY | LONDONDERRY | NH | 03053 |
| 010 019 0 | BOLES, CURTIS M. & LINDA E. 2011 REVOCABLE TRUSTS | 57 HOVEY RD | LONDONDERRY | NH | 03053 |
| 010 023A 0 | CHAPDELAIN KEVIN M | 43 TROLLEY CAR LANE | LONDONDERRY | NH | 03053 |
| 010 023A A | DERRY TOWN OF, C/O TOWN ADMINISTRATOR | 14 MANNING ST | DERRY | NH | 03038 |
| 010 028 0 | D'URSO, LORRENE & ERIN | 42 TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 029C 1A | CARTER CHARLES + LINDA | 34A TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 029C 1B | COSTA LISETTE D | 34B TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 029C A | BROWN ROCHELLE | 36A TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 029C B | LANGLEY, JAMES E | 36B TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 030 0 | DOUCETTE DENNIS + CAROLYN L | 28 TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 030 3 | DICKENSON DONNA E | 24 TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 033 | OLIVERIO, CHRIS M. & RHINE, TRACY A. | 1 TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 034 | TOWN OF LONDONDERRY | 268B MAMMOTH RD | LONDONDERRY | NH | 03053 |
| 010 037C 7A | 15A TROLLEY CAR LLC | 4 PARK DRIVE | BEDFORD | NH | 03310 |
| 010 037C 7B | SALVAGE JACQUELINE L & JAMES M. HOWE | 15 B TROLLEY CAR LN | LONDONDERRY | NH | 03053 |
| 010 038 0 | DANIELS JOSEPH E | PO BOX 342 | LONDONDERRY | NH | 03053 |
| 010 038 2 | MILLER DORIS J | 16 HOVEY RD | LONDONDERRY | NH | 03053 |
| 010 038 3 | ROY GAIL M REVOCABLE TRUST | 32 HOVEY RD | LONDONDERRY | NH | 03053 |
| 010 038 4 | BOLES JEFFREY M + RACHEL B | 28 HOVEY ROAD | LONDONDERRY | NH | 03053 |
| 010 038 5 | STODDARD JOHN | 26 HOVEY RD | LONDONDERRY | NH | 03053 |
| 010 039 0 | TROMBLEY EDWARD J & TIMOTHY TROMBLEY | 24 HOVEY RD | LONDONDERRY | NH | 03053 |
| 010 039 1 | GOODWIN JASON M | 20 HOVEY RD | LONDONDERRY | NH | 03053 |

| Parcel ID | Owner(1) | Owner Address | City | State | Zip |
|------------|--|------------------------------|-------------|-------|-------|
| 010 040 0 | HALL CHESTER H JR | PO BOX 75 | LONDONDERRY | NH | 03053 |
| 010 040 1 | IRONWOOD REAL EST HOLDINGS LLC | 57 NOYES RD | LONDONDERRY | NH | 03053 |
| 010 042 1 | RICHARD D. & DAWN N. SHEA | 1 HOVEY ROAD | LONDONDERRY | NH | 03053 |
| 010 042 5 | HILLARD RICHARD J JR & LORI E. BELLINO | 9 HOVEY RD | LONDONDERRY | NH | 03053 |
| 010 042 6 | ONYX PROPERTY DEVELOPMENT INC | ONE JEFFERSON DR | LONDONDERRY | NH | 03053 |
| 010 042 7 | ONYX PROPERTY DEVELOPMENT INC | ONE JEFFERSON DR | LONDONDERRY | NH | 03053 |
| 010 043 0 | VELIE JAMES D & KIMBERLY A. | 38 PILLSBURY RD | LONDONDERRY | NH | 03053 |
| 010 044 0 | SARTORI RONALD F & NANCY H. | 42 PILLSBURY RD | LONDONDERRY | NH | 03053 |
| 010 045 1 | COMES BRUCE R + CATHARINE | 8 ASH ST | LONDONDERRY | NH | 03053 |
| 010 045 2 | RUSSELL GARY H + NANCY E | 6 ASH ST | LONDONDERRY | NH | 03053 |
| 010 045 3 | KURTIS R. TANGUAY | 4 ASH ST | LONDONDERRY | NH | 03053 |
| 010 045 4 | DANAHY DANIEL A | 2 ASH ST | LONDONDERRY | NH | 03053 |
| 010 049 0 | SEVEN HILLS DEVELOPMENT LLC | PO BOX 1220 | DERRY | NH | 03038 |
| 010 053 0 | NEW HAMPSHIRE STATE OF, DEPT OF PW & HWYS | 85 LOUDON RD | CONCORD | NH | 03301 |
| 010 054 0 | NEW ENGLAND INDUST PROP INC | 12 LEXINGTON ST | LEWISTON | ME | 04240 |
| 010 055 0 | KC PROPCO LLC C/O KNOWLEDGE LEARNING CORP | PO BOX 6760 | PORTLAND | OR | 97228 |
| 010 056 0 | NEW HAMPSHIRE STATE OF, DEPT OF PW & HWYS | 7 HAZEN DRIVE | CONCORD | NH | 03301 |
| 010 060 0 | SILVA LORI ANN | 2 BUYCK AV | LONDONDERRY | NH | 03053 |
| 010 061 0 | THERIEN ANDREW J & VALLEE, MICHELLE M | PO BOX 633 | LONDONDERRY | | 03053 |
| 010 063 0 | MORRISSETTE HAROLD J JR | 20 ASH ST | LONDONDERRY | NH | 03053 |
| 010 064 | DONAGHEY, WILLIAM & CANDY | 18 ASH ST | LONDONDERRY | NH | 03053 |
| 010 065 0 | ROSA JOSEPH A + MARION M | 16 ASH ST | LONDONDERRY | NH | 03053 |
| 010 066 0 | TILLEY DANIEL K | 14 ASH ST | LONDONDERRY | NH | 03053 |
| 010 067 0 | MACK DANIEL + CHARLENE | 12 ASH ST | LONDONDERRY | NH | 03053 |
| 010 068 0 | WANAT LARRY D + SHEILA M | 10 ASH ST | LONDONDERRY | NH | 03053 |
| 010 072 0 | INNIE IRENE P FAMILY TRUST | 3 ASH ST | LONDONDERRY | NH | 03053 |
| 010 083 0 | PINKNEY DEBBIE | 21 ASH ST | LONDONDERRY | NH | 03053 |
| 010 084 0 | NAULT DENISE M | 23 ASH ST | LONDONDERRY | NH | 03053 |
| 010 085 0 | 27 ASH ST LLC | 27 ASH ST | LONDONDERRY | NH | 03053 |
| 010 086 0 | NEW HAMPSHIRE STATE OF | PO BOX 483, 1 HAZEN DR | CONCORD | NH | 03301 |
| 010 087 0 | £33 LONDONDERRY ROAD LLC C/O LOUISE VON DAMM | 64-17 28TH AVE | ASTORIA | NY | 11103 |
| 010 138 0 | CRACKER BARREL COUNTRY STORE LLC | 6920 POINTE INVERNESS | FT. WAYNE | IN | 46804 |
| 010 142 0 | PENNICHUCK EAST UTILITY INC | 25 MANCHESTER ST PO BOX 1947 | MERRIMACK | NH | 03054 |
| 010 148 0 | LIONEL R. LABONTE | 335 RTE 125 | BRENTWOOD | NH | 03833 |
| 010 148 1 | LIONEL R. LABONTE & GRETA A. LABONTE | 335 RTE 125 | BRENTWOOD | NH | 03833 |
| 010 150 0 | STATE OF NEW HAMPSHIRE | P.O. BOX 483 7 HAZEN DR | CONCORD | NH | 03302 |
| 010 151 0 | NEW HAMPSHIRE STATE OF, DEPT OF PW & HWYS | 85 LOUDON RD | CONCORD | NH | 03301 |
| 010 154 0 | LONDONDERRY TOWN OF | 268B MAMMOTH RD | LONDONDERRY | NH | 03053 |
| 013 001 11 | FERREIRA JOHN L JR & BARBARA A. | 58 HOVEY RD | LONDONDERRY | NH | 03053 |
| 013 001 12 | DUMONT RONALD A + CRISTETA A | 47 TROLLEY CAR LANE | LONDONDERRY | NH | 3053 |
| 013 001 14 | LONDONDERRY TOWN OF | 268B MAMMOTH RD | LONDONDERRY | NH | 03053 |
| 013 088 1 | DORAIAKANNAN KUMARAVELAN M | 3 COTE LN | LONDONDERRY | NH | 03053 |
| 013 088 2 | ROSSI DAVID + MICHELLE | 18 SNOWCAP WAY | LONDONDERRY | NH | 03053 |

| Parcel ID | Owner(1) | Owner Address | City | State | Zip |
|------------|--|--------------------|-------------|-------|-------|
| 013 088 4 | MANDRIOLI JOHN SCOTT | 60 SEASONS LN | LONDONDERRY | NH | 03053 |
| 013 088 5 | TONKIN-BEATON ELIZABETH T & SCOTT BEATON | 62 SEASONS LN | LONDONDERRY | NH | 03053 |
| 013 088 6 | JAMES W. & CYNTHIA G. ALLEY | 64 SEASONS LN | LONDONDERRY | NH | 03053 |
| 013 088 7 | WALL GARY & CONNIE | 66 SEASONS LN | LONDONDERRY | NH | 03053 |
| 013 088 8 | ROTHERMUND VICTORIA L + MARC | 58 BOSTON ST | MIDDLETON | MA | 01949 |
| 013 090 0 | DEMERS MARCEL E + RITA A | 30 COTEVILLE RD | LONDONDERRY | NH | 03053 |
| 013 094 0 | PECK JAMES D JR & PECK, TIMOTHY | 40 COTEVILLE RD | LONDONDERRY | NH | 03053 |
| 013 095 0 | SULLIVAN KIM J + PATRICK F | 37 COTEVILLE RD | LONDONDERRY | NH | 03053 |
| 013 095A 0 | HYRAX DERRY PARTNERS LLC C/O RICH MAY | 176 FEDERAL ST | BOSTON | MA | 02210 |
| DERRY: | | | | | |
| | 31011 SEVEN HILLS DEVELOPMENT LLC | PO BOX 1220 | DERRY | NH | 03038 |
| | 31011 SEVEN HILLS DEVELOPMENT LLC | PO BOX 1220 | DERRY | NH | 03038 |
| | 35003 HYRAX DERRY PARTNERS LLC, C/O RICH MAY | 176 FEDERAL ST | BOSTON | MA | 02210 |
| | 35101 HYRAX DERRY PARTNERS LLC, C/O RICH MAY | 176 FEDERAL ST | BOSTON | MA | 02210 |
| 31005-006 | DERRY BUSINESS CENTER LLC | 11 CORPORATE DRIVE | BELMONT | NH | 03220 |
| 31005-007 | PEARL E. SCIUTO | 5 TSIENNETO RD U30 | DERRY | NH | 03038 |
| 31005-023 | GLENN D. & ROSALIND WANTE | 19 ASH ST | DERRY | NH | 03038 |

4.2 MASTER PLAN TRAFFIC IMPACT ASSESSMENT

4.3 INFRASTRUCTURE MEMORANDUM

4.4 MASTER FISCAL IMPACT ANALYSIS

These three sets of documents are provided as separately bound reports.

Section 4.2 Master Plan Traffic Impact Assessment includes the *Master Plan Traffic Impact Assessment*, dated February 6, 2013 and the *Exit 4A Sensitivity Analysis* dated April 10, 2013, both by TEC.

Section 4.3 Infrastructure Memorandum includes the *Utility Memo*, dated May 15, 2013, by CMA.

Section 4.4 Master Fiscal Impact Analysis contains the *Woodmont Commons Fiscal Impact Analysis*, dated May 17, 2013 and the *Technical Memorandum, Woodmont Commons Fiscal Impact, Londonderry School District*, dated May 20, 2013, both by Development Planning & Financing Group, Inc.

All three sections are to be used as described in *Section 2.0 PUD Regulations and Standards* as baseline reports for comparison to later supplemental reports that may be provided with PUD Subdivision and/or PUD Site Plan submittals.

4.5 DEVELOPMENT AGREEMENT

The Development Agreement has not yet been finalized between the Town of Londonderry and the Developer.

4.6 SAMPLE RESTRICTIVE COVENANT

This section includes a form of restrictive covenant to be executed and recorded by each Developer.

Return to: Town of Londonderry Planning Department
268B Mammoth Road
Londonderry, NH 03053

RESTRICTIVE COVENANT

[GRANTOR], a New Hampshire [Entity] having offices at [Address] (“Covenantor”), for consideration received and as a condition of approval for the Woodmont Commons Master Plan approved by the Planning Board of the Town of Londonderry, New Hampshire on *Month Day, 2013* for a Planned Unit Development known as Woodmont Commons (“Approval”), hereby enters into this Restrictive Covenant relating to said real property in Londonderry, New Hampshire and further identified as Town of Londonderry Parcel Identification Nos. _____ (“the Restricted Properties”).

As a condition of the Approval, Covenantor irrevocably agrees that the development, redevelopment, operation and maintenance of the Restricted Properties shall now and forever conform to the Woodmont Commons Master Plan, as the same may be amended by the Town of Londonderry Planning Board from time to time.

The covenants, conditions, reservations and restrictions of this Restrictive Covenant shall run with and bind the Restricted Properties in perpetuity and shall inure to the benefit of the Town of Londonderry, New Hampshire.

Prior to the Covenantor’s conveyance of any property within Woodmont Commons, each deed conveying land within Woodmont Commons recorded subsequent to the recordation of this Declaration shall state that the respective conveyance is subject to this Declaration, but that the failure to include such statement, or to include a sufficiently suitable statement, in a deed, shall not affect the enforceability of the Declarations with respect to the property conveyed or invalidate the respective conveyance. Further, no projects, rezoning petition, Master Plan adjustment, variation, alteration or amendment, application for variance, special exception or conditional use approval shall be submitted to the Town, or its land use boards, or proposed in any manner, without the written consent of the Master Developer, which shall not be unreasonably withheld.

Use and development of the Restricted Properties is subject to regulations applicable to Woodmont Commons as stated in the Master Plan, a copy of which is available for inspection at the Planning

Department of the Town of Londonderry (the "Planning Department) at the Londonderry Town Hall, 268 B Mammoth Road, Londonderry, New Hampshire 03053.

Enforcement of this Restrictive Covenant shall be by any appropriate proceeding in law or equity in any court or administrative tribunal having jurisdiction against any person, persons, firm, company, trust, corporation, or other entity violating or attempting to violate or circumvent any covenant, condition, reservation or restriction contained herein, either to restrain a violation or to recover damages, or both. Failure to enforce any covenant, condition, reservation or restriction contained herein shall in no event be deemed a waiver of the right to do so thereafter. In the event of litigation or administrative proceedings to enforce these covenants, conditions, reservations and restrictions, or to recover damages, the prevailing party shall be entitled to recover court costs and reasonable attorney's fees.

Meaning and intending to restrict real properties conveyed to Covenantor by deed of _____ dated _____, 20____, and recorded in the Rockingham County Registry of Deeds in Book _____, Page _____.

Executed this _____ day of _____, 2013.

[Land Owner]

BY _____, ITS _____

STATE OF NEW HAMPSHIRE
COUNTY OF _____

This instrument was acknowledged before me on _____ by _____ as a _____ of _____.

Notary Public

Print Name _____
My Commission Expires _____

4.7 SAMPLE FORMS

The following two forms are examples of the forms described in *Section 2.6 Forms and Records*.

4.7.1 Sample Submittal Form

| | |
|--|-----------------------|
| Woodmont Commons PUD | |
| Subarea: _____ | Project Name: _____ |
| Project Type _____ | Project ID: _____ |
| PUD Subdivision <input type="checkbox"/> | Date Submitted: _____ |
| PUD Site Plan <input type="checkbox"/> | Date Approved: _____ |

Town of Londonderry Planning Department
 268B Mammoth Road
 Londonderry, NH 03053

SUBMITTAL FORM

The Developer certifies that this *PUD Subdivision/PUD Site Plan/PUD Subdivision and PUD Site Plan* submittal is in conformance with the PUD Master Plan.

The initial inquiry was conducted on _____.
 (date)

The Developer accepted the formal application submission on _____.
 (date)

The impact of this project on the PUD and Subarea Maximum Development is as follows:

| | Land Use Categories | Allocated Development for This Project | Total Maximum PUD Development | New Total Allocated To Date | Total Maximum Subarea Development | New Total Allocated to Date |
|-----------------|-----------------------------------|--|-------------------------------|-----------------------------|-----------------------------------|-----------------------------|
| | Agricultural | | No limit | | | |
| Residential | New Primary Residence | | 1,300 units | | | |
| | New Accessory Units | | 130 units | | | |
| | Existing Units | | 9 units | | | |
| Institutional | Hospital | | 250,000 GSF or 300 beds | | | |
| | Nursing Homes and Assisted Living | | 250,000 GSF | | | |
| | Civic | | No limit | | | |
| Accommodation | Hotels | | 3 hotels/550 rooms | | | |
| | PUD Bed and Breakfast | | No limit | | | |
| Commercial Uses | New Commercial Uses | | 882,500 GSF | | | |
| | Office Building Space | | 700,000 GSF | | | |
| | Existing Commercial Uses | | 272,000 GSF | | | |

4.7.1 Sample Submittal Form

| | |
|--|-----------------------|
| Woodmont Commons PUD | |
| Subarea: _____ | Project Name: _____ |
| Project Type _____ | Project ID: _____ |
| PUD Subdivision <input type="checkbox"/> | Date Submitted: _____ |
| PUD Site Plan <input type="checkbox"/> | Date Approved: _____ |

The impact of this project on the Open Space and Perimeter Buffer minimums is as follows:

| Open Space and Buffer Categories | Allocated for This Project | Total Minimum PUD Development | New Total Allocated To Date | Total Minimum Subarea Development | New Total Allocated to Date |
|----------------------------------|----------------------------|-------------------------------|-----------------------------|-----------------------------------|-----------------------------|
| Conserved Green Space | | 89 | | | |
| Shared Open Space | | 64 | | | |
| Perimeter Buffer | | 38 | | | |

Notes:

Signed this _____ day of _____, 2013.

[Developer]

BY _____
 _____, ITS _____

4.7.2 Sample Tracking Form

Summary Page: Total PUD and Subarea Totals

Woodmont Commons

Sample Tracking Form: Summary Sheet

Last Updated:

Allowable Densities Table

| LAND USE CATEGORIES | | UNIT OF MEASURE | Total PUD Maximum Development | Total PUD Maximum Allocated To Date | Remaining Allocation Capacity | WC-1-GL |
|---------------------|-----------------------------------|-----------------|-------------------------------|-------------------------------------|-------------------------------|---------|
| Total Land Area | | Acres | - | - | - | - |
| Agricultural | | All Types | N/A | No limit | | |
| Residential | New Primary Residence | Units | 1,300 | - | 1,300 | - |
| | New Accessory Units | Units | 130 | - | 130 | - |
| | Existing Units | Units | 9 | - | 9 | - |
| | Maximum Total | Units | 1,439 | - | 1,439 | - |
| Institutional | Hospital | Beds/GSF | 250,000 | - | 250,000 | - |
| | Nursing Homes and Assisted Living | GSF | 250,000 | - | 250,000 | - |
| | Maximum Total | GSF | 500,000 | - | 500,000 | - |
| Civic | | All Types | N/A | No limit | | |
| Accommodations | Hotels (all types) | No./Rooms | 3/550 | - | 550 | - |
| | PUD Bed and Breakfast | Rooms | No limit | - | | - |
| Commercial Use | New Commercial Uses | GSF | 882,500 | - | 882,500 | - |
| | Office Building Space | GSF | 700,000 | - | 700,000 | - |
| | Existing Commercial Uses | GSF | 272,000 | - | 272,000 | - |
| | Maximum Subtotal | GSF | 1,854,500 | - | 1,854,500 | - |
| Flexibility Factors | | | | | | 15% |

| OPEN SPACE AND BUFFER CATEGORIES | | UNIT OF MEASURE | Total PUD Minimum Area | Total PUD Minimum Allocated To Date | Remaining Allocation Capacity | |
|----------------------------------|-----------------------|-----------------|------------------------|-------------------------------------|-------------------------------|---|
| Green and Open Space | Conserved Green Space | Acres | 88.5 | - | 88.5 | - |
| | Shared Open Space | Acres | 63.5 | - | 63.5 | - |
| | Total | Acres | 152.0 | - | 152.0 | - |
| Perimeter Buffers | | Acres | 38 | 38 | - | - |

4.7.2 Sample Tracking Form

Examples of Subarea Tracking Pages

Woodmont Commons

Sample Tracking Form: Subarea WC-1-GL

Last Updated:

| Allowable Densities Table | | UNIT OF MEASURE | Total Subarea Maximum WC-1-GL | Total Projects to Date | Difference | Project 1 | Project 2 | Project 3 | Project 4 | Project 5 |
|----------------------------|-----------------------------------|-----------------|-------------------------------|------------------------|------------|-----------|-----------|-----------|-----------|-----------|
| LAND USE CATEGORIES | | | | | | | | | | |
| Total Land Area | | Acres | 38 | - | 38 | | | | | |
| Agricultural | | All Types | N/A | | | | | | | |
| Residential | New Primary Residence | Units | - | - | - | | | | | |
| | New Accessory Units | Units | - | - | - | | | | | |
| | Existing Units | Units | - | - | - | | | | | |
| | Maximum Total | Units | - | - | - | - | - | - | - | - |
| Institutional | Hospital | GSF | - | - | - | | | | | |
| | Nursing Homes and Assisted Living | GSF | - | - | - | | | | | |
| | Maximum Total | GSF | - | - | - | - | - | - | - | - |
| Civic | | All Types | N/A | | | | | | | |
| Accommodations | Hotels (all types) | No./Rooms | - | - | - | | | | | |
| | PUD Bed and Breakfast | Rooms | - | - | - | | | | | |
| Commercial Use | New Commercial Uses | GSF | 50,000 | - | 50,000 | | | | | |
| | Office Building Space | GSF | - | - | - | | | | | |
| | Existing Commercial Uses | GSF | 225,000 | - | 225,000 | | | | | |
| | Maximum Subtotal | GSF | 275,000 | - | 275,000 | - | - | - | - | - |
| Flexibility Factors | | | 15% | | | | | | | |

| OPEN SPACE AND BUFFER CATEGORIES | | UNIT OF MEASURE | Total Subarea Minimum | Total Projects to Date | Difference | Project 1 | Project 2 | Project 3 | Project 4 | Project 5 |
|----------------------------------|-----------------------|-----------------|-----------------------|------------------------|------------|-----------|-----------|-----------|-----------|-----------|
| Green and Open Space | Conserved Green Space | Acres | | - | - | | | | | |
| | Shared Open Space | Acres | 1.5 | - | 2 | | | | | |
| | Total | Acres | 1.5 | - | 1.5 | | | | | |
| Perimeter Buffers | | Acres | - | - | - | | | | | |

4.7.2 Sample Tracking Form

Woodmont Commons

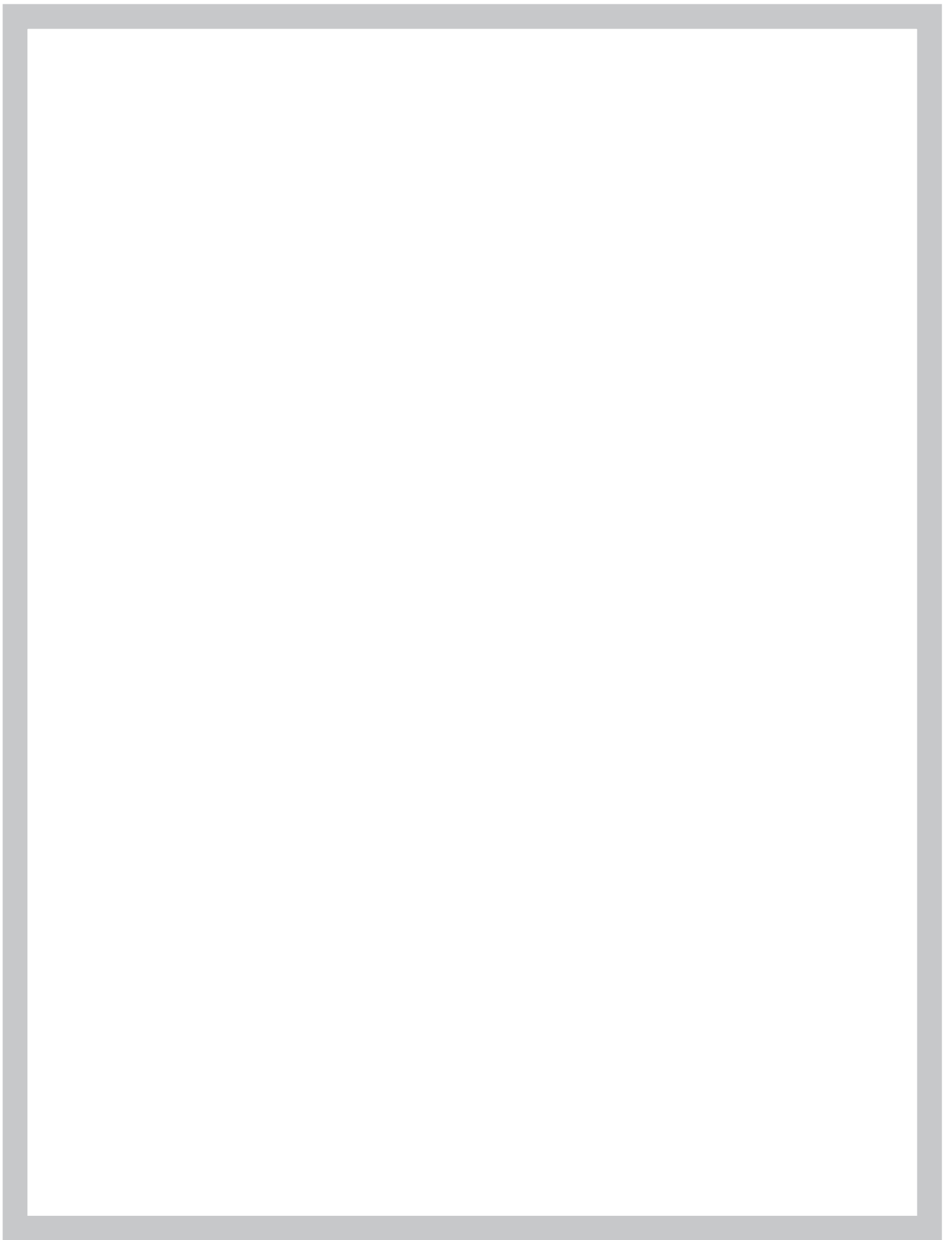
Sample Tracking Form: Subarea WC-8

Last Updated:

| Allowable Densities Table | | UNIT OF MEASURE | Total Subarea | Total Projects to | Difference | Project 1 | Project 2 | Project 3 | Project 4 | Project 5 |
|----------------------------------|-----------------------------------|-----------------|---------------|-------------------|------------|-----------|-----------|-----------|-----------|-----------|
| LAND USE CATEGORIES | | | Maximum | Date | | | | | | |
| Total Land Area | | Acres | 70 | - | 70 | | | | | |
| Agricultural | | All Types | N/A | | | | | | | |
| Residential | New Primary Residence | Units | 190 | - | 190 | | | | | |
| | New Accessory Units | Units | 40 | - | 40 | | | | | |
| | Existing Units | Units | | - | - | | | | | |
| | Maximum Total | Units | 230 | - | 230 | - | - | - | - | - |
| Institutional | Hospital | Beds/GSF | | - | - | | | | | |
| | Nursing Homes and Assisted Living | GSF | 20,000 | - | 20,000 | | | | | |
| | Maximum Total | GSF | | - | 20,000 | - | - | - | - | - |
| Civic | | All Types | N/A | | | | | | | |
| Accommodations | Hotels (all types) | No./Rooms | | - | - | | | | | |
| | PUD Bed and Breakfast | Rooms | | - | - | | | | | |
| Commercial Use | New Commercial Uses | GSF | 5,000 | - | 5,000 | | | | | |
| | Office Building Space | GSF | | - | - | | | | | |
| | Existing Commercial Uses | GSF | | - | - | | | | | |
| | Maximum Subtotal | GSF | 5,000 | - | 5,000 | - | - | - | - | - |
| Flexibility Factors | | | 15% | | | | | | | |
| OPEN SPACE AND BUFFER CATEGORIES | | UNIT OF MEASURE | Total Subarea | Total Projects to | Difference | Project 1 | Project 2 | Project 3 | Project 4 | Project 5 |
| | | | Minimum | Date | | | | | | |
| Green and Open Space | Conserved Green Space | Acres | 10.0 | - | 10 | | | | | |
| | Shared Open Space | Acres | 1.0 | - | 1 | | | | | |
| | Total | Acres | 11.0 | - | 11.0 | | | | | |
| Perimeter Buffers | | Acres | 1.9 | - | 1.9 | | | | | |

- 4.8 LONDONDERRY ZONING ORDINANCE, AS AMENDED THROUGH DECEMBER 2011
- 4.9 SUBDIVISION REGULATIONS, JUNE 10, 2009
- 4.10 SITE PLAN REGULATIONS, JUNE 10, 2009
- 4.11 TYPICAL DETAILS FOR SITE AND ROADWAY INFRASTRUCTURE, MAY 2009

These four documents will be bound with the final PUD Master Plan as reference documents.





Woodmont Public Hearing
Wednesday 08/28/13

RE: Public Comments Submission

To the Chair and Members of the Londonderry Planning Board:

I am respectfully requesting that the following comments are included as part of the public record for tonight's hearing and that a copy of the same be included as part of the official record for this application. I also want to apologize in advance if any of the topics I'm going bring up tonight have been previously discussed during the prior two hearings. Hopefully, you will indulge me the time to deliver my comments to the board in full as part of tonight's proceedings.

For the first part of my comments this evening, I would like to call the Board's attention to some pertinent references from our existing zoning ordinance which in my opinion should be an integral part of all public discussions & hearings on the Woodmont project going forward (including tonight's meeting). These sections of our zoning ordinance are directly related to helping our community properly plan for & mitigate against infrastructure-related costs from future residential development projects.

The first section is Residential Development Phasing and the second is Growth Management and Innovative Land Use Control. Both sections can be found on pages 14 through 20 in the town's existing Zoning Ordinance.

In summary, Residential Development Phasing & Growth Management (GMO) are zoning & planning tools that were expressly enabled by the NH Legislature under RSA 674:21 & 674:22 respectively. The legislature provided these tools so that local communities just like Londonderry would have access to appropriate safety valves against the costs of future residential development projects, regardless of the location, size or type of project that comes before the town. The spirit & intent of these tools is clearly defined as such in the respective sections of our Zoning Ordinance.

In essence, both of these ordinances are expressly designed to help the Town monitor, evaluate, and plan for residential growth that is consistent with the Town's capacity for planned, orderly, and sensible expansion of its services to accommodate such development - including but not limited to education, fire protection, road maintenance, waste disposal, police protection and recreation. These two ordinances are grounded upon their correlation with the Master Plan and Capital Improvements Plans of the Town of Londonderry. More importantly, as a former Town Councilor, Planning Board member & Zoning Board member, I have personally witnessed the effectiveness of both of these sections of our zoning ordinance over the last 13 years. "Working in tandem", these tools allow us to accurately forecast the maximum amount of residential development that the town will need to plan for on-a-year-over-year basis. In effect, this combination is the ultimate planning tool (crystal ball) we have at our disposal

when it comes to managing the future impacts & infrastructure/service-related costs associated with residential development.

When the PUD was brought before the Council that I was a sitting member of for adoption, I specifically asked the town's planning staff at that time if subsequent PUD projects would be subject to both Residential Phasing & the GMO like any other zoning district. This was pre-Woodmont. The answer was YES. I also asked the same question again at a subsequent Woodmont-related workshop held by the PB, and again the answer from staff was YES. Additionally, I attended a public meeting/workshop held by the Town Council (last year I believe) that sought public input on the infrastructure needs & potential related impacts the Woodmont project might entail. Again I asked the same question and received the same answer. YES.

PHASING NOTE: From my reading of our current ordinance, the maximum number of residential "dwelling units" that any developer can build in any one year from the date of PB approval, ranges from 15 to 48, depending on the type of residential units being proposed in a given zoning district (AR-1 = 15, AR-1 w/water & sewer = 20, CS=25, IH = 25, RIII = 48, WFH = 48). It is clearly NOT the 60 or more units per year that has been previously mentioned during the initial charettes & subsequent workshops regarding this project.

GMO NOTE: While the Town has not been in a period of unsustainable growth in recent years, there is no guarantee or predictor (i.e. crystal ball) that says that won't happen again in the near future or distant future. Given the large number of residential units the Woodmont project is projecting to include and the estimated 20 year timeline for doing so, it's absolutely critical that the GMO ordinance remain in full effect as part of any Master Plan that is approved for Woodmont. It's equally important to note that the GMO works in parallel with the Residential Phasing Ordinance so as to provide the community and its residents with the highest level of protection against future infrastructure costs (current and long-term).

While I'm not a lawyer and have no legal training, it's my understanding that there is no legal requirement that would require the Londonderry Planning Board to waive or exempt ANY development from these two sections, absent a variance from the ZBA. While this applicant or any other before you certainly has a right to request a waiver or exemption from one or both of these sections as part of their application submission, it is solely in your purview to decide whether or not to grant such requests.

Speaking as one member of the community, I believe it is critical that the PB come to a consensus "early in this process as possible" that these two sections of our zoning ordinance should remain in FULL effect as part of any PUD Master Plan approved for Woodmont. A project of this size, scale & length of development time requires nothing short of that. To do otherwise IMO, would be poor planning on our part and a disservice to our collective constituents (the residents & taxpayers of Londonderry).

I would therefore urge the Planning Board to take this matter up for full discussion in short order, make a formal decision and provide that feedback to this applicant. Doing so now in my opinion, would greatly assist in setting the proper expectations for all concerned going forward – including the applicant and the community at large.

I have similar concerns regarding the applicant's request to eliminate all CUP's from the Woodmont Master Plan, their request to bypass the town's duly appointed ZBA for variances, and supplanting our well-crafted and effective sign ordinance with their own. In addition, I also don't see any reference to our Heritage Commission playing an active role in the design review process for the commercial buildings & associated signs the project will be proposing. I strongly believe the Heritage Commission's long-standing advisor role to the Planning Board in this regard, should be formally included in any MP approved for Woodmont. I will be sharing my comments and specific concerns on these additional items at the next available hearing.

I want to thank you for providing this opportunity for me to speak this evening and I also want to thank you for your service to the town. It's very much appreciated by me.

That concludes my comments Mr. Chair. If the you or the board have any questions for me at this time, I would be glad to try and answer them.

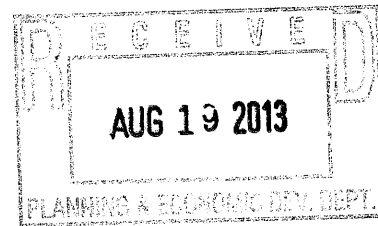
Sincerely,

Michael Brown
5 Carousel Court
Londonderry, NH

Mike Speltz
18 Sugar Plum Lane
Londonderry, NH 03053

July 16, 2013

Art Rugg, Chair
Londonderry Planning Board
268-B Mammoth Road
Londonderry, NH 03053



Mr. Chairman,

Due to a long-planned trip, I am unable to attend the next Planning Board meeting that will continue the discussion of the proposed Woodmont Commons PUD master plan. Please include into the official record the following comments intended for the public hearing portion of the meeting. (As of this date the Development Agreement has not been made public). I am confident that many of the concerns I might include in this letter will be addressed by others, so I will not repeat them here. There are two overall themes that I urge the Board to consider.

Justifying departures from existing ordinances and regulations

While one of the primary purposes of the PUD is to create flexibility for both the developer and the town, the intent of a PUD is not to abandon development guidelines; rather it is to allow a unique set of guidelines adapted to a particular place. Thus, for every provision of an ordinance or regulation that is superseded by the PUD master plan and development agreement, there must be a corresponding replacement guideline in the PUD master plan, and/or a rationale for the departure from "business as usual."

In some areas the proposed plan accomplishes this, for example, proposing detailed setbacks for various structure types and making it clear that the "cookie cutter" setbacks in our current regulations are too simple for this complex development. However, in other areas, such as the request to ignore the conservation overlay district, no replacement guidelines are provided and no compelling rationale is presented. The Board should ensure that each requested waiver is both *explicitly* justified and the purpose of the superseded provision is achieved in some other manner.

Deciding on appropriate density

Beyond the technical details of the proposal, the common theme expressed in many public comments is whether such intensive and dense development is appropriate for Londonderry. Will it diminish the quality of life in a community largely set among

orchards and woods (if one looks past the strip malls)? On the other hand, Woodmont Commons has the potential to provide another, more urban, life style choice. Its density increases the likelihood, but does not guarantee, that the project as a whole will be tax positive. Where can the Planning Board look for guidance to navigate between these two extremes?

The PUD ordinance provides five general considerations and fifteen specific objectives to guide the Board in judging “appropriate land uses, densities, and other development standards” (Section 2.8.8). The Board may find that making a judgment on each of these criteria would be a systematic way to proceed. I believe the proposed development falls short on at least the following criteria:

- 2.8.8.2.1, Inclusion of a harmonious mix of uses, in that the proposed uses are almost entirely residential and commercial, with no provision for public facilities, no specific proposed agricultural use, and very limited provision of outdoor recreation space outside of wetlands.
- 2.8.8.2.4, Preservation of open space, in that the proposed open space is almost entirely undevelopable land located either on the east side of I-93 or in the existing large wetland, known locally as the “Duck Pond.”
- 2.8.8.2.5, Preservation of natural vegetation and other important natural features, in that the most important natural feature of the site, the prime agricultural soils, will be largely lost.
- 2.8.8.2.6, Development of active or passive recreational areas, in that the provision of space for passive outdoor recreation on dry ground is extremely limited.
- 2.8.8.2.12, Sustainable design and construction practices promoting energy conservation, in that the plan does not address, much less incentivize, subsequent development of individual structures to conserve energy.

Two years ago Andres Duany, in a series of public informational meetings held in the Woodmont apple storage facility, introduced us to the “New Urbanism” and its conceptual implementation on the 600 acres comprising Woodmont Commons. In fact, Andres Duany both literally and figuratively “wrote the book(s)” on the New Urbanism, making copies of *The Smart Growth Manual* available at the public meetings. Mr. Duany is also a key contributor to the Smart Code, a model zoning ordinance that creates a legal framework to guide New Urbanism development. Our new Town Master Plan is filled with the concepts and even the vocabulary of Mr. Duany’s work. The same can be said of much, but not all, of the proposed PUD master plan.

New Urbanism is built on two fundamental concepts: the “rural-urban transect” and neighborhoods based on “pedestriansheds.”

The rural-urban transect describes the continuum of land uses ranging from wilderness to the urban core. In our town master plan, these are called “character zones.” They describe the types of development that should occur, or not occur, in a given area, called a “sector” in planners’ jargon. The key point is that each sector should have a mix of development types.

A “neighborhood” is based on how far a person can walk in five minutes, about one quarter mile. This is called a pedestrianshed, because, like a watershed, it maps where people “flow” from the edge to the center, which might be a village center, commercial area, or transit stop. The key point is that neighborhoods are the building blocks, and each should include a center or focal point and several of the development types on the rural-urban transect.

The problem that we in Londonderry have right now is agreeing on how much of Woodmont Commons should fall on which part of the rural-urban transect: how much should preserve what Duany calls “legacy farmland,” how much should be cluster development surrounded by open space, how much should be traditional dense neighborhoods surrounded by open space, and how much should be a regional center development built around I-93 and mass transit. The Woodmont Common subareas appear to serve as neighborhoods, so another way to ask the question is: which of the subareas, WC-1 through WC-12, should fall into which type of development.

Our new town master plan places Woodmont Commons in the “Intended Growth Sector.” In the Smart Code, Duany and his colleagues advise, for this sector, a mix of traditional, compact neighborhoods and regional center types of development that include elements on the rural-urban transect ranging from “suburban” to “urban core.” However our town master plan also states, “In all Sectors, the Town should work proactively to permanently protect open spaces that are identified in the 2010/2011 Open Space Plan and maintain the rural character of Town” (page 151).

The current proposal provides a limited mix of development types across the 600 acres of Woodmont Commons, but, to comply with the guidance in the Master Plan and to conform to the standards established by their own consultant and his colleagues, Woodmont Commons should go further in buffering the urban core proposed for subarea WC-1 with more open space among the densely developed neighborhoods (subareas) and, in particular, should use the added open space to preserve and make available to its residents the extremely valuable agricultural soils on the property. In short, the “mix” in this mixed-use development should be broadened to include cluster neighborhoods of less intensive development, in accordance with nationally accepted standards for smart growth.

I thank you and the Planning Board members for the opportunity to point out a middle ground between maximizing the market value of the development and maximizing our remaining natural resources.

Sincerely,

Mike Speltz

Jaye Trottier

From: Art Rugg <arthur_rugg@vrtx.com>
Sent: Monday, August 19, 2013 7:11 AM
To: Cynthia May; Jaye Trottier
Cc: mramsdell@ramsdelllawfirm.com
Subject: FW: Woodmont Commons



From the Stocks, to be read into the record at our next Planning Board meeting.

Art

From: walterstocks39@comcast.net [<mailto:walterstocks39@comcast.net>]
Sent: Sunday, August 18, 2013 5:15 PM
To: Art Rugg; mjws2000@comcast.net; jfarrell@londonderrynh.org; tdolan@londonderrynh.org; townmanager@londonderrynh.org
Cc: lynnbwiles@myfairpoint.net; lelazem@hotmail.com; chemchief@comcast.net; RBrideau@londonderrynh.org; laferrij@myfairpoint.net; Reanew2@comcast.net; sbenenson@bensonslumber.com; leithareilly@hotmail.com; tfreda@londonderrynh.org; jgreen@londonderrynh.org; jjbutler@londonderrynh.org; jack@Falvey.org; Chris davies nh
Subject: Re: Woodmont Commons

Londonderry Planning Board please read this ENTIRE Email, below, dated August 18, 2013 into the minutes of your next meeting on Woodmont Commons .

Londonderry Town Council please read the ENTIRE Email, below, dated August 18, 2013 into the minutes of your next meeting.

Thank you,

Walter & Marilyn Stocks

39 Gordon Dr

Londonderry, NH

03053

From: walterstocks39@comcast.net
To: "Arthur rugg" <Arthur_rugg@vrtx.com>, mjws2000@comcast.net, jfarrell@londonderrynh.org, tdolan@londonderrynh.org, townmanager@londonderrynh.org
Cc: lynnbwiles@myfairpoint.net, lelazem@hotmail.com, chemchief@comcast.net, RBrideau@londonderrynh.org, laferrij@myfairpoint.net, Reanew2@comcast.net, sbenenson@bensonslumber.com, leithareilly@hotmail.com, tfreda@londonderrynh.org, jgreen@londonderrynh.org, jjbutler@londonderrynh.org, jack@Falvey.org, "Chris davies nh" <Chris.davies.nh@gmail.com>
Sent: Sunday, August 18, 2013 4:50:46 PM
Subject: Woodmont Commons

Londonderry Planning Board Members, Londonderry Town Council Members and Londonderry Town Manager

In the Derry News, August 15, 2013, page 19 there is a foreclosure sale at public auction for a local restaurant along Route 102, in Londonderry, that states along heavily traveled Route 102, average daily traffic count of 23,000 (see attachment). Route 102 is a 4 lane highway, in the business district, with multiple traffic lights and multiple turning lanes. The Woodmont Commons projected traffic count is 40,000 to 50,000 vehicles a day. Can you imagine one and a half to two times plus traffic along Gilcreast Rd, Pillsbury Rd, Hardy Rd, Gordon Dr, Wedgewood Dr, Hovey Rd, Londonderry Rd and many other local roads in Londonderry?

The Woodmont Commons project, when first presented to the Londonderry tax payers was a walking community with less traffic. The Woodmont Commons project, if approved, needs to be revised to scale back the traffic. Examples of this, could be more single family houses, with a minimum of 1 acre lots whose back boundaries match the existing house lots that they abut. Or the high rise structures, need to be scaled down to two stories maximum. And structures like hotels, hospital, multiple condominiums and multiple apartment buildings need to be eliminated.

Even if you are not an abutter or live near this project, you are going to be affected by it. The developer of this project wants the existing roads and any other existing improvements to be payed by the Londonderry tax payer for the next 20 years.

If it is illegal for the Londonderry Town Council to be involved in this project, something needs to be changed, so that our elected officials are looking out for the Londonderry tax payers.

Attention: Londonderry tax payers you need to attend the Londonderry Planning Board and Town Council meetings. Email and write letters to them about this project, before it is too late and we are paying for this project for the next 20 years.

Thanks,
Walter & Marilyn Stocks
39 Gordon Dr
Londonderry, NH
03053

This email message and any attachments are confidential and intended for use by the addressee(s) only. If you are not the intended recipient, please notify me immediately by replying to this message, and destroy all copies of this message and any attachments. Thank you.

**WOODMONT COMMONS
DEVELOPMENT AGREEMENT**

Pillsbury Realty Development, LLC

HYRAX Derry Partners, LLC

Demoulas Super Markets, Inc.

Robert D. and Stephen R. Lievens

and

The Town of Londonderry, New Hampshire

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WOODMONT COMMONS DEVELOPMENT AGREEMENT

This Woodmont Commons Development Agreement (the "Agreement") is made and entered into this ____ day of August, 2013 by and between Pillsbury Realty Development, LLC, HYRAX Derry Partners, LLC, Demoulas Super Markets, Inc., and Robert D. and Stephen R. Lievens and the Town of Londonderry, New Hampshire (hereinafter the, "Town"), all of which have legal or equitable interests in the properties listed on Exhibit A (the "PUD Properties") attached hereto.

I. STATEMENT OF PURPOSE

Pillsbury Realty Development, LLC (hereinafter, the "Developer") has petitioned the Town to recognize as a Planned Unit Development ("PUD") approximately 600 acres within the Town's jurisdiction for a sustainable, multi-phased, mixed use commercial, industrial and residential planned unit development to be known as "Woodmont Commons" (hereinafter referred to as "Woodmont" or the "Development"). Pillsbury Realty Development, LLC, HYRAX Derry Partners, LLC, Demoulas Super Markets, Inc., and Robert D. and Stephen R. Lievens (hereinafter, collectively the "Landowners") each own real property within Woodmont. Woodmont is a unique and innovative land use development which the Town recognizes for the potential benefits – fiscal, environmental, and otherwise – that the Development may offer to the residents of the Town.

Woodmont is a project of magnitude and complexity with elements that are described in a planned unit development master plan and referenced in this development agreement between the Developer, the Landowners and the Town. This Agreement is subject to, and will be executed upon, approval by the Londonderry Town Council following a public hearing and in

connection with approval of the Woodmont Commons Planned Unit Development by the Londonderry Planning Board, as requested by the Developer.

NOW, THEREFORE, based upon the terms and conditions set forth herein and in consideration of the mutual promises and assurances herein provided, the parties agree as follows:

II. BACKGROUND

A. The Developer, on behalf of the Landowners, has petitioned to recognize the Properties as a PUD, as defined in a planned unit development master plan, which comprise approximately 600 acres located within the Town and generally straddling Interstate 93 between existing exits 4 and 5 in the location of Pillsbury Road and its intersection with Gilcreast Road. The Development real property is more particularly described within Exhibit B, the so-called “Woodmont Commons Master Plan: Planned Unit Development” (the “PUD Master Plan”), which is attached hereto and incorporated herein by reference. The PUD Master Plan includes overall conceptual plans, permitted land uses, development and design standards related to land use and infrastructure, and site plan and subdivision review procedures specific to the Woodmont Commons PUD.

The real property described and depicted within Exhibit B comprises the Development and shall be subject to the terms and provisions of the planned unit development master plan and this Agreement. This Agreement may be amended as provided by law (i) to extend to additional real property rezoned by the Town and incorporated into Woodmont, (ii) to modify its terms and provisions, and (iii) to address additional matters, whether or not contemplated or potentially contemplated at the time of first execution.

B. After careful review and deliberation, the Londonderry Town Council has determined that, subject to the Planning Board's approval of the PUD application, Woodmont is consistent with the Town's land use planning objectives and policies, as well as the health, safety, welfare and economic well-being of residents of and visitors to the Town.

C. The Developer, the Landowners and the Town enter into this Agreement for the purpose of: 1) confirming the potential maximum amount of, and the permitted types of, uses and associated development and design standards for Woodmont Commons; 2) coordinating the construction, apportionment of use, and design of infrastructure that will serve the Development and the community at large; 3) confirming the regulations relating to the dedication and/or provision of public facilities by the Developer as described herein; and 4) providing assurances to the Developer that it may proceed with the Development, in good faith reliance upon the Woodmont planned unit development master plan as an approved PUD and without encountering future changes in ordinances and regulations that would materially impair its ability to develop Woodmont as contemplated in the approved planned-unit development zoning and under the terms of this Agreement.

III. ADOPTION, DURATION, AND DEVELOPMENT

A. Public Hearing.

The Londonderry Town Council conducted a public hearing on August ____, 2013 to consider the approval and execution of this Agreement at the recommendation of the Planning Board. The Notice of Public Hearing specified, among other things, the location of the Development, the planned unit development master plan applicable to the Development, and a place where a copy of this Agreement and the Woodmont PUD Master Plan could be obtained.

During the public hearing, the Council approved this Agreement, subject to the Planning Board's approval of the PUD Master Plan, as requested by the Developer.

B. Duration.

The term of this Agreement shall commence on the date of its execution by the parties, which has been inserted on the first page hereof. Subject to any subsequent agreement extending the original duration, this Agreement shall expire twenty (20) years from the date of execution.

C. Development of Woodmont.

1. PUD Master Plan.

Subject to subparagraph III.D. hereafter, Woodmont shall be developed in accordance with the terms and conditions of the PUD Master Plan approved by the Planning Board and authorizing rezoning. A copy of the approved PUD Master Plan is attached hereto as Exhibit B. Capitalized terms not otherwise defined in this Agreement shall have the same meaning set forth in the PUD Master Plan.

2. Single Planned-Unit Development.

Woodmont shall be developed as a single planned-unit development and considered one comprehensive project. Analyses regarding density, transportation, open space and other matters have been made consistent with the entire Development and not in isolation or on a project-by-project basis as each project shall be integrated into the entire Development and PUD Master Plan.

3. Timing Schedule for the Development.

Woodmont shall be developed as provided for in the PUD Master Plan, with development areas and the timing of development determined on the basis of sound planning and demographic and market conditions, and as approved in the form of subdivision and site plan

approvals by the Planning Board. Subject to the requirement that active and substantial development shall commence within four (4) years and such other requirements as the Planning Board conditions its approval of the PUD Master Plan, the identification of development areas and the timing of development shall be determined by the Developer, as defined in the PUD Master Plan, and as may be designated or assigned by the Developer from time to time.

D. Future Changes and Modifications.

It is understood and agreed that because of its magnitude and complexity, the development of Woodmont will occur over a period of multiple years and in market conditions that cannot accurately be predicted or anticipated. Further, as a planned unit development to be developed with the principles of anticipatory design as set forth in the PUD Master Plan, Woodmont will utilize technologies, methods, designs, and techniques that will change over time. Accordingly, it is acknowledged and agreed that the Developer may seek Planning Board approval for modifications and supplementations to the PUD Master Plan from time to time as provided in the PUD Master Plan. All references to the PUD Master Plan in this Agreement shall include and consider all such minor and major amendments to the PUD Master Plan as may be approved by the Planning Board from time to time.

E. Declaration of Restrictive Covenants.

It is understood and agreed that prior to the Developer's or Landowners' conveyance of any property within Woodmont, and subsequent to the execution of this Agreement, the Developer shall record in the Rockingham County Registry of Deeds with respect to the Development a Declaration of Restrictive Covenants and other conditions upon conveyances, in a form attached hereto as Exhibit C, (hereinafter, collectively, the "Declaration"), duly executed by the record owner or owners of each parcel included within the Development, stating that the

Development is within the zoning jurisdiction of the Town of Londonderry and is zoned as a planned-unit development pursuant to the terms of the PUD Master Plan. Said Declaration shall further state that the Development and the use of property therein are subject to regulations applicable to Woodmont as stated in the PUD Master Plan, a copy of which is available for inspection at the Planning and Economic Department of the Town of Londonderry (the "Planning Department) at the Londonderry Town Hall, 268 B Mammoth Road, Londonderry, New Hampshire 03053. In addition, the Declaration shall specify that the Developer, its successors and assigns, as the Developer as therein defined, has the preliminary and initial authority to make certain decisions with regards to the use and improvement of real properties comprising Woodmont (prior to the filing of applications for individual projects with the Town's land use boards). Finally, the Declaration shall provide that (i) each deed conveying land within Woodmont recorded subsequent to the recordation of the Declaration shall state that the respective conveyance is subject to this Declaration, but that the failure to include such statement, or to include a sufficiently suitable statement, in a deed, shall not affect the enforceability of the Declaration with respect to the property conveyed or invalidate the respective conveyance; and (ii) no projects, rezoning petition, PUD Master Plan adjustment, variation, alteration or amendment, application for variance, special exception or conditional use approval shall be submitted to the Town, or its land use boards, or proposed in any manner, without the written consent of the Developer, which shall not be unreasonably withheld.

IV. PUD MASTER PLAN PERMITTED USES AND POTENTIAL TOTAL DEVELOPMENT

The permitted uses and potential total development for Woodmont are established in the Section 2.2.3 of the PUD Master Plan. In the event that additional real property is incorporated

into Woodmont, the total authorized development shall be adjusted as set forth in an amended PUD Master Plan.

V. ARCHITECTURAL AND DESIGN STANDARDS

Architectural and design standards for Woodmont shall be as set forth in the PUD Master Plan.

VI. TRANSPORTATION

A. General.

1. Transportation Impact Assessment and Responsibility for Mitigation and the Cost of Transportation Improvements.

As provided in Section 2.3.3 of the PUD Master Plan, a Transportation Impact Assessment, with its multiple iterations, has been prepared for Woodmont and attached to the Master Plan as a Supplemental Document. The Transportation Impact Assessment, along with Section 3.0 of the PUD Master Plan, addresses the potential maximum densities and allowable building square footage of the land uses authorized in the PUD Master Plan and recommends both on-site and off-site thoroughfare mitigation and/or improvements to be completed at intervals, as discussed in the Master Transportation Impact Analysis included at Section 4.2 of the Master Plan, and as occurring during the build-out of Woodmont.

The Developer shall be solely responsible for the cost of on-site and PUD Master Plan-related off-site thoroughfare mitigation and/or transportation improvements as discussed in the Master Transportation Impact Analysis, as the same Analysis may be supplemented from time to time, and as approved by the Planning Board in the form of subdivisions and site plans developed within the PUD and in accordance with the PUD Master Plan. The additional traffic and transportation capacities triggered by PUD Master Plan-related mitigation and improvements shall be reserved by the Town, and the Planning Board, for consumption and utilization by

Developer for Woodmont and its successors and assigns specifically in regards to development of the PUD Master Plan.

The Town, through its various administrative departments and municipal boards, shall require that all third parties seeking to develop in the Town, and potentially having affect upon or having impact to thoroughfares, intersections and other transportation infrastructure mitigated and/or improved by Developer in connection with the development of the PUD Master Plan study transportation impacts using the assumption that the PUD will reach its projected full build-out. These third party projects shall be evaluated and all traffic generation and transportation impacts for such third party projects shall be reviewed and undertaken with the assumption that the PUD Master Plan is or shall be fully developed. For example, a third party proposing commercial development outside of the PUD, but in such reasonable proximity to the PUD that its traffic generation will mix with Woodmont's traffic generation, then such third party proposal shall be considered and evaluated as if the PUD Master Plan has been fully developed.

To the extent PUD-related mitigation and/or improvements, paid for by the Developer, generates traffic and thoroughfare capacities that are beyond that which is reasonably necessary to service the subdivisions and site plans occurring in accordance with the anticipated development of the full PUD Master Plan, then Developer shall be entitled to receive, and the Planning Board shall be required to arrange as a condition of such third-party's planning approval, reimbursement to Developer for third-party's utilization and/or consumption of said excess capacity.

2. NHDOT and FHWA Approvals.

The Developer and the Town agree that all roads owned, managed and maintained by the State of New Hampshire are subject to permits and approvals of the New Hampshire Department of Transportation ("NHDOT") and the Federal Highway Administration ("FHWA"). With respect to any permit or approval necessary for a site plan or project approved by the Planning Board, the Town shall support the efforts of the Developer in obtaining any and all requisite permits and approvals from NHDOT and FHWA for development of Woodmont consistent with the general development threshold and mitigation concepts contained within the PUD Master Plan, including the furtherance of the Town's Master Plan with respect to infrastructure improvements to existing Town and State improvements.

B. Dedications and Maintenance.

With the exception of such roadways and traffic improvements as are determined by the Developer in accordance with the PUD Master Plan to be private and to be maintained at the expense of the Developer, all roadways, thoroughfares and traffic-related improvements within Woodmont may be publicly dedicated, accepted and maintained, if approved by the Planning Board and accepted by the Town Council.

C. Thoroughfare Standards.

Roadways, thoroughfares, sidewalks and pathways, and traffic-related improvements within Woodmont shall be designed and constructed as provided in the PUD Master Plan and the Master Transportation Impact Analysis included at Section 4.2 of the PUD Master Plan, subject to such standards of the Town, NHDOT, and FHWA.

D. Traffic Signals.

The Developer shall maintain the right to purchase, construct, and install poles and traffic signals within Woodmont consistent with its design standards, subject to the approval of the traffic signal design by the Town and State for safety, function, and compliance with Town Standards and/or NHDOT standards, whichever are applicable. All poles and traffic signals may be dedicated to, accepted by and thereafter become the property of and maintained by the Town or other relevant governmental entity, if they are within the public right-of-way or thoroughfare.

E. Condemnation of Right of Way or Easements for Off-Site Improvements.

If acquisition of right of way or easements is necessary or appropriate to meet any requirement of the PUD Master Plan, this Agreement, or the Transportation Impact Study, then the Developer, if it is responsible for the construction of the improvement, will exert reasonable and good faith efforts to obtain by private negotiation from the relevant property owners, the right of way or easements necessary to meet such requirement. In the event that the Developer is unable to obtain any of the required right of way after exerting reasonable, good faith efforts to negotiate to do so, the Developer may request that the Town obtain the required right of way by exercise of eminent domain. If in the exercise of its discretion to open streets and install infrastructure, the Town determines that certain property is necessary for a public use and purpose, as the same is referenced in RSA Chapter 498-A, which determination shall not be unreasonably delayed, then the Town shall without undue delay obtain the required right of way or easement by purchasing the same or through eminent domain proceedings. In the event that the Town purchases any required right of way or easements at the request of the Developer, then the Developer shall reimburse the Town for its purchase price and reasonable expenses related to the purchase. In the event that the Town acquires any required right of way or easements through

eminent domain proceedings, the Developer will reimburse the Town for any award of just compensation and/or damages (as determined through settlement or verdict), including interest, that the Town is required to pay, and for appraisal fees, reasonable attorney's fees and other reasonable costs and expenses incurred by the Town in connection therewith. The Developer shall be entitled to recommend qualified condemnation counsel to the Town, should the Town elect to use outside counsel, although the final decision on selection of condemnation counsel is left to the Town. The Developer shall not be liable for settlement payments in excess of appraised value unless it has given its prior written consent to be bound by the same.

VII. PUBLIC FACILITIES

A. Public Financing.

The Town and the Developer acknowledge that Woodmont may provide for economic growth, the expansion of the non-residential tax base, and the creation of employment opportunities within the Town. The Town agrees to cooperate with and assist the Developer and other applicable governmental entities, including the State of New Hampshire and various Federal Governmental entities, to facilitate the development and construction of public facilities that the Town deems in the best interests of the citizens of Londonderry. The Developer believes that some form of Public Financing will be necessary for the development and construction of certain public facilities and utilities. For the purpose of this Agreement, Public Financing shall refer to: (i) any grant, reimbursement, loan or other payment made by any governmental entity with respect to the development and construction of such public facilities, in whole or in part, (ii) the proceeds of any bond financing lawfully issued by any governmental entity used for such purposes (other than general obligation bonds); and, (iii) the authorization of one or multiple development districts available pursuant to RSA Chapter 162-K, including the adoption of a tax

increment financing plan pursuant to RSA 162-K:9 to offset the private costs of developing infrastructure and other improvements consistent with RSA 162-K:6, III within Woodmont. If approved, Public Financing may be utilized for all costs associated with the development and construction of such public facilities and development district improvements, including planning, permitting, design, fees, mitigation, engineering, construction, construction management, bonding, insurance, construction finance expenses, and all other costs permitted by applicable law.

1. Road and Transportation Improvements.

Construction of, and improvements to, major road and transportation facilities in, through and/or proximate to the periphery of Woodmont, as identified in the Master Transportation Impact Analysis incorporated into the PUD Master Plan at Section 4.2, including interchanges with, and improvements on, Interstate 93, Exit 4a, and State Route 102, are to be constructed in accordance with the PUD Master Plan and the Master Transportation Impact Analysis. With the approval of the Planning Board, the Developer may construct such other transportation or parking related facilities as may be appropriate to further the development of Woodmont. With the exception of parking related facilities, all transportation and related infrastructure referred to in this subsection shall be hereinafter referred to as "Major Road Improvements." Similarly, the Town shall periodically update its municipal master plan in accord with the requirements of RSA 674:1 et seq.

2. Wastewater Collection System.

A wastewater collection system shall be installed in phases to serve Woodmont and the Town. Pump stations, force mains and gravity sewer lines will be constructed in

accordance with the PUD Master Plan and as approved by the Planning Board in subsequent subdivision and site plan applications.

3. Potable Water Distribution Lines and Facilities.

Major water distribution lines will be constructed in accordance with the PUD Master Plan as needed to serve Woodmont and the Town. Further, if required to maintain potable water storage and system pressure requirements, the Master Developer will work with the Town and Pennichuck Water Works to secure an appropriate site within the PUD for, and the construction of, a water storage tank and/or potable water storage facilities.

4. Recreational and Community Facilities.

The Developer may provide for the construction of the recreation facilities to benefit Woodmont, the Town and surrounding areas, including but not limited to those provided for in the PUD Master Plan.

B. Water and Wastewater Systems.

1. Wastewater Capacity.

For Woodmont to meet its long term development goals, and provided mutually-agreeable financial arrangements are made between Woodmont and the Town, the Town shall provide and/or contract with the Town of Derry, New Hampshire or the City of Manchester, New Hampshire, over the term of this Agreement, for sufficient wastewater capacity as development occurs within Woodmont. Wastewater generated within the Development shall be treated in accordance with the PUD Master Plan.

2. Water Supply.

For Woodmont to meet its long term development goals, and provided mutually-agreeable financial arrangements are made between Woodmont and the Town, the Town shall

provide and/or contract with Pennichuck Water Works, over the term of this Agreement, for sufficient water supply as development occurs within Woodmont, at locations consistent with the PUD Master Plan, and at the periphery of Woodmont, the exact locations of which shall be mutually agreed upon by all parties.

VIII. FISCAL MATTERS

A. General.

It is acknowledged and agreed (i) that the public facilities developed and constructed pursuant to the foregoing Article VII and dedicated to the Town may facilitate economic growth, expand the Town's tax base, and create employment opportunities for the Town's citizens, all with significant economic benefit to the Town; and (ii) if so, it is appropriate that certain credits should be made available to the Developer for expenditures associated with such public facilities and utilities. Further, it is acknowledged that the Woodmont Commons Fiscal Impact Analysis performed by Development Planning & Financing Group, Inc. ("DPFG"), dated May 17, 2013, as peer-reviewed by RKG Associates, Inc., reasonably projected that the PUD will be substantially fiscally-positive at full build-out at the conclusion of an estimated twenty (20) years.¹

B. Commitment to Maintain Net Positive Fiscal Impact.

While DPFG projected that the PUD will be substantially fiscally-positive at projected full build-out at the conclusion of twenty (20) years, it is acknowledged that the PUD may not always maintain cumulative and continual net positive fiscal impact at all times throughout the projected span of the twenty-year development projection. Further, since said fiscal projections are inherently based upon estimations, and the actual sequence development is unknown at this

¹ RKG Associates opined that the development should be revenue positive at full build-out, although likely less so than DPFG's estimate.

time, such projections are only reliable to the extent they ultimately mimic future realities. Therefore, to protect the Town against financial shortfalls, Developer will reassess cumulative fiscal impacts annually and report the same to the Town. The annual update of the Woodmont Commons fiscal impact analysis will rely upon the transparency established in the Woodmont Commons Fiscal Impact Analysis dated May 17, 2013 (2103 Report), using the following: (1) real property tax revenues shall be calculated by the Town of Londonderry Tax Assessor for all real property within the PUD area based on the assessed value as of April 1 of the previous year; (2) the Town of Londonderry Town Clerk shall calculate the motor vehicle permit fees based on the motor vehicles registered to addresses within the PUD area; and, (3) incremental general fund expenditures generated by Woodmont Commons based on methodologies employed in the 2013 Report (a copy of which is attached to this Development Agreement as Exhibit C) with inputs updated to reflect (a) current data and costs and (b) the current budgeting. Should said reporting indicate that Woodmont, taken cumulatively and as a whole, has placed a negative impact upon the financial resources of the Town in the years from inception to the year of review, then Developer shall make payment to the Town, to correct the deficiency. Said payment to the Town shall be in the amount necessary to bring Woodmont back to cumulative financial neutrality for the years assessed

C. Sewer and Water Access, and Construction User Fees.

The purpose of Sewer and Water Access, and Construction Fees for water and sewer service is to recover the cost of previous capital investments in water and sewer systems and to build capital reserve funds for future investment in water and sewer collection, distribution and treatment facilities. In view of the investment necessary to provide water and sewer service to and within Woodmont, the Developer will receive a credit against prevailing impact fees, as the obligation to pay the same may be modified by the PUD Master Plan, payable with respect to

development within Woodmont for any and all capital expenditures made in water and sewer infrastructure that is dedicated to the Town, on a dollar-for-dollar basis and prorated for its share of use.

D. Recreation Impact Fees.

Subject to Planning Board approval, the Developer may receive a credit against any recreation impact fees owed with respect to development within Woodmont for the value, included any amounts expended upon the creation, restoration or rehabilitation of any open space historic or cultural resource sites or facilities dedicated to the Town or donated to any non-profit entity for the purpose of recreation. In addition, subject to Planning Board approval, the Developer may receive a credit against any Recreation Impact Fees owed for any amounts expended on the development of public parks, recreation or open space sites as stated in the PUD Master Plan.

E. Transportation Development Fees.

The purpose of Transportation Development Fees is to recover a portion of the cost associated with the construction of Major Road Improvements. Accordingly, the Developer shall receive a credit against prevailing Transportation Development Fees owed for amounts expended on the construction of Major Road Improvements, as discussed in the Master Transportation Impact Analysis included at Section 4.2 of the Master Plan, and as provided in this Agreement.

F. Access and Connection Fees.

The purpose of Access and Connection Fees is to recover a portion of the costs associated with the construction of water and wastewater treatment facilities. The calculation of the fee owed is based on Town or State published estimates of daily combined water/sewer service

demand. To the extent that standards that require reduction in water use are adopted for Woodmont, then Access and Connection Fees owed with respect to development within Woodmont shall be calculated based on the reduction in daily combined water/sewer service demand or the actual usage data within the PUD, to the extent available. In addition, Access and Connection Fees shall be reduced by readings off a “deduct meter” for development within Woodmont, paid by Woodmont, that uses water for irrigation or reduces potable water consumption for irrigation. Access and Connection Fees shall be reduced by use of a “deduct meter” throughout Woodmont for the installation of water re-use distribution infrastructure as Woodmont is developed, and development within Woodmont shall be exempt from impact fees for water re-use infrastructure improvements that duplicate Woodmont’s then-existing systems.

G. Use of Fee Credits.

As Woodmont develops, to the extent that the Developer is entitled to fee credits under this Article VIII which exceed amounts of the respective fee then payable, the Developer may utilize the excess credit amounts in satisfaction of future development fees for similar development within Woodmont.

H. Optional Reimbursement.

With respect to the cost of any of the public facilities constructed under Article VII for which the Developer is entitled to a credit hereunder, when such public facilities serve other property owners who pay development fees with regard thereto, the Developer may elect to claim a credit for such cost as provided herein or, if it so elects, receive as a reimbursement that portion of the development fees paid by such other property owner equivalent to the amount of the credit. If the Developer elects to receive as a reimbursement that portion of the development fees paid by such other property owner equivalent to the amount of the credit, it shall be

incumbent on the Developer to provide the Town's Department of Planning and Economic Development with a full accounting of the credit and the reimbursement.

**IX. LAWS AND PERMITS IN EFFECT AT TIME OF AGREEMENT
GOVERN DEVELOPMENT**

A. Applicable Laws, Regulations and Ordinances.

Unless specifically provided in the PUD Master Plan, the Town Standards applicable to Woodmont are and shall be those in force for other, non-PUD Master Plan development as the same are amended from time to time by the Town. Accordingly, except as set forth in Section 2.8.3.8 of the Londonderry Zoning Ordinance and as the same may be modified in the PUD Master Plan and, the Developer shall have a vested right to develop Woodmont in accordance with the PUD Master Plan, the terms of this Agreement, and any of the Town Standards as they exist as of the date of this Agreement. To the extent there are any such revisions, implementations, or amendments to the PUD Master Plan subsequent to the date of this Agreement or establishes new, modified, or elective standards or regulations, the Developer shall become vested as to those laws, standards and regulations as though they existed at the date of the PUD Master Plan approval. Additionally, no future town development moratoria or increases in Town development impact or development fees shall apply to the Development without the written consent of the Developer. In other words, absent its written consent, the Developer shall pay the lesser amount of: (1) the development impact or development fees in effect at the time of Planning Board approval of a particular site plan or project; or (2) the development impact or development fees in effect on March 1, 2013. Each of the Developer and the Town shall, for the duration of this Agreement, retain on file copies of the PUD Master Plan and all other applicable Town Standards as they exist on the date of this agreement.

B. Statutory and Common Law Vesting.

The vesting of rights and privileges as described in subparagraph A. above shall not preclude the Developer from establishing permanent vested rights pursuant to the doctrines and principles of New Hampshire statutory (i.e., NHRSA 674:39) and common law.

X. ADDITIONAL PROVISION

A. Developer's Voluntary Capital Contributions to Town.

1. Police, Fire and EMS

Like any significant development project, the development of Woodmont, in accordance with its PUD Master Plan, and as development is subsequently approved in the form of future subdivisions and site plans by the Planning Board, is anticipated to consume a proportionate amount of essential municipal services, including resources for police, fire and emergency response.

At such time as the Town, through its Planning Board, as advised by the various subdivisions of the Town, reasonably determines that within the succeeding twenty-four (24) months said municipal police, fire and emergency responders may not be able to reasonably service the needs generated by the Town as the same are augmented by Woodmont, then Developer shall be responsible for the provision to the Town of the following: (1) temporary dedication and fit-up of a portion of the existing "Woodmont Packing House" located at 15A Pillsbury Road, Londonderry, NH for use as a municipally-operated police, fire and emergency substation; (2) permanent dedication and fit-up of a suitable substation facility to be made available within twenty-four (24) months after the Town has reasonably determined, as referenced above, that it cannot service the municipal police, fire and emergency response needs of the Town and, (3) purchase or reimbursement of the costs to purchase an EMS ambulance, police cruiser AND a police motorcycle for the Town (all of similar class and type as those then

and otherwise owned and operated by the Town). The Town shall be responsible for arranging the provision of any and all equipment, maintenance servicing and operations personnel to be stationed at 15A Pillsbury Road without contribution from Developer. Developer's contribution, as stated herein, shall be limited to the stated capital improvements, substation fit-up and the enumerated capital equipment items, as stated, and shall not under any circumstances extend to operating and maintenance expenses incurred in the normal course of staffing, utilizing and maintaining the substation or its integration with other then existing or future municipal assets, personnel or systems.

2. Cemetery Donation to the Trustees of the Trust Funds.

Following final approval of the Master Plan, and full execution of this Agreement, and after the successful conclusion of any applicable appeals periods relating thereto, Developer shall file with the Planning Board such land use applications as may be reasonably necessary to subdivide up to three (3) acres of land adjacent to and between Pillsbury Cemetery and Woodmont for the purpose of conveying said acres to the Town. Said land may be owned, held and utilized by the Trustees of Trust Funds for the benefit of Pillsbury Cemetery and for any other purposes relating to the construction, operation and maintenance of a cemetery, including, without limitation, administration, burials, visitation, landscaping, privacy buffering, privacy screening, and travel ways. Said subdivision and donation of the land shall constitute the full and complete donation by Developers for the benefit of Pillsbury Cemetery, as Developers shall have no further obligation, financial, engineering or otherwise, beyond the act of subdivision, and exclusive of site planning, other than conveyance of the donated lands.

3. Land for School within Woodmont.

At such time as the Town reasonably determines that within the succeeding twenty-four (24) months, it will become necessary to construct a school within Woodmont, Developer shall offer to sell to the Town a parcel of land in a mutually-agreeable location solely for the purpose of constructing a new

school at a price no greater than eighty-five percent (85%) of fair market value. The parcel shall be the minimum acreage reasonably necessary for development of a public grade school. If the parties cannot agree on the fair market value of the property, it shall be determined by an independent and contemporaneous appraisal performed by a mutually-agreeable appraiser with the cost of the appraisal divided equally between the Town and Developer.

B. Binding Effect.

The terms and provisions of this Agreement, and the benefits and burdens associated therewith, shall inure to the parties to this Agreement and their assigns, grantees and successors in interest.

C. Notices.

Any notice, demand, request, consent, approval or communication which a signatory party is required to or may give to another signatory party hereunder shall be in writing and shall be delivered or addressed to the other at the address below set forth or to such other address as such party may from time to time direct by written notice given in the manner herein prescribed. Such notice or communication shall be deemed to have been given or made when communicated by personal delivery or by independent courier service or by facsimile or if by mail on the fifth (5th) business day after the deposit thereof in the United States Mail, postage prepaid, registered or certified, addressed as hereinafter provided. Parties shall make reasonable inquiry to determine whether the names of the persons listed in this Agreement should be substituted with the name of the listed person's successor. Notwithstanding the foregoing, it is not the intent of this section that formal notice be made for regular or routine communications between the Town and the Developer concerning permit requests, site plan reviews or other daily matters related to the development of the project.

All notices, demands, requests, consents, approvals or communications shall be addressed:

To the Town at:

Kevin Smith
Town Manager
Town of Londonderry
268 B Mammoth Road
Londonderry, New Hampshire 03053

with copies to:

Michael D. Ramsdell, Esq.
Ramsdell Law Firm
46 South Main Street
Concord, New Hampshire 03301

To the Developer at:

Michael L. Kettenbach, Member
Pillsbury Realty Development, LLC
100 Ferry Street
Hudson, New Hampshire 03051

with copies to:

Ari B. Pollack, Esq.
Gallagher, Callahan & Gartrell, P.C.
214 North Main Street
P.O. Box 1415
Concord, New Hampshire 03302-1415

D. Construction.

The parties agree that each party and its legal counsel have reviewed and had equal opportunity to revise this Agreement and that any rule of construction resolving ambiguities against the drafting party shall not apply in the interpretation of this Agreement or any amendments or exhibits hereto.

E. Assignment.

With the Town's approval, which will not be reasonable withheld, the Developer may assign substantially all of its rights and responsibilities hereunder to subsequent developer(s) or successor(s), provided that no assignment shall relieve the assigning party of responsibility with respect to the remaining portion of the relevant parcel or parcels of land owned by the assigning party or the Developer's capital contribution obligation pursuant to section XA above without the written consent of the Town, which consent shall not be unreasonably withheld. In the event that the Developer sells all of its relevant parcel or parcels of land and assigns its respective rights and responsibilities to a subsequent developers or other successor in interest, then the Developer shall be relieved of all of its covenants, commitments and obligations hereunder.

F. Governing Law and Venue.

This Agreement shall be governed by the laws of the State of New Hampshire and interpreted by the Superior Court for the State of Hampshire or the United States District Court for the District of New Hampshire.

G. Counterparts.

This Agreement may be executed in several counterparts, each of which shall be deemed an original, and such counterparts shall constitute one and the same instrument.

H. Severability.

If any term or provision herein shall be judicially determined to be void or of no effect, such determination shall not affect the validity of the remaining terms and provisions.

I. Authority.

Each party represents that it has undertaken all actions necessary for corporate or public approval of this Agreement, and that the person signing this Agreement has the authority to bind the Town or the Developer.

J. Approval and Recordation.

This Agreement shall be executed by each of the Town, the Landowners and the Developer and recorded by the Developer within fourteen (14) days of the Town's execution. Failure to record within fourteen (14) days shall not be deemed a material breach of this Agreement enabling the Town to modify or terminate this Agreement.

IN WITNESS WHEREOF, the parties hereby set their hands and seals, effective the date first above written.

DEVELOPER
Pillsbury Realty Development, LLC

By: _____
Title: _____

LANDOWNERS
HYRAX Derry Partners, LLC

By: _____
Title: _____

Demoulas Super Markets, Inc.

By: _____
Title: _____

Robert D. and Stephen R. Lievens

By: _____
Individually

By: _____
Individually

TOWN
Town of Londonderry

By: _____
Title _____

Witness: _____
Title: _____

APPROVED AS TO FORM

Michael D. Ramsdell, Esq.
Town Attorney
State of New Hampshire
County of _____

I hereby certify that _____ (Name) personally appeared before me this day acknowledging to me that he voluntarily signed the foregoing document for the purpose stated therein and in the capacity indicated above.

Date: _____

Notary's Signature
My Commission Expires: _____

State of New Hampshire
County of _____

I hereby certify that _____ (Name) personally appeared before me this day acknowledging to me that he voluntarily signed the foregoing document for the purpose stated therein and in the capacity indicated above.

Date: _____

Notary's Signature
My Commission Expires: _____

State of New Hampshire
County of _____

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County of _____

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Date: _____

Notary's Signature
My Commission Expires: _____

EXHIBIT A
Depiction of Properties Subject to Agreement and Within Master Plan

EXHIBIT B
Approved PUD Master Plan for Woodmont Commons